DEVELOPMENT APPLICATION

APPLICATION NUMBER:	PLN-24-270
PROPOSED DEVELOPMENT:	Twelve Multiple Dwellings and works in the road reserve (Residential)
LOCATION:	168A Abbotsfield Road Claremont
APPLICANT:	Cunic Homes
ADVERTISING START DATE:	01/07/2025
ADVERTISING EXPIRY DATE:	15/07/2025

Plans and documentation are available for inspection at Council's Offices, located at 374 Main Road, Glenorchy between 8.30 am and 5.00 pm, Monday to Friday (excluding public holidays) and the plans are available on Glenorchy City Council's website (**www.gcc.tas.gov.au**) until **15/07/2025.**

During this time, any person may make representations relating to the applications by letter addressed to the Chief Executive Officer, Glenorchy City Council, PO Box 103, Glenorchy 7010 or by email to gccmail@gcc.tas.gov.au.

Representations must be received by no later than 11.59 pm on **15/07/2025,** or for postal and hand delivered representations, by 5.00 pm on **15/07/2025.**

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Prepared for
Cunic Homes

168a Abbotsfield Road Claremont

FLOOD HAZARD REPORT

FE_24055
12 August 2024

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Document Information

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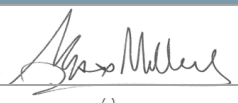
168a Abbotsfield Road, Claremont, Flood Hazard Report	**Cunic Homes**	FE_24055	Max W. Möller *Principal Hydraulic Engineer*

Document Initial Revision

REVISION 00	Staff Name	Signature	Date
Prepared by	Max W. Moller ***Principal Hydraulic Engineer***		22/07/2024
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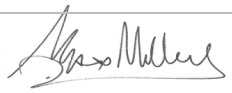
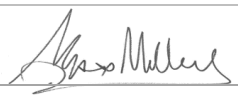
Reviewed by	Max W. Möller ***Principal Hydraulic Engineer***		12/08/2024
Authorised by	Max W. Moller ***Principal Hydraulic Engineer***		12/08/2024

Rev No.	Description	Prepared by	Authorised by	Date
01	Change in post-development scenario	Max W. Moller	Max W. Moller	27/01/2024

Contents

List of Tables

List of Figures

1. Introduction

Flüssig Engineers has been engaged by **Cunic Homes** to undertake a site-specific Flood Hazard Report for the development at 168a Abbotsfield Road, Claremont in the **Glenorchy City Council** municipality. The purpose of this report is to determine the flood characteristics on the existing and post-development hazard scenarios for the 1% AEP plus climate change, for the purpose of development.

1.1 Development

The proposed development consists of 12 unit dwellings, a concrete driveway including parking areas and a concrete path. Each unit is approximately 70 m^2 which are to be constructed on piers. The concrete driveway and parking areas will be 608 m^2 while the concrete path will be 59 m^2. The 2676 m^2 site currently has a building with an area of 80 m^2 which is to be demolished.

This development triggers the inundation code as the development falls within Glenorchy City Council, flood prone area.

1.2 Objectives and Scope

This report is to assess the proposed development at 168a Abbotsfield Road, Claremont under C12.0 Flood Prone Areas Hazard Code of the Tasmanian Planning Scheme 2021- Glenorchy (TPS 2021). The objectives of this study are:

- Provide an assessment of the site's flood characteristics under the combined 1% AEP plus climate change (CC) scenario.
- Provide comparison of flooding for post-development against acceptable solution and performance criteria.
- Provide flood mitigation recommendations for a potential future development, where appropriate.

1.3 Limitations

This study is limited to the objectives of the engagement by the clients, the availability and reliability of data, and including the following:

- The flood model is limited to a 1% AEP + CC worst case temporal design storm.
- All parameters have been derived from best practice manuals and available relevant studies (if applicable) in the area.
- All provided data by the client or government bodies for the purpose of this study is deemed fit for purpose and has not been checked for accuracy.
- The study is to determine the effects of the new development on flooding behaviour and should not be used as a full flood study outside the specified area without further assessment.

1.4 Relevant Planning Scheme Requirements

This report addresses the Tasmanian Planning Scheme codes C12.5.1 and C12.6.1 of the Flood Prone Areas Hazard Code of which the objective is to ensure that risk from riverine, watercourse or inland flooding is appropriately managed and takes into account the use of the buildings.

Table 1. TPS Planning Scheme Requirements

Planning Scheme Code	Objective
C12.5.1 Uses within a flood prone area	That a habitable building can achieve and maintain a tolerable risk from flood
C12.6.1 Building and works within a flood prone area	(a) building and works within a flood-prone hazard area can achieve and maintain a tolerable risk from flood; and
	(b) buildings and works do not increase the risk from flood to adjacent land and public infrastructure.

Specific details of this code and how this report addresses these requirements is shown in Table 8 and Table 9.

2. Model Build

2.1 Overview of Catchment

The contributing catchment for 168a Abbotsfield Road, Claremont is approximately 321 ha stretching from the peak located at Mount Faulkner to the development site with an average slope of 20-25 %. The immediate area surrounding the development has an average slope of 5-6%.

The land use of the catchment is Landscape Conservation and General Residential with the specific site being listed as General Residential. Figure 1 below outlines the approximate contributing catchment for the site at 168a Abbotsfield Road, Claremont.

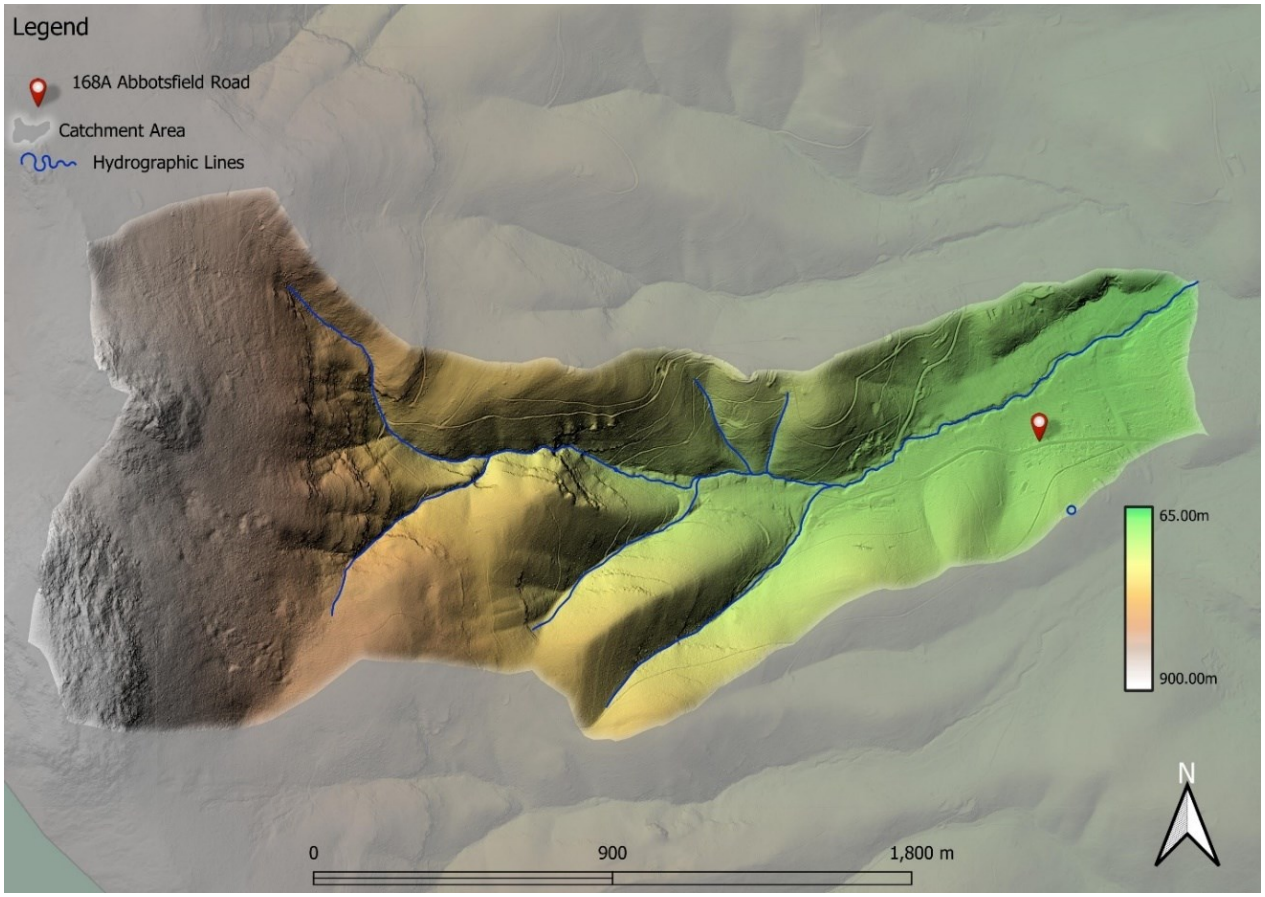


Figure 1. Contributing Catchment, 168a Abbotsfield Road, Claremont

2.2 Hydrology

Table 2 states the adopted hydrological parameters for the RAFTS catchment, as per best practice guidelines.

Table 2. Parameters for RAFTS catchment

Catchment Area (ha)	Initial Loss Perv/imp (mm)	Continuing Loss Perv/imp (mm/hr)	Manning's N pervious	Manning's N impervious	Non-linearity factor
321	27/1	3.8/0.0	0.045	0.02	-0.285

2.2.1 Design Rainfall Events

Figure 2 shows the box and whisker output of the model run. The model shows that the 1% AEP 4.5-hour storm temporal pattern 3 was the worst-case median storm. Therefore, this storm event was used within the hydraulic model. This particular storm event was selected as the worst-case scenario for further integration into the hydraulic model. The utilisation of this specific storm pattern ensures a comprehensive assessment of the system's response under conditions representing a high level of hydrological stress, thereby enhancing the model's ability to simulate and address extreme weather scenarios.

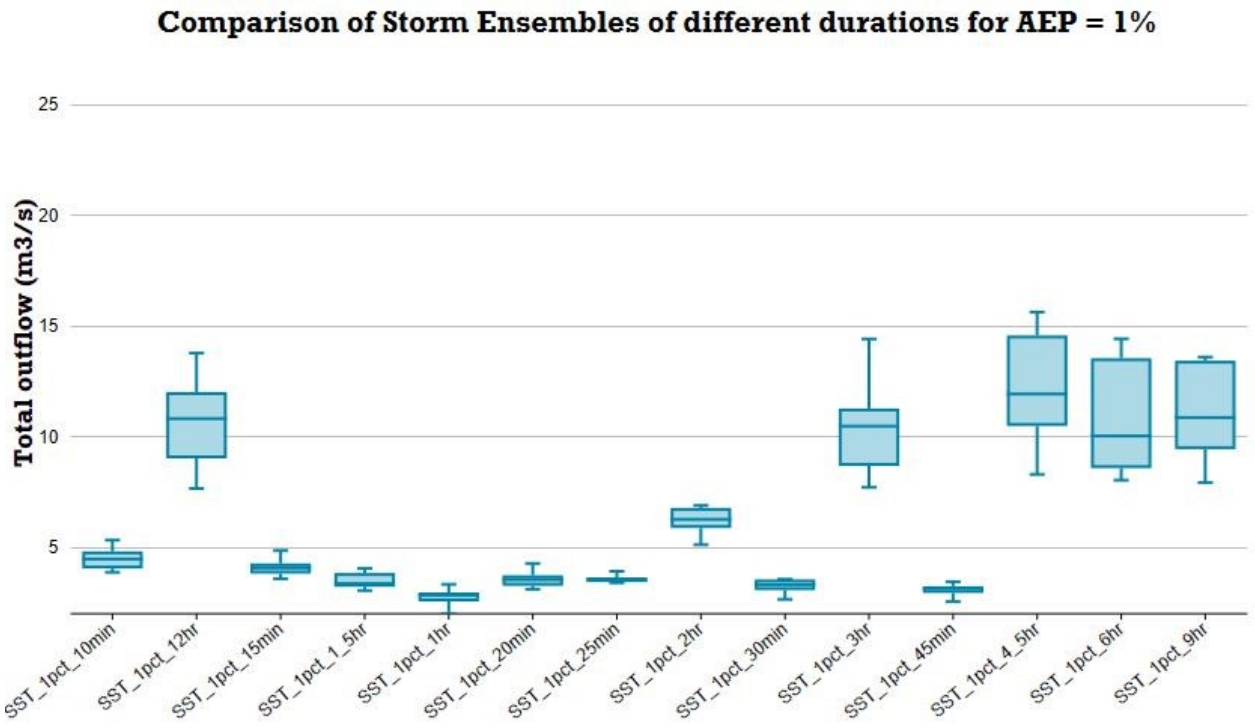


Figure 2. 1% AEP Flood Event Model, Box and Whisker Plot

2.2.2 Climate Change

As per ARR 2019 Guidelines, for an increase in rainfall due to climate change at 2100, it is recommended the use of RCP 8.5.Table 3 shows the RCP 8.5 increase compared to the revised increase of 14.6% suggested by Climate Futures Tasmania. The Glenorchy CBD 2018 flood study adopts 24% as the climate change factor and therefore, climate change increase of 24% was adopted in the model as a conservative estimate.

Table 3. Climate Change Increases

Catchment	CFT increase @ 2100	ARR 8.5 increase @ 2100	Glenorchy CBD 2018 Flood study
South East Tasmania	14.6%	16.3%	24%

2.2.3 Calibration/Validation

This catchment has no stream gauge to calibrate the model against a real-world storm event. Similarly, there is little historical information available, and limited available past flood analysis undertaken to validate against the flows obtained in the model. A Regional Flood Frequency Estimation model (RFFE) has been used to calibrate our rain on grid rainfall estimation. The RFFE values are listed in Table 4.

Table 4. Regional Flood Frequency Estimation model (RFFE) v/s Flussig Result.

AEP (%)	Lower Confidence Limit (5%) (m^3/s)	Upper Confidence Limit (95%) (m^3/s)	Discharge (m^3/s)	Flussig Discharge (m^3/s)
50	0.76	3.47	1.61	1.89
20	1.41	6.52	3.03	3.60
10	1.7	10.6	4.26	5.09
5	1.87	16.6	5.71	6.76
2	2.05	28.3	7.98	9.53
1	2.17	40.9	10.00	11.80

Input Data	
Date/Time	23/07/2024 10:22
Catchment Name	168a Abbotsfield
Latitude (Outlet)	-42.794
Longitude (Outlet)	147.247
Latitude (Centroid)	-42.825
Longitude (Centroid)	147.057
Catchment Area (km^2)	3.2
Distance to Nearest Gauged Catchment (km)	18.55
50% AEP 6 Hour Rainfall Intensity (mm/h)	4.548336
2% AEP 6 Hour Rainfall Intensity (mm/h)	9.879495
Rainfall Intensity Source (User/Auto)	Auto
Region	Tasmania
Region Version	RFFE Model 2016 v1
Region Source (User/Auto)	Auto
Shape Factor	5.29*
Interpolation Method	Natural Neighbour
Bias Correction Value	0.099

2.3 Hydraulics

A 1D-2D hydraulic model was created to determine the flood level through the target area.

2.3.1 Survey

The 2D surface model was taken from a combination of Greater Hobart LIDAR DEM 2013 to create a 1m cell size DEM. For the purposes of this report, 1m cells are enough to capture accurate flow paths. The DEM with hill shading can be seen below (Figure 3).

The site survey provided by client titled '1533101 - Contour and Detail Plan (3D) - 168A Abbotsfield Road, Claremont' has been incorporated into the revised Digital Elevation Model (DEM) for both the pre and post-development scenario. This mesh has been fused with the ELVIS DEM to establish the ground conditions for existing and post development conditions.

Figure 3. 1m DEM (Hill shade) of Lot Area

2.3.2 Key Stormwater Assets including pipes and pits

Pipes and pits were modelled as 1D underground network within the catchment model included identified culverts and discharge outlets. All upstream stormwater infrastructure was included within the model to provide insight into the capacity of the stormwater system. Where data was missing, this was inferred from surrounding data and where invert levels were missing, a 600 mm cover was applied.

2.3.3 Roads

Roads often form the basis for overland flow in high frequency events, however the kerb and channel are not always picked up by DEM surface. To correct for the drainage lines, mesh polygons were used to delineate road corridors with the roads being incorporated a z-line along the gutter to ensure the kerb invert is represent in the mesh.

In our Digital Elevation Model (DEM), a "z-line" refers to a line representing a constant elevation or contour line. These lines connect the existing kerb points of equal elevation on the terrain surface, allowing for visualisation of the terrain's shape and elevation changes.

2.3.4 Buildings

Specifically, residential houses and commercial buildings were integrated into the DEM by elevating the corresponding grid cells representing these structures by a standardised height of 0.3 meters above the natural ground surface. Subsequently, the re-sampled grids were utilised to establish the Infoworks ICM model, thus forming a foundational framework for the subsequent analysis and simulation of flood dynamics.

This method allows for flow through the building if the flood levels/ pressure become great enough. The aim is to mimic flow through passageways such as doors, windows, and hallways.

2.3.5 Boundary Conditions

Infoworks ICM operates as a single-use software, streamlining the hydrology and hydraulic modelling processes within a unified framework. This unique feature eliminates the necessity for separate inflow boundary conditions, as the hydrology model seamlessly integrates with the hydraulic model through a 1D or 2D link.

According to LISTmap, Roseneath Rivulet, a minor stream, is located 40 meters downstream of the development site with an approximate height difference of 4 meters. Despite its close proximity to the development site, the immediate catchment originating from 64 Mount Faulkner Road has a greater impact on the development due to its steep grade and relatively urbanized nature.

The rain on grid model originated from Mount Faulkner to the west with the extents stretching further downstream of the site to the Brooker Highway. Roseneath Rivulet eventually discharges into Derwent River further downstream at Austins Ferry.

2.3.6 Structures

In the process of crafting a two-dimensional grid to depict the ground surface of the floodplain, we initiated by re-sampling high-resolution LiDAR data to generate a digital elevation model (DEM) through the utilisation of GIS software.

Within this procedure, the attention was directed towards identifying and incorporating pertinent features such as residential structures, commercial buildings, walls, and roadways. Ensuring the comprehensive inclusion of these features within the re-sampled DEM was of utmost importance.

2.3.7 Roughness (Manning's n)

The model grid's roughness and equivalent Manning's n values were derived from land use data. Table 5 shows Manning's values used in the model. Values for this layer were derived from the ARR 2019 Guidelines. These parameters have proven effective in previous flood mapping projects undertaken in Tasmania.

Table 5. Manning's Coefficients (ARR 2019)

Land type	Roughness, Manning's N	Equivalent Manning's 'n' (1/Roughness)
Built up areas	8	0.125
Open space	28	0.025
Waterways	33	0.029
Roads	55	0.013
Houses/ Buildings Roof	56	0.010

2.3.8 Walls

All significant fences and retaining structures were included as 2D linear wall structures within the 2D model. Fences were modelled 300 mm above the ground level. In the post development scenario, the fences surrounding the development were modelling 300 mm above the ground level as shown in the plans titled 'U249 DA PLANS - A – 110724'.

2.3.9 Piers

As shown in the plans, the 12 proposed units are to be constructed on piers. Therefore, in the post-development scenario of the model, the piers were modelled with a conservative height of 300 mm.

2.4 Development Runoff

Stormwater runoff from the development site has been assessed under pre- and post-development models to determine the potential impact the development at 168a Abbotsfield Road, Claremont has

on the immediate local flows. As per planning guidelines it is a requirement that this does not have a negative impact from pre to post development. Site Characteristics for the pre- and post-development model are summarised in Table 6.

Table 6. Site Characteristics

	Pre-Development		Post-Development	
Land Use	**Area (m²)**	**% of total**	**Area (m²)**	**% of total**
Total Impervious	2596	97	1169	44
Total Pervious	80	3	1507	56

3. Model Results

The result of 1% AEP + CC were run through the pre-development and post-development model scenarios to compare the changes to flooding onsite and to surrounding properties.

3.1 Pre-development Scenario

It can be seen from the pre-development model runs (Figure 4), that there is a shallow overland flood path flowing from the southern lot boundary, through the development site and towards Roseneath Rivulet. The flood depth in the pre-development scenario within the lot at the western corner is 0.19 m observed adjacent to the existing building. Flood depths of 0.27 m are observed at the northern lot boundary at the cross-sectional results line in the pre-development scenario.

There is a 1.5 m drainage easement along the western lot boundary which channelises some of the flow path. There is a flow path along the minor stream Roseneath Rivulet approximately 40 m north downstream of the development which has no significant impact on the development site.

3.2 Post-Development Scenario

Figure 5 shows the effect that the inclusion of the proposed unit development including the concrete areas and the fences has on the overland flood flow.

To facilitate the free passage of overland flow, the fences along the western lot boundary are recommended to be constructed with a **minimum clearance of 150 mm from the natural ground level**. This clearance ensures that the post-development overland flow path remains consistent with the pre-development conditions.

Additionally, the proposed units will be constructed on **piers**, which will further help in maintaining the existing overland flow path.

The maximum depth at the cross-sectional results line at the northern lot boundary is 0.29 m. The maximum depths within the extents of unit dwellings within the lot are up to 0.1 m, however as they units are constructed on piers, they are free from inundation.

It can be deemed that the proposed units including its access are free from inundation.

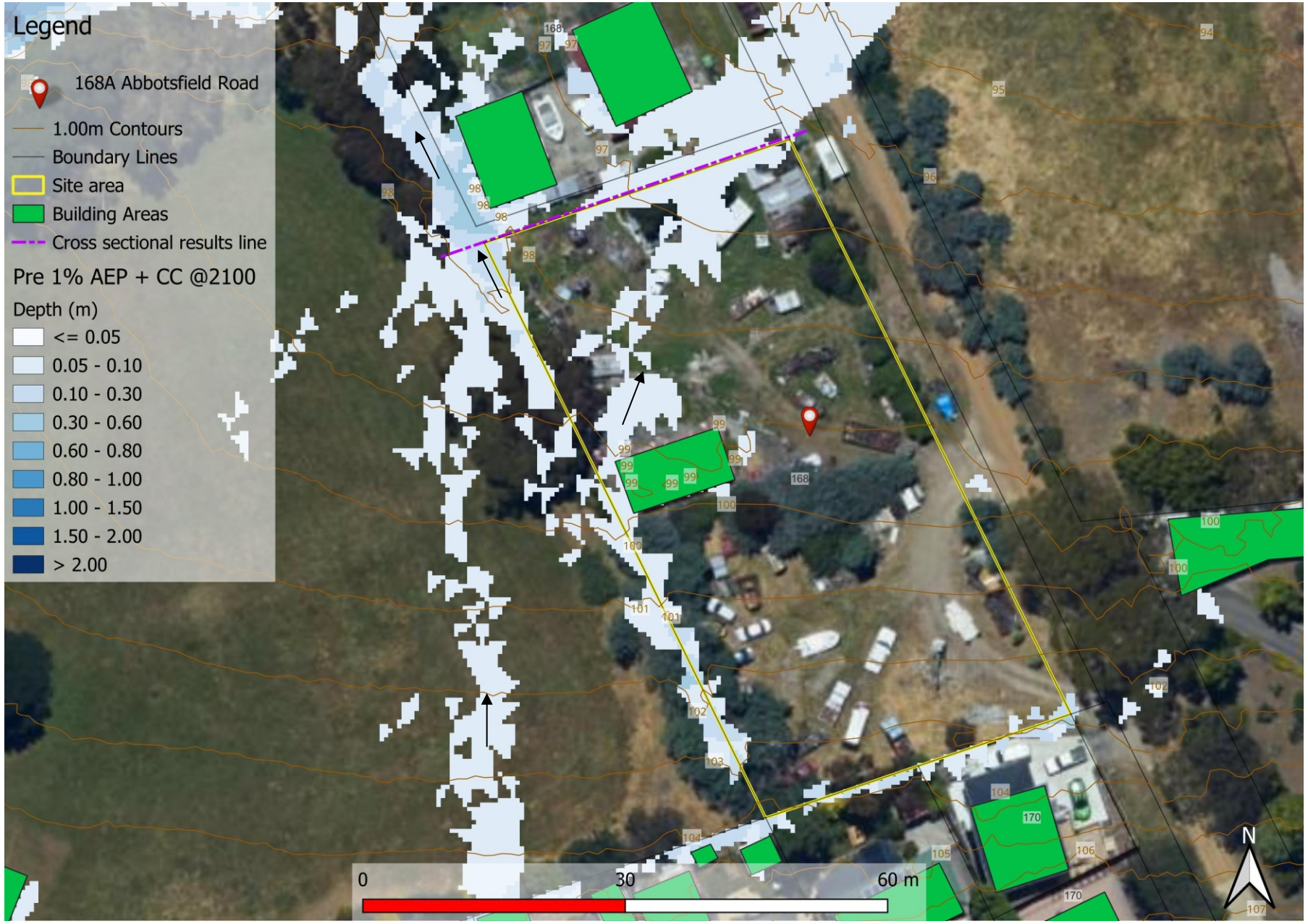


Figure 4. Pre-Development 1% AEP + CC Depth

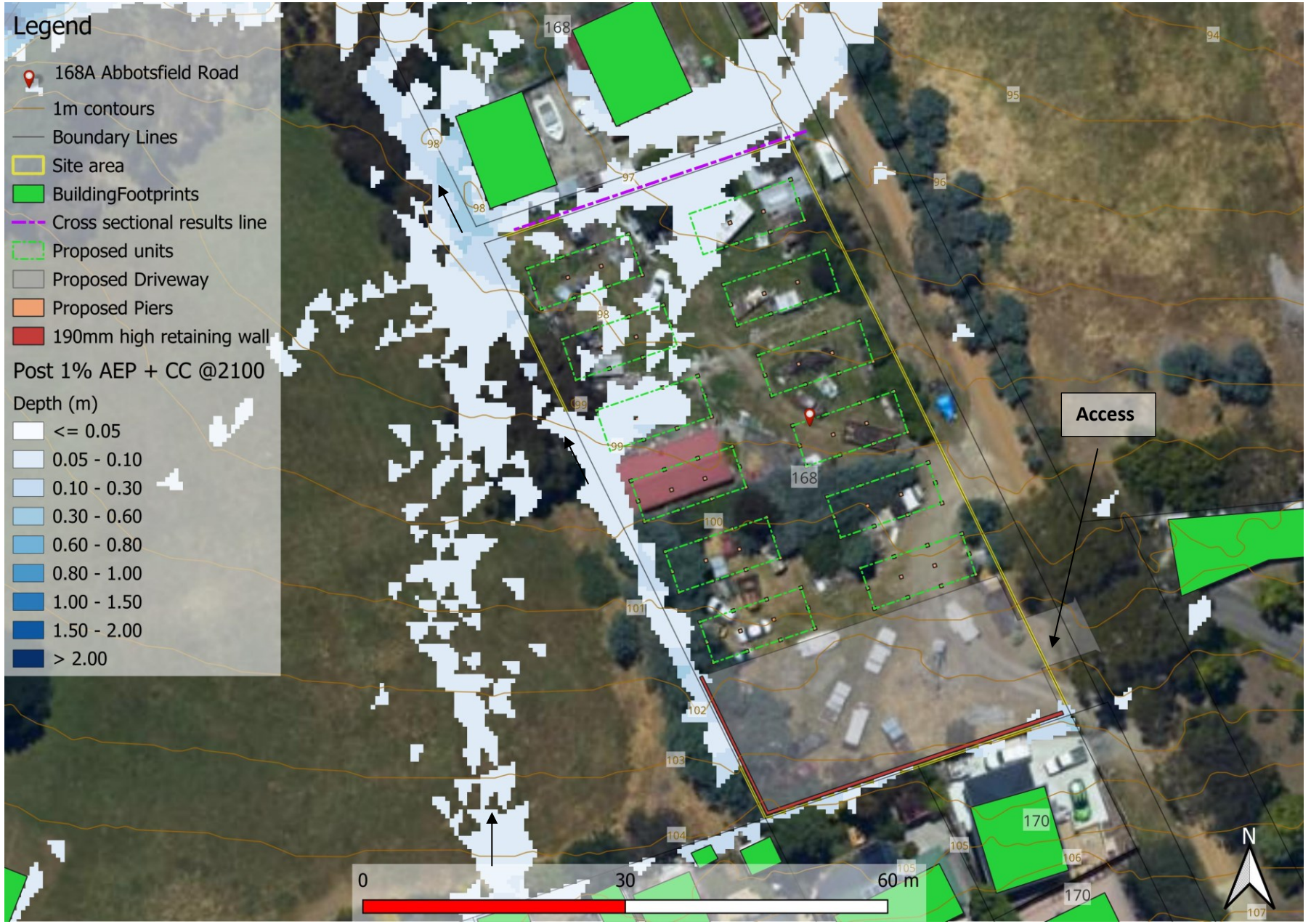


Figure 5. Post-Development 1% AEP + CC including Depth

3.3 Displacement of Overland Flow on Third Party Property

Post-development flows in Figure 5 show that when compared against pre-development in Figure 4, there is no increase in flood depths on adjacent properties to the north of the development lot, with the overland flow continuing towards the natural overland flow path to the northern boundary of the lot.

Therefore, it can be stated that the development does not have any measurable effect on third party property.

3.4 Development Effects on Flooding

The proposed development is within the natural overland flow path. However, the proposed development built on piers has no adverse effect on flooding during a 1% AEP storm event, both within the lot and on surrounding areas. Velocities and depths in the post-development scenario are within the lowest hazard band, and therefore the post development models show that there is no increase to the risk rating on surrounding properties or infrastructure.

3.5 Development Effects on Stormwater Discharge

Figure 6 below shows the discharge hydrograph from the property boundary for the overland flow through the development area. The graph was captured in the model for both pre- and post-development runs and combined in graph format to demonstrate the change in net discharge. It demonstrates the discharge increasing by 0.05 m^3/s from 0.97 m^3/s to 1.02 m^3/s from the pre-development to post-development scenarios, while velocity shows an increase of 0.08 m/s from 0.88 m/s to 0.96 m/s.

Given that both the discharge and velocity in the pre-development scenario are low, the slight increases observed are could also be due to model sensitivity and do not significantly impact the overall discharge from the lot following development.

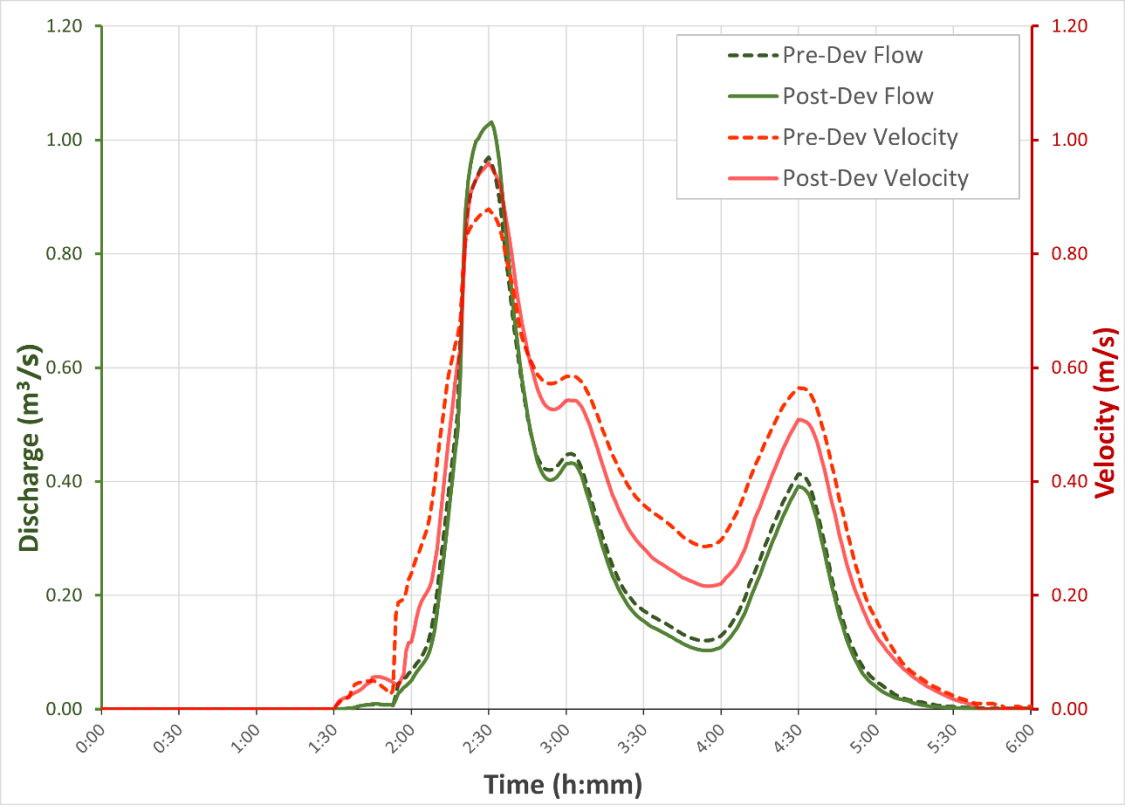


Figure 6. Pre and Post development net discharge and velocity 1% AEP + CC

3.6 New Habitable Building

To meet the performance criteria of the Building Regulations S.54, the construction of a new habitable building is required to have a habitable floor level > 300 mm above the 1% AEP + CC flood level. The new development at 168a Abbotsfield Road, Claremont must meet this regulation in respect to the habitable dwelling areas. (The floor level >1% AEP + CC flood level + 300mm does not apply for non-habitable areas).

As the proposed entrances to all the units within the development are free from inundation, this performance criteria does not apply. Furthermore, the unit dwellings are built on piers above the natural ground level which provides an additional layer of safety.

3.7 Model Summary

Table 7. Pre-development and post-development at the cross-sectional line

	Pre-development	Post-development	Net Change
Depth (m)	0.27	0.29	+0.02
Velocity (m/s)	0.88	0.96	+0.08
Discharge (m^3/s)	0.97	1.02	+0.05

4. Flood Hazard

Under existing conditions prior to development, the proposed location of the building is subject to be inundated to < 0.27 m flood depth and < 0.88 m/s velocity. This places the hazard rating as adopted by Australian Flood Resilience and Design Handbook as a maximum H1 – *generally safe for people and vehicles and buildings* as shown in Appendix A – Hazard maps.

In both pre and post development scenarios, there is a minor localised area of H2 hazard rating within the western lot boundary of relatively higher depths of 0.33 m. This is due to the 1.5 m drainage easement which is located away from any habitable area and lot access. Apart from this small, localised area, majority of the lot is at the lowest hazard rating H1-*generally safe for people, vehicles, and buildings*.

The post-development scenario sees the depth at the lot boundary slightly increasing to 0.29 m from the pre-development level and the velocity showing an increase to 0.96 m/s, which has no effect on the hazard rating that remains within the hazard band of H1. The proposed driveway and access to units are not subject to hazard ratings as they are free from inundation.

As this study does not extend to the public access roads we cannot comment on the accessibility to the site, only within the site. Therefore, this report would advise that occupants and visitors remain inside in the event of a flood unless instructed by emergency services. A summary of the hazard ratings is shown below.

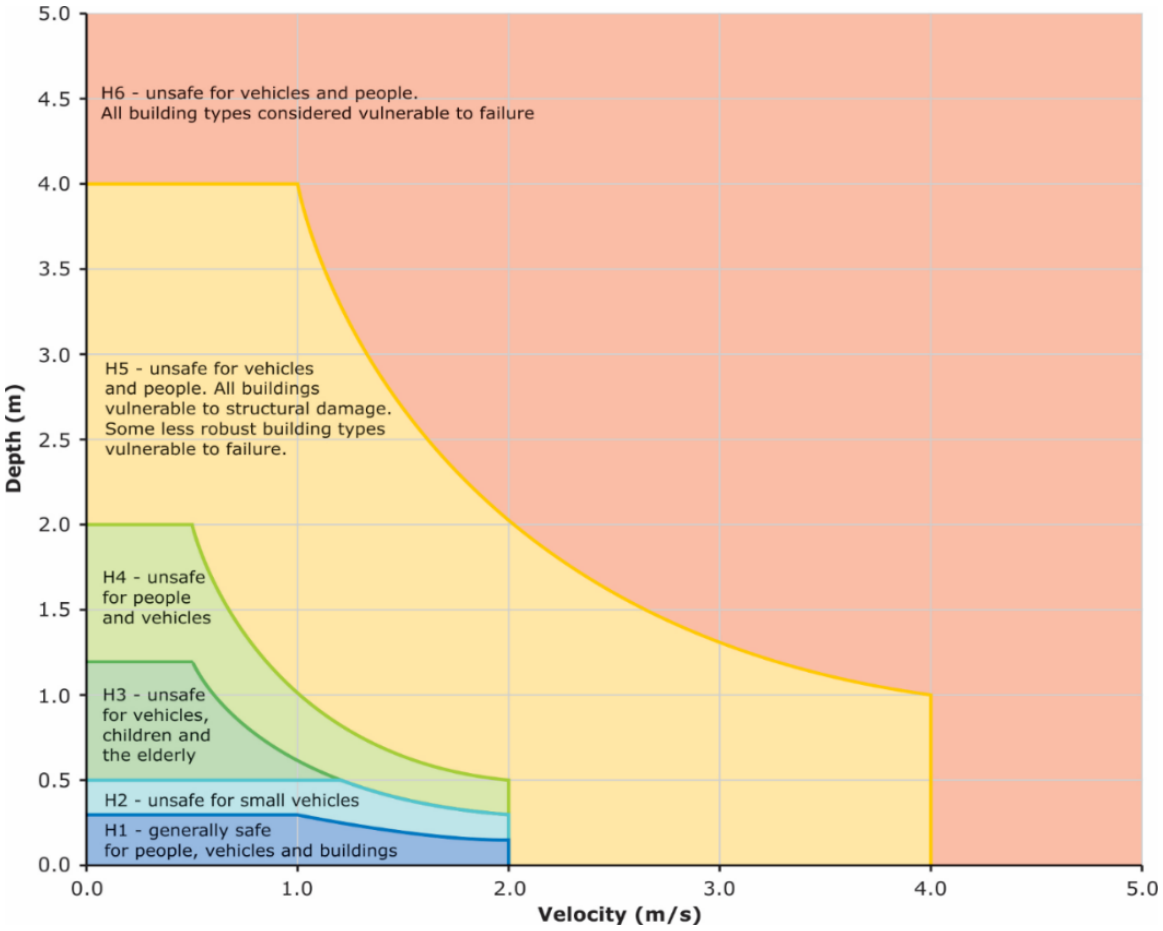


Figure 7. Hazard Categories Australian Disaster and Resilience Handbook

4.1 Tolerable Risk

The lot at 168a Abbotsfield Road, Claremont is susceptible to a shallow, slow-moving flood plain flow, with the majority of the immediate surrounding region classified low (H1) hazard rating in the 1% AEP + climate change event. The hazard remains at H1 in the post development scenario which means that it will not pose any risk to occupants or structures during a 1% AEP storm event.

Even at minor velocity and depths during a storm event, erosion and debris movement nevertheless pose a threat. If the recommendations in this report are implemented, the proposed structure, which is intended to be class 1a structures with a 50-year asset life (BCA2022), can achieve a tolerable risk of flooding over its asset life.

Table 8. Tasmanian Planning Scheme – Glenorchy summary C12.5.1

C12.5.1 Uses within a flood prone hazard area			
Objectives: That a habitable building can achieve and maintain a tolerable risk from flood			
Performance Criteria			
P1.1		**P1.1**	
A change of use that, converts a non-habitable building to a habitable building, or a use involving a new habitable room within an existing building, within a flood-prone hazard area must have a tolerable risk, having regard to:		Response from flood report	
(a)	the location of the building;	(a)	Proposed 12-unit development including proposed concrete path and proposed driveway located within a shallow overland flow path.
(b)	the advice in a flood hazard report;	(b)	Assuming recommendations of this report are implemented, no additional flood protection measures required for the life expectancy of the building.
(c)	any advice from a state authority, regulated entity or a council;	(c)	N/A
P1.2		**P1.2**	
A flood hazard report also demonstrates that:		Response from flood report	
(a)	any increase in the level of risk from flood does not require any specific hazard reduction or protection measures;	(a)	No increase in level of risk from pre-development scenario.
(b)	the use can achieve and maintain a tolerable risk from a 1% annual exceedance probability flood event for the intended life of the use without requiring any flood protection measures	(b)	Maximum hazard rating at the proposed development is H1 in both the pre-development and post-development scenarios. The minor localised area of H2 observed in both the scenarios is due to the presence of a drain, adjacent to the western lot boundary which doesn't impact the lot and the development.

Table 9. Tasmanian Planning Scheme – Glenorchy summary C12.6.1

C12.6.1 Building and works within a flood prone area			
Objective: (a) building and works within a flood-prone hazard area can achieve and maintain a tolerable risk from flood; and, (b) buildings and works do not increase the risk from flood to adjacent land and public infrastructure.			
Performance Criteria			
P1.1		**P1.1**	
Buildings and works within a flood-prone hazard area must achieve and maintain a tolerable risk from a flood, having regard to:		Response from flood report	
(a)	the type, form, scale and intended duration of the development;	(a)	Proposed 12-unit development constructed on piers including proposed concrete path and proposed driveway located within a shallow overland flow path.
(b)	whether any increase in the level of risk from flood requires any specific hazard reduction or protection measures;	(b)	Assuming recommendations of this report are implemented, no additional flood protection measures required for the life expectancy of a habitable building.
(c)	any advice from a State authority, regulated entity or a council; and	(c)	N/A
(d)	the advice contained in a flood hazard report.	(d)	Flood report and recommendations provided within.
Performance Criteria			
P1.2		**P1.2**	
A flood hazard report also demonstrates that the building and works:		Response from Flood Report	
(a)	do not cause or contribute to flood on the site, on adjacent land or public infrastructure; and	(a)	Does not increase flooding extents and depths within the site, on adjacent land or public infrastructure.
(b)	can achieve and maintain a tolerable risk from a 1% annual exceedance probability flood event for the intended life of the use without requiring any flood protection measures.	(b)	Assuming recommendations of this report the proposed site and development can achieve a tolerable risk to the 1% AEP storm event for the life expectancy of the building.

5. Conclusion

The Flood Hazard Report for 168a Abbotsfield Road, Claremont development site has reviewed the potential development flood scenario.

The following conclusions were derived in this report:

1. A comparison of the post-development peak flows for the 1% AEP at 2100 were undertaken against C12.0 of the Tasmanian Planning Scheme – Glenorchy, Flood Prone Areas code.
2. A slight increase of 0.02 m in depth at the northern property boundary at the cross-sectional result line.
3. Peak discharge sees an increase of 0.05 m^3/s from both pre-development to post-development riverine flood scenario.
4. Velocity shows a slight increase of 0.08 m/s between pre- and post-development riverine flood scenarios.
5. Hazard from flooding within the lot remain at the majority category of H1 for both pre and post development riverine scenarios, including on neighbouring properties. Hazard rating of H2 is observed in a small, localised area but does not increase in the post-development scenario.

6. Recommendations

Flüssig Engineers therefore recommends the following engineering design be adopted for the development and future use to ensure the works meets the Inundation Code:

1. The proposed unit dwellings must be constructed on piers to prevent direct contact with floodwaters and to allow for the unobstructed passage of overland flow beneath the structure. This design reduces the risk of water damage and maintains the natural flow path.
2. Pailing fences on the western lot boundary to have a minimum clearance of 150 mm to allow the natural overland flow path through.
3. Proposed structures, located in the inundation area, are to be designed to resist flood forces including debris.
4. No additional solid structures be constructed around the property without further flood assessment.
5. Future use of lot areas to be limited to areas deemed safe under the ARR Disaster manual categories.
6. All future proposed structures within the flood extent not shown within this report will require a separate report addressing their impacts.

Under the requirements of this Flood Hazard Report, the proposed development will meet current acceptable solutions and performance criteria under the Tasmanian Planning Scheme 2021-Glenorchy.

7. Limitations

Flüssig Engineers were engaged by **Cunic Homes** on behalf of the developer, for the purpose of a site-specific Flood Hazard Report for 168a Abbotsfield Road, Claremont as per C12.0 of the Tasmanian Planning Scheme – Glenorchy 2021. This study is deemed suitable for purpose at the time of undertaking the study. If the conditions of the site should change, the report will need to be reviewed against all changes.

This report is to be used in full and may not be used in part to support any other objective other than what has been outlined within, unless specific written approval to do otherwise is granted by Flüssig Engineers.

Flüssig Engineers accepts no responsibility for the accuracy of third-party documents supplied for the purpose of this Flood Hazard Report.

8. References

- Australian Disaster Resilience Guideline 7-3: Technical flood risk management guideline: Flood hazard, 2014, Australian Institute for Disaster Resilience CC BY-NC
- Ball J, Babister M, Nathan R, Weeks W, Weinmann E, Retallick M, Testoni I, (Editors), 2019, Australian Rainfall and Runoff: A Guide to Flood Estimation, Commonwealth of Australia
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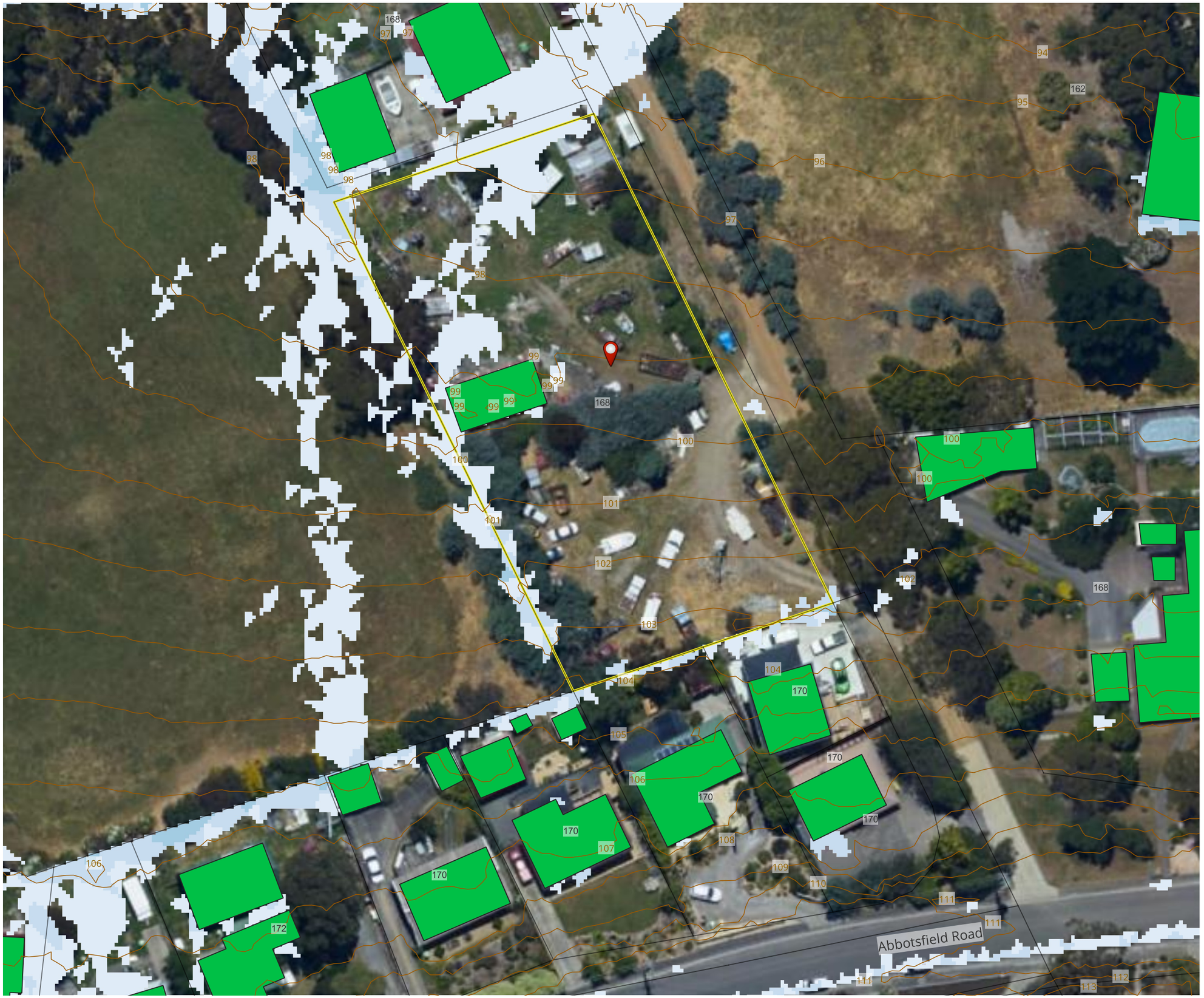
Appendices

Appendix A Flood Study Maps

PRE 1% AEP + CC @2100



Legend

- 168A Abbotsfield Road
- 1.00m Contours
- Boundary Lines
- Building Areas
- Site area

Pre 1% AEP + CC @2100

Depth (m)

- <= 0.05
- 0.05 - 0.10
- 0.10 - 0.30
- 0.30 - 0.60
- 0.60 - 0.80
- 0.80 - 1.00
- 1.00 - 1.50
- 1.50 - 2.00
- > 2.00

0 10 20 m

meters

N

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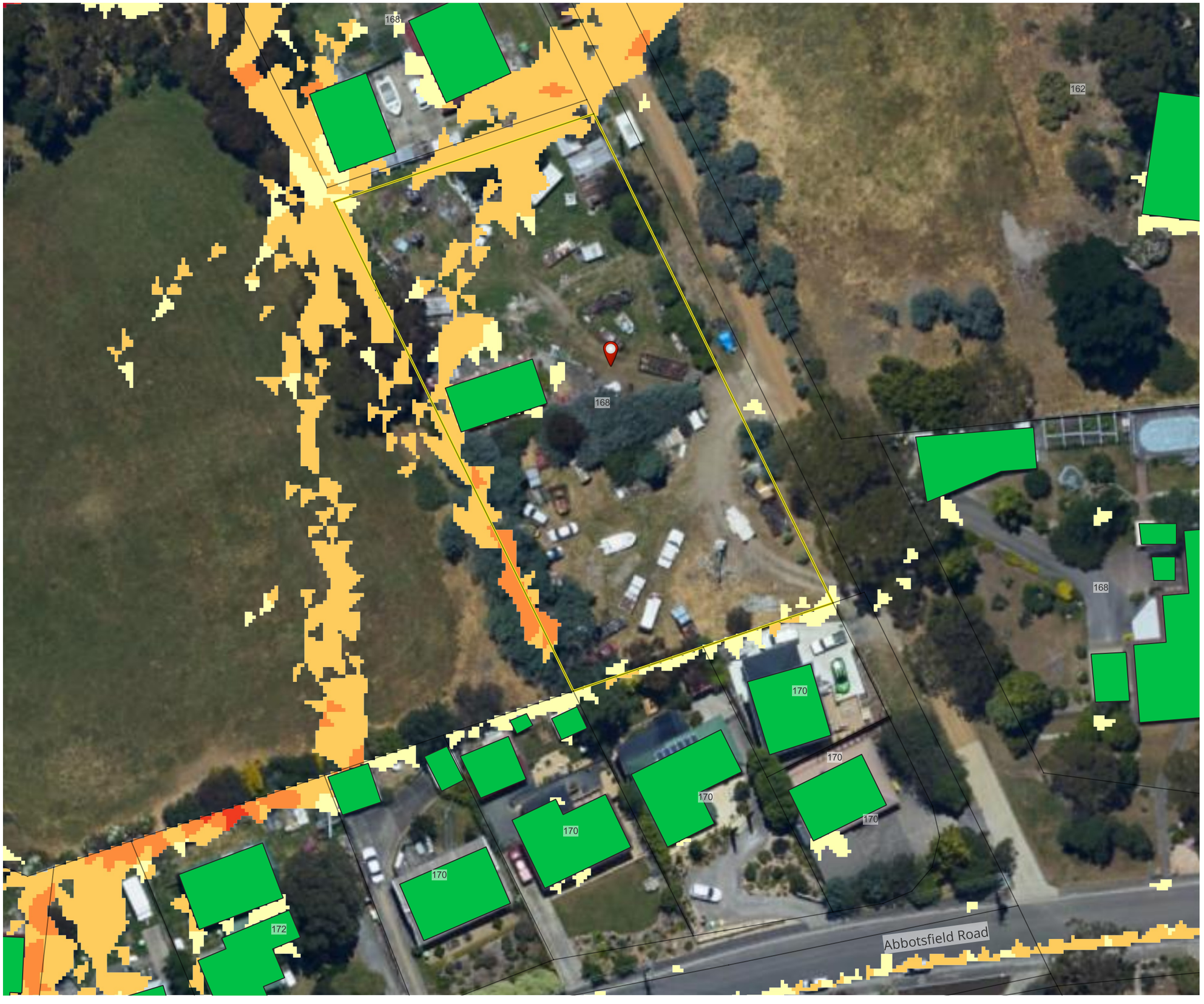
25-07-2024 - Flussig Engineers Map CRS: GDA94 / MGA zone 55 EPSG:28355

Document Set ID: 3604455
Version: 1, Version Date: 26/08/2025

PRE 1% AEP + CC @2100



Legend

168A Abbotsfield Road

Boundary Lines

Building Areas

Site area

Pre 1% AEP + CC @2100

Velocity (m/s)

- <= 0.50
- 0.50 - 1.00
- 1.00 - 1.50
- 1.50 - 2.00
- > 2.00

0 10 20 m

meters

N

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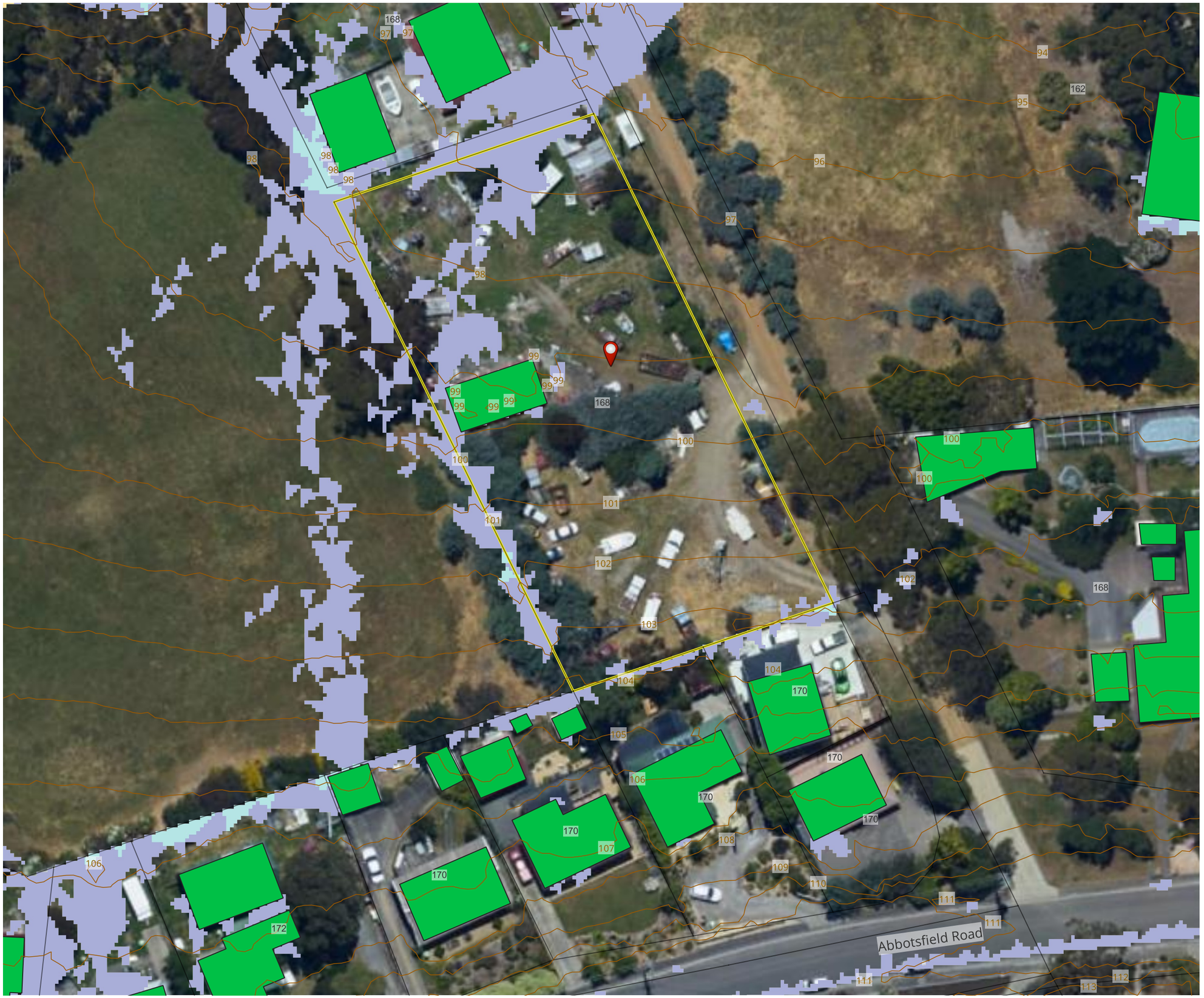
PRE 1% AEP + CC @2100



Legend

168A Abbotsfield Road

Boundary Lines

Building Areas

Site area

Pre 1% AEP + CC @2100

Hazard

H1

H2

H3

H4

H5

H6

0 10 20 m

meters

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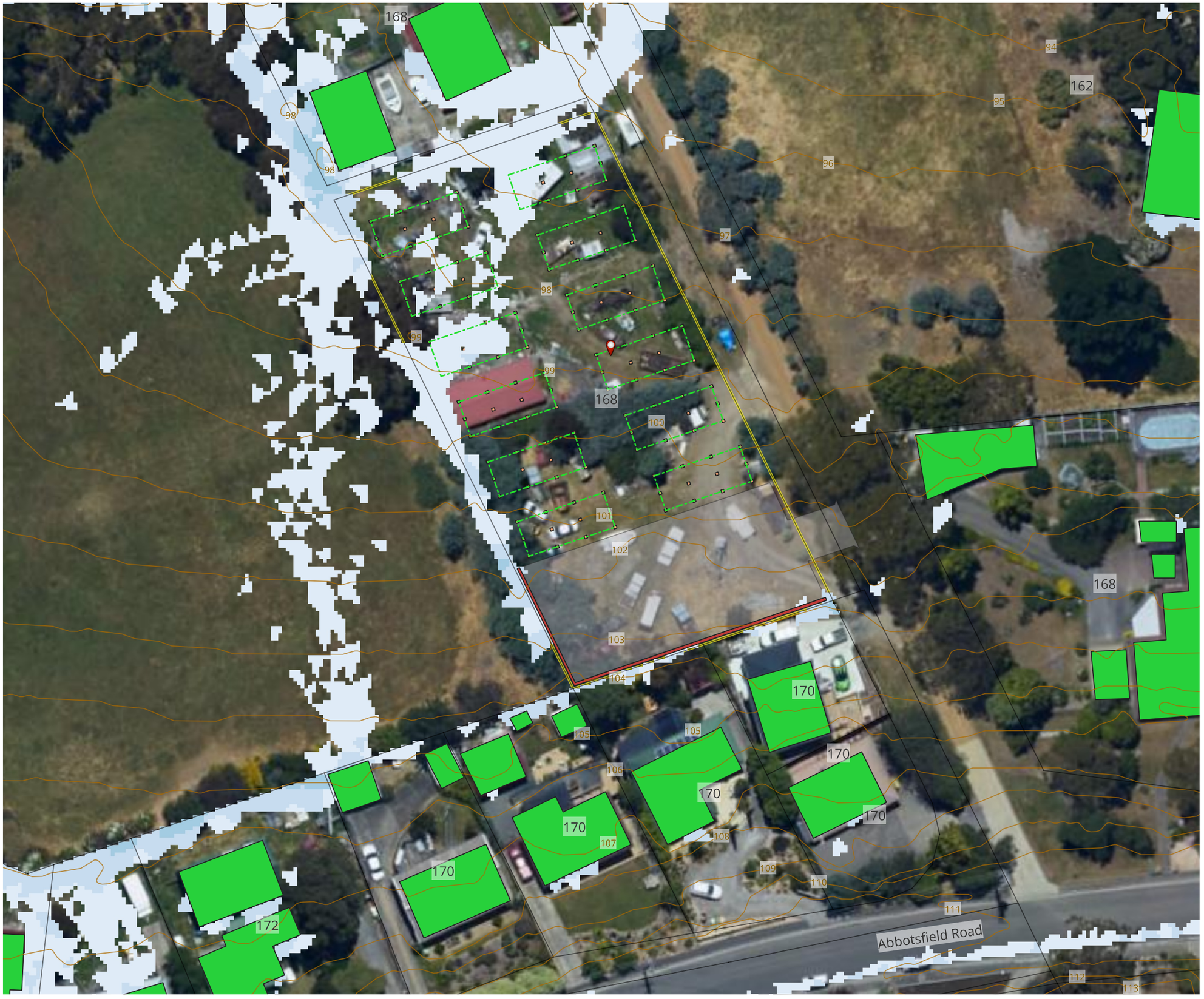
POST 1% AEP + CC @2100



Legend

- 168A Abbotsfield Road
- 1m contours
- Boundary Lines
- BuildingFootprints
- Site area
- 190mm high retaining wall
- Proposed Driveway
- Proposed Piers
- Proposed units

Post 1% AEP + CC @2100

Depth (m)

- <= 0.05
- 0.05 - 0.10
- 0.10 - 0.30
- 0.30 - 0.60
- 0.60 - 0.80
- 0.80 - 1.00
- 1.00 - 1.50
- 1.50 - 2.00
- > 2.00

0 10 20 m

meters

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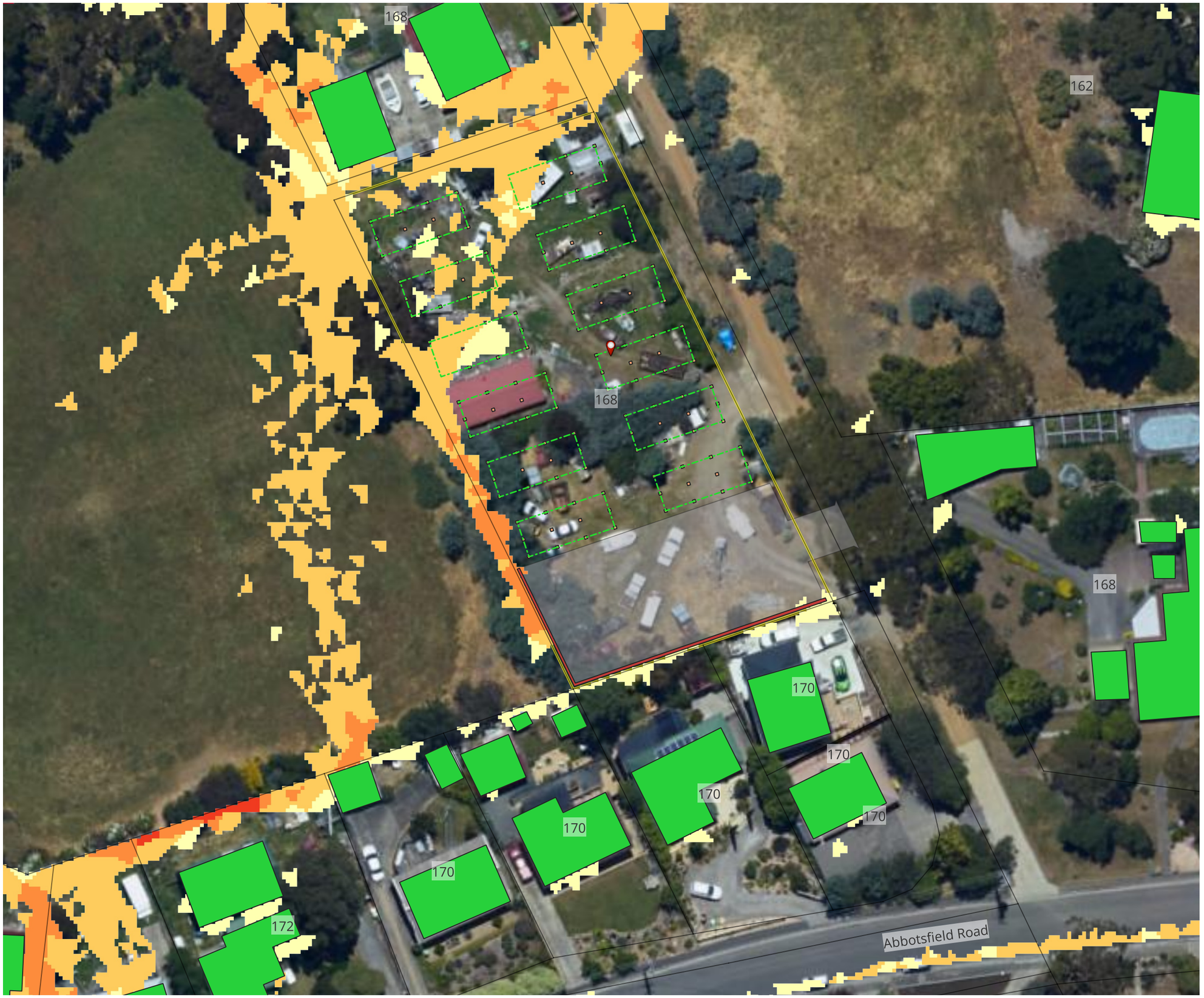
24-01-2025 above Map CRS: GDA94 / MGA zone 55 EPSG:28355

POST 1% AEP + CC @2100



Legend

- 168A Abbotsfield Road
- Boundary Lines
- BuildingFootprints
- Site area
- 190mm high retaining wall
- Proposed Driveway
- Proposed Piers
- Proposed units

Post 1% AEP + CC @2100

Velocity (m/s)

- <= 0.50
- 0.50 - 1.00
- 1.00 - 1.50
- 1.50 - 2.00
- > 2.00

0 10 20 m

meters

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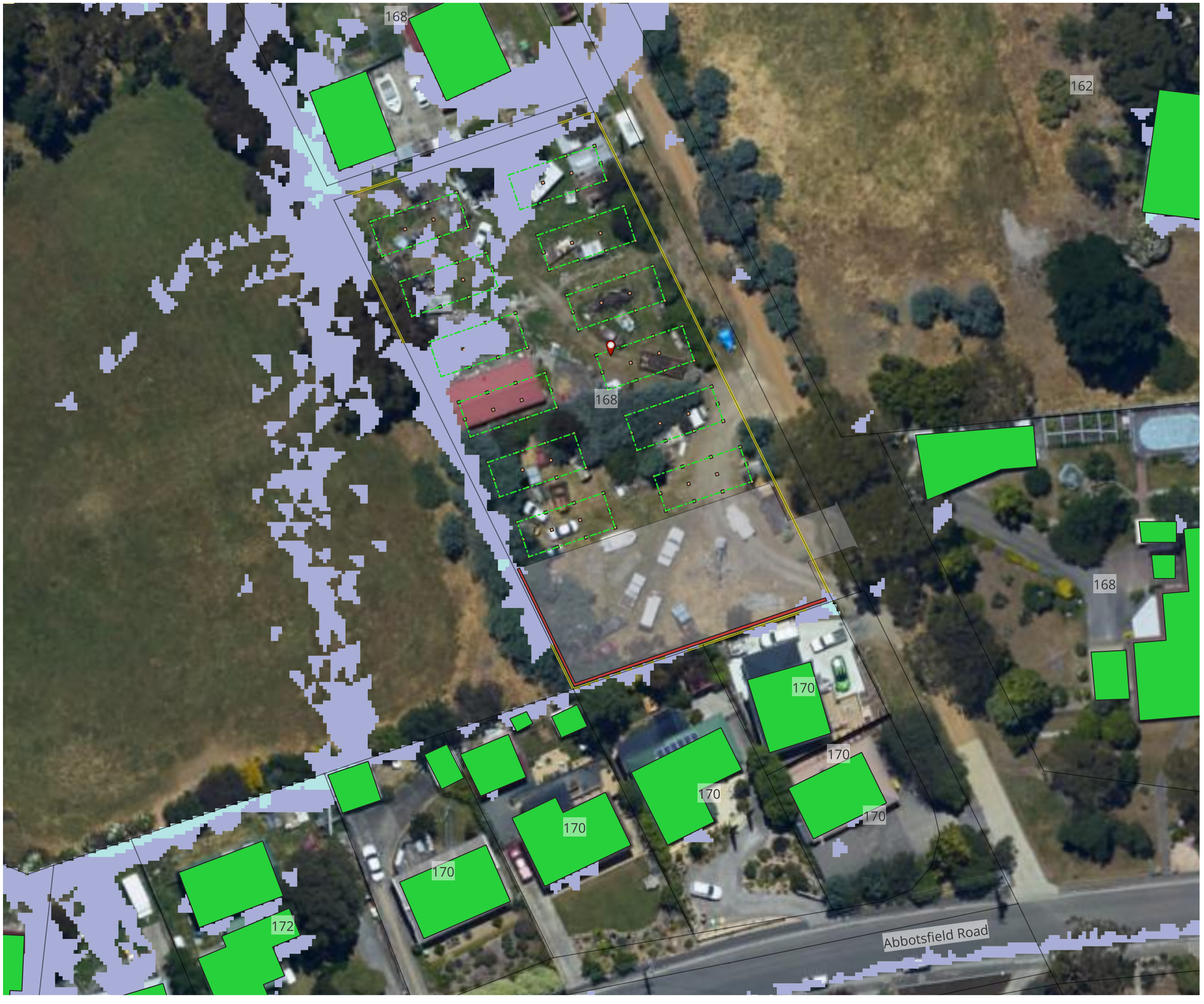
Map CRS: GDA94 / MGA zone 55

EPSG:28355

POST 1% AEP + CC @2100



Legend

- 168A Abbotsfield Road
- Boundary Lines
- BuildingFootprints
- Site area
- 190mm high retaining wall
- Proposed Driveway
- Proposed Piers
- Proposed units

Post 1% AEP + CC @2100

Hazard

- H1
- H2
- H3
- H4
- H5
- H6

0 10 20 m

meters

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Map CRS: GDA94 / MGA zone 55

EPSG:28355

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COLLECTIVE CONSULTING

INFRASTRUCTURE SERVICES REPORT

FEBRUARY 2025

PREPARED FOR

CUNIC HOMES

241043 – ISR02 ISSUE 02 VERSION 02

DOCUMENT TRANSMITTAL

RECORD OF ISSUE

Issue	Reason	Version	Date	Prepared By	Approved By
01	Development Application	01	27/09/2024	OWM	JTA
02	Revised Development Application	02	18/02/2025	OWM	JTA

RECORD OF ISSUE

Company	Name & Address	Contact	Copies
Cunic Homes	Domonee van Heerden 209 Elizabeth Street Hobart TAS 7000	Email: dom@cunic.com.au	1

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1. INTRODUCTION

This infrastructure services report has been prepared to provide supplementary information to the planning authority for the purpose of assessing and approving the planned development.

The proposal is a 12 unit residential development on a single, undeveloped title (CT: 61276/27). The lot is accessed through a Right of Way over 166a Abbotsfield Road (CT: 181572/1)

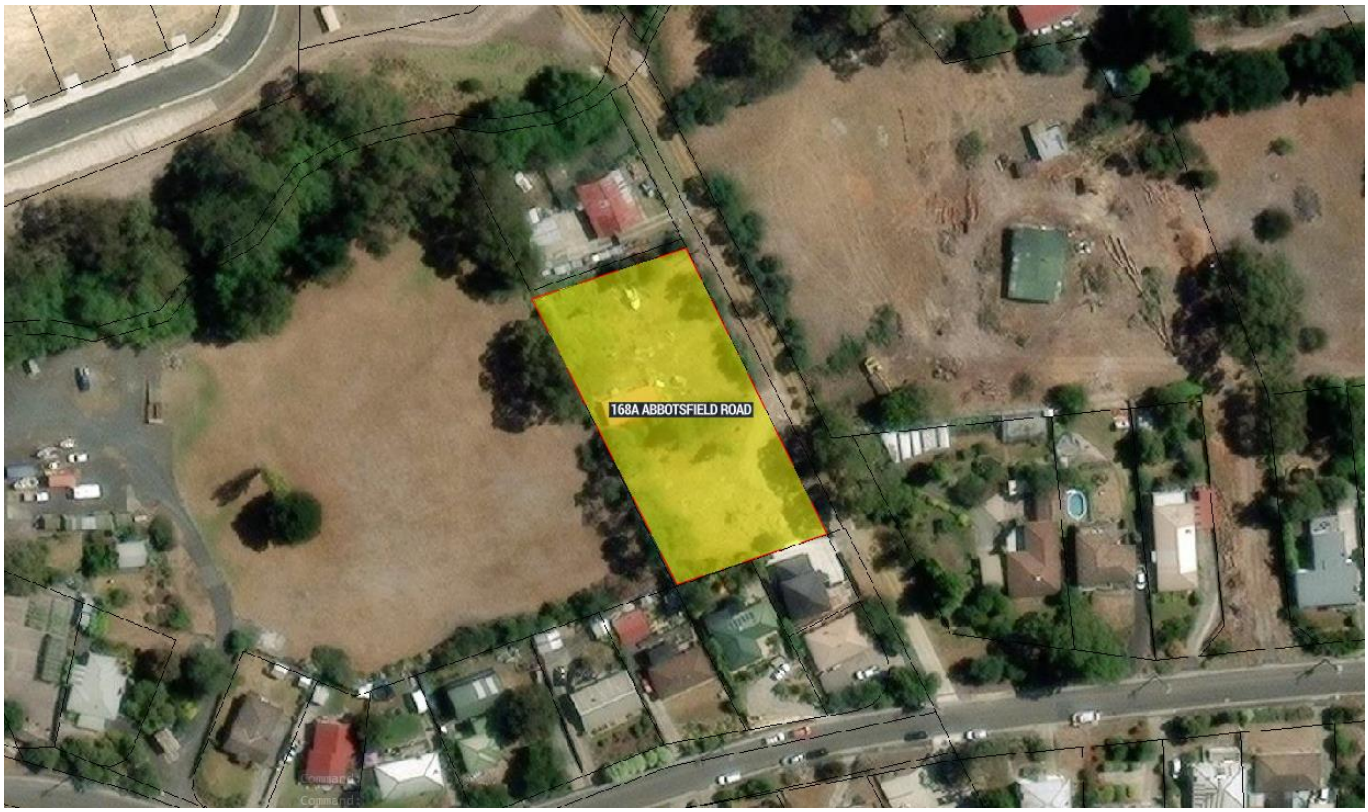

Figure 1 - 168a Abbotsfield Title

This report details the demand for water and sewer infrastructure services, stormwater management for the 12 units.

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2. WATER INFRASTRUCTURE

2.1. Water Introduction

This report should be read in conjunction with the approval drawings prepared by Collective Consulting project number, 241043 (Appendix A).

The lot is currently serviced by a DN20 water meter (Asset ID: A3416818). It is proposed to decommission this asset and install a new meter to service the new development. Internal sub-meters will be installed for each unit, however these will be managed by the body corporate and will be installed internally.

An existing DN100 Ductile Iron, Cement Lined TasWater water main (Asset ID: A323275) is present in the Right of Way and a new lot connection for the residential site will be serviced from this main.

2.2. Demand

TasWater's supplement to Water Supply Code of Australia (WSA-03) specifies two methods to determine the required flow rate of a development depending on the number of equivalent tenements.

Appendix A of TasWater's supplement to WSA-03 provides equivalent tenements (ET) for various residential, commercial and industrial developments. The supplement suggests 0.8ET's per unit therefore the design ET for this development is 9.6 ET.

For developments under 100 ET, the probable simultaneous Demand (PSD) method, outlined in AS3500.1 Plumbing and Drainage – Part 1: Water Services, should be used.

The probable simultaneous flow rate for varying ET's (or dwellings) can be found in Table 3.2.3 of AS3500.1. For developments of 10 ET, a minimum flow rate of 1.74 L/s should be used.

2.3. Sizing

Based on a conservative pressure head drop of 4m from the existing DN100 main to the first branch line, which accounts for pressure losses due to junctions, the meter and rise in water line. The 4m pressure head drop section of table C.1 of AS3500.1 has been used with an index length of 80m to the first junction, a DN40 meter will be sufficient.

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3. SEWER INFRASTRUCTURE

3.1. Sewer Introduction

This report should be read in conjunction with the approval drawings prepared by Collective Consulting project number 241043 (Appendix A).

The site does not currently service by a sewer connection. It is proposed to connect into the existing TasWater sewer manhole located near the southeastern corner of the property.

3.2. Demand

Based on each unit equating to one equivalent tenement (ET) each, the ET for the development is 12.

From Water Services Association of Australia – Sewerage Code of Australia (WSA 02)

- The average dry weather flow (ADWF) = 0.0021 x EP and
- Peak dry weather flow (PDWF) = d x ADWF
- EP = Equivalent population = ET x 3.5 = 42

3.3. Sizing

Appendix C of WSA02 was used to calculate a flow rate of 0.712 l/s and utilising AS3500, a pipe size of DN100 is sufficient for the entirety of this development.

3.4. Pump Station

To service the lot, a private pump station will be required with a rising main. The rising main will discharge into a private manhole, before gravity draining to the new lot connection. The pump station will be designed to accommodate a flow rate of 0.712 l/s with 6m of head and a pipe length of 80 metres.

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4. STORMWATER MANAGEMENT

4.1. Stormwater Introduction

The stormwater drainage system on the development application documents produced by Collective Consulting, project number 241043, have been prepared with reference to the State Stormwater Policy. The purpose of this report is to supplement the afore mentioned documents to show compliance with the council and state policies and to ensure that the councils' downstream infrastructure and adjacent properties will not be negatively affected by the development.

This report should be read in conjunction with the Development Application drawings prepared by Collective Consulting project number, 241043 (appendix A).

The following items have been used in the preliminary design: -

4.1.1. Design Storm Events and Climate Effects

- Major Design Storm Event, 1% AEP
- Minor Design Storm Event, 5% AEP, or 2% AEP (for industrial zones) U.N.O.
- Climate Change Allowance as per AR&R scenario RCP 8.5 for the year 2090 equating to 16.3% increase in rainfall (see below section 0).

4.1.2. Stormwater Quality Targets

This report does not address the stormwater treatment of rainfall and runoff, rather the future developments of each lot will be required to meet the stormwater quality targets as per the State Stormwater Policy.

These are;

- 90% reduction in the average annual load of litter/gross pollutants based on typical urban stormwater concentrations; AND
- 80% reduction in the average annual load of total suspended solids (TSS) based on typical urban stormwater TSS concentrations; AND
- 45% reduction in the average annual load of total phosphorus (TP) based on typical urban stormwater TP concentrations; AND
- 45% reduction in the average annual load of total nitrogen (TN) based on typical urban stormwater TN concentrations.

4.1.3. Onsite Stormwater Detention

The onsite stormwater detention has been assessed based on restricting the stormwater flow off the site to the predevelopment levels for the minor and major storm events.

4.1.4. Climate Change

Climate change is expected to reduce annual rainfall but generate more intense rainfall events in a warming climate. This will intensify the challenges of providing secure water supplies and mitigating urban stormwater runoff. To allow for the effects of climate change the stormwater design has incorporated a climate change allowance as per the Glenorchy CBD 2018 Flood Study. This requires a 24% increase in rainfall intensity.

4.2. Pre-development Hydrology

The site is a 2675m^2 residential site. The site in its predevelopment state consists of two small sheds, gravel tracks/driveways and the rest as landscaping area. It has a time of concentration of approximately 6 minutes.

4.3. Post-development Hydrology

The site is to be developed as a multi-residential site containing 12 units and a private carpark.

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SITE AREA TABLE

Type / Location	Pre-Development Area	Post-Development Area
Buildings	84m2	932m2
Concrete / Asphalt Hardstand / Gravel	210m2	654m2
Landscaping	2,381m2	1089m2
Total Site Area	2,675m2	2,675m2

4.4. Calculation of On-Site Detention Requirements

The post-development impervious fraction for the site is 59% impervious. This gives a Rational Method coefficient of C_{10} = 0.58 which is increased from the pre-development coefficient of C_{10} = 0.22.

The on-site stormwater detention calculations review a series of storm events for the 5% AEP and 1% AEP for 5 minute storms through to 72 hours storms. The calculations utilise the Rational Method of AR&R to calculate the permissible discharge (based on the predevelopment discharge rate as per 4.2 Pre-development Hydrology), the site volume rainfall and thus the required storage volume to maintain the permissible discharge rate.

The pre-development discharge rate for the 5% AEP storm event is 7.5 L/s and for the 1% AEP storm event is 22.8 L/s (refer calculations Appendix B1 – Permissible Discharge).

The area surrounding the boundary between 168A Abbotsfield Road and 168B Abbotsfield Road is classified as a flood prone hazard area as per the Tasmanian Planning Scheme. Thus, the underground stormwater detention tank will be located outside of this zone.

As a result of the location, stormwater runoff from some hardstand areas and Units 5, 6, 7 and 8 will be unrestricted. The flow rate produced by this area is 5.4 L/s (refer Appendix B2 – Unrestricted Flow).

To maintain the site's predeveloped flow rate, the underground inline detention tank has been designed to accommodate the difference in the predeveloped discharge and the unrestricted flow rate of 2.1 L/s (7.5 – 5.4). The storage calculations can be reviewed in (refer Appendix B3 – Detention Volume).

The peak storage requirement for the 5% AEP is 28m^3. A reduced outlet will be installed to restrict flow from the site.

4.5. Stormwater Treatment

Along with stormwater volume management, stormwater quality management through treatments is an essential part of the stormwater design. Each development relies on the networks of stormwater infrastructure downstream to manage the runoff impacts. Effective water quality treatments as the source collection points are essential in improving water quality and minimising the potential harm caused to waterways, estuaries and the ocean environments.

Stormwater treatment is achieved through Water Sensitive Urban Design (WSUD). WSUD is as the integration of urban planning with the management, protection and conservation of the urban water cycle, that ensures urban water management is sensitive to natural hydrological and ecological processes. The Environmental Protection Authority Tasmania (EPATAS) has prepared the State Stormwater Strategy (2010) which state the minimum stormwater quality targets as listed in 4.1.2 Stormwater Quality Targets

These targets can be met using a specialised system such as Atlan treatment train.

Generally, each pit will be protected by a filter system of minimum 200µm mesh bag and filtration cartridges will be located in maintenance holes at the end of the line prior to discharging from the site.

The final design of the treatment system will be modelled using the MUSIC software prior to the final design for Plumbing Approval.

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4.6. Stormwater Summary

In conclusion this report, and accompanying calculations and drawings indicate that the development will not detrimentally affect the downstream council assets, nor will it flood the adjacent properties for all storm events up to and including the 1% AEP storm.

The proposed DN150 lot connection is sufficient to carry the flow rates for the proposed development for all storms up to and including the 5% AEP storm. All flow through this discharge will be pre-treated through a pre-treatment system prior to being discharged to council assets.

At certain storms between the 5% AEP storm and the 1% AEP storms, the lot connection will be at capacity and flows created by events less frequent than this up to and including the 1% events will overflow and discharge overland, in a controlled manner, to the rock lined vee drain within the unmade road reserve.

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5. SUMMARY

The above report in conjunction with the attached development application drawings and calculations demonstrate that the surrounding infrastructure can accommodate the new development and will not have detrimental affects on the public drainage and water infrastructure.

New lot connections for stormwater drainage (DN150), sewer drainage (DN100) and water supply (DN40) are required and will be installed to the local authority standards and requirements.

It has been determined that stormwater detention is required for this development and that pretreatment solution for stormwater runoff can be accommodated within the site to meet the requirements of the State Stormwater Strategy.

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6. APPENDICES

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MIDSON traffic pty ltd

Cunic Homes

168A Abbotsfield Road, Claremont
Traffic Impact Assessment

January 2025

CELEBRATING 15 YEARS
2008 - 2023

Contents

Figure Index

1. Introduction

1.1 Background

Midson Traffic were engaged by Cunic Homes to prepare a traffic impact assessment for a proposed residential unit development at 168A Abbotsfield Road, Claremont.

1.2 Traffic Impact Assessment (TIA)

A traffic impact assessment (TIA) is a process of compiling and analysing information on the impacts that a specific development proposal is likely to have on the operation of roads and transport networks. A TIA should not only include general impacts relating to traffic management, but should also consider specific impacts on all road users, including on-road public transport, pedestrians, cyclists and heavy vehicles.

This TIA has been prepared in accordance with the Department of State Growth (DSG) publication, *Traffic Impact Assessment Guidelines*, August 2020. This TIA has also been prepared with reference to the Austroads publication, *Guide to Traffic Management,* Part 12: *Integrated Transport Assessments for Developments*, 2020.

Land use developments generate traffic movements as people move to, from and within a development. Without a clear understanding of the type of traffic movements (including cars, pedestrians, trucks, etc), the scale of their movements, timing, duration and location, there is a risk that this traffic movement may contribute to safety issues, unforeseen congestion or other problems where the development connects to the road system or elsewhere on the road network. A TIA attempts to forecast these movements and their impact on the surrounding transport network.

A TIA is not a promotional exercise undertaken on behalf of a developer; a TIA must provide an impartial and objective description of the impacts and traffic effects of a proposed development. A full and detailed assessment of how vehicle and person movements to and from a development site might affect existing road and pedestrian networks is required. An objective consideration of the traffic impact of a proposal is vital to enable planning decisions to be based upon the principles of sustainable development.

This TIA also addresses the relevant clauses of C2.0, *Parking and Sustainable Parking Code*, and C3.0, *Road and Railway Assets Code*, of the Tasmanian Planning Scheme – Glenorchy, 2021.

1.3 Statement of Qualification and Experience

This TIA has been prepared by an experienced and qualified traffic engineer in accordance with the requirements of Council's Planning Scheme and The Department of State Growth's, *Traffic Impact Assessment Guidelines*, August 2020, as well as Council's requirements.

The TIA was prepared by Keith Midson. Keith's experience and qualifications are briefly outlined as follows:

- 29 years professional experience in traffic engineering and transport planning.
- Master of Transport, Monash University, 2006
- Master of Traffic, Monash University, 2004

- Bachelor of Civil Engineering, University of Tasmania, 1995
- Engineers Australia: Fellow (FIEAust); Chartered Professional Engineer (CPEng); Engineering Executive (EngExec); National Engineers Register (NER)

1.4 Project Scope

The project scope of this TIA is outlined as follows:

- Review of the existing road environment in the vicinity of the site and the traffic conditions on the road network.
- Provision of information on the proposed development with regards to traffic movements and activity.
- Identification of the traffic generation potential of the proposal with respect to the surrounding road network in terms of road network capacity.
- Review of the parking requirements of the proposed development. Assessment of this parking supply with Planning Scheme requirements.
- Traffic implications of the proposal with respect to the external road network in terms of traffic efficiency and road safety.

1.5 Subject Site

The subject site is located at 168A Abbotsfield Road, Claremont. The site currently contains a residential dwelling.

The subject site and surrounding road network is shown in Figure 1.

Figure 1 Subject Site & Surrounding Road Network

Image Source: LIST Map, DPIPWE

1.6 Reference Resources

The following references were used in the preparation of this TIA:

- Tasmanian Planning Scheme – Glenorchy, 2021 (Planning Scheme)
- Austroads, *Guide to Traffic Management,* Part 12: *Integrated Transport Assessments for Developments*, 2020
- Austroads, Guide to Road Design, Part 4A: Unsignalised and Signalised Intersections, 2021
- Department of State Growth, *Traffic Impact Assessment Guidelines*, 2020
- Roads and Maritime Services NSW, *Guide to Traffic Generating Developments*, 2002 (RMS Guide)
- Roads and Maritime Services NSW, *Updated Traffic Surveys,* 2013 (Updated RMS Guide)
- Australian Standards, AS2890.1, *Off-Street Parking*, 2004 (AS2890.1)

2. Existing Conditions

2.1 Transport Network

For the purposes of this report, the transport network consists of Abbotsfield Road.

Abbotsfield Road is a collector road that connects between Main Road and Toffolis Road. It provides access to a predominantly residential catchment area. The general urban speed limit of 50-km/h is applicable to Abbotsfield Road. Abbotsfield Street carries approximately 1,000 vehicles per day to the east of Toffolis Road.

Figure 2 Abbotsfield Road

2.2 Public Transport

Metro Tasmania operate regular bus services along Abbotsfield Road near the site. Services on Abbotsfield Road extend to Harbord Road (approximately 300 metres walking distance from the subject site). Routes 510 and X10 service the road network near the subject site.

2.3 Road Safety Performance

Crash data can provide valuable information on the road safety performance of a road network. Existing road safety deficiencies can be highlighted through the examination of crash data, which can assist in determining whether traffic generation from the proposed development may exacerbate any identified issues.

Crash data was obtained from the Department of State Growth for a 5+ year period between 1st January 2020 to 31st December 2024 for Abbotsfield Road between Toffolis Road to Elliston Street. Only one crash was reported during this time:

- 5:35pm, 11th June 2020 – 'fell from vehicle' incident immediately east of the subject site's access resulting in minor injury.

The crash data indicates that there are no pre-existing road safety deficiencies in the surrounding transport network.

3. Proposed Development

3.1 Development Proposal

The proposed development involves the construction of 12 residential units. A total of 24 on-site car parking spaces are proposed within a car park accessed via a single driveway connecting to the adjacent right-of-way.

The proposed development layout plans are shown in Figure 3.

Figure 3 Proposed Development Plans

4. Traffic Impacts

4.1 Trip Generation

Traffic generation rates were sourced from the RMS Guide. The RMS Guide states the following traffic generation rates for medium density residential developments with three or more bedrooms:

- Daily vehicle trips 4.0 – 5.0 per dwelling
- Weekday peak hour vehicle trips 0.4 – 0.5 per dwelling

Based on these trip generation rates, the new traffic generation from the unit development when fully developed is likely to be in the order of 60 trips per day, and 6 trips per hour during peak periods (using a rate of 5 trips per dwelling per day, and peak of 0.5 vehicles per hour per dwelling).

4.2 Trip Assignment

All traffic will utilise the right-of-way to access Abbotsfield Road. At the junction with Abbotsfield Road, traffic will predominantly right-in/ left-out movements.

4.3 Access Impacts

Access to the site is via an existing ROW that connects to Abbotsfield Road. The ROW has a sealed pavement surface for approximately 20 metres from Abbotsfield Road, then is unsealed for the remainder of its length (the length of unsealed pavement is approximately 30 metres from the edge of the seal to the access to the site). The ROW varies in width between 3.5 to 4 metres, with unsealed verges that can be utilised for vehicle passing if necessary. The ROW access is shown in Figure 4.

For completeness, this assessment investigates both the access to the ROW and the ROW access to Abbotsfield Road.

The Acceptable Solution A1.4 of Clause C3.5.1 of the Planning Scheme states "*Vehicular traffic to and from the site, using an existing vehicle crossing or private level crossing, will not increase by more than the amounts in Table C3.1*".

Table C3.1 states a maximum increase of 20% or 40 vehicles per day (for a minor road), whichever is greater. In this case the traffic generation of 60 vehicles per day will exceed the threshold requirements for both the ROW and Abbotsfield Road junctions.

The access therefore does not comply with the requirements of Acceptable Solution A1.4 of Clause C3.5.1 of the Planning Scheme.

The Performance Criteria P1 of Clause C3.5.1 of the Planning Scheme is applicable to both accesses, which states:

"Vehicular traffic to and from the site must minimise any adverse effects on the safety of a junction, vehicle crossing or level crossing or safety or efficiency of the road or rail network, having regard to:

(a) any increase in traffic caused by the use;

(b) the nature of the traffic generated by the use;

(c) the nature of the road;

(d) the speed limit and traffic flow of the road;

(e) any alternative access to a road;

(f) the need for the use;

(g) any traffic impact assessment; and

(h) any advice received from the rail or road authority".

The following is relevant to the proposed development:

a. Increase in traffic. The increase in traffic will be in the order of 60 vehicles per day, with a peak increase of 6 vehicles per hour (an average of 1 vehicle movement every 10 minutes). The relatively low traffic generation at each access will result in a high level of efficiency and safety.

b. Nature of traffic. The traffic will be residential in nature, which is consistent with the traffic currently utilising the surrounding road network.

c. Nature of road. Abbotsfield Road is a collector road that carries a predominantly residential traffic. It provides access to residential and rural catchment areas along its length, including the area of the subject site. The nature of the roads is compatible with the nature and type of traffic generated by the proposed development.

d. Speed limit and traffic flow of road. Abbotsfield Road has a posted speed limit of 50-km/h and a volume of approximately 1,000 vehicles per day. The traffic flow of Abbotsfield Road increases to the east as more residential catchment connects with the road and the arterial road network (Brooker Highway and Main Road). The low volume near the subject site is reflective of the rural nature of the road network to the west of the site (Toffolis Road and connecting roads).

e. Alternative access. No alternative access is possible or considered necessary.

f. Need for use. The accesses are required to service the parking areas associated with the residential development.

g. Traffic impact assessment. This report documents the findings of a traffic impact assessment.

h. Road authority advice. Council (as road authority) have requested that a TIA be prepared in support of the proposed development.

Based on the above assessment, the proposed access arrangements comply with the requirements of Performance Criteria P1 of Clause C3.5.1 of the Planning Scheme. Specifically, there is sufficient spare capacity in these accesses to accommodate the traffic generation associated with the proposed development at a high level of service.

Figure 4 ROW Access to Site

4.4 Sight Distance

Australian Standards, AS2890.1, provide the sight distance requirements for residential and domestic driveways. Sight distance requirements are lower for driveways compared to road junctions.

The sight distance requirements are determined by the frontage road speed limit. The AS2890.1 sight distance requirements for each access are summarised as follows (noting that the frontage speed of the ROW has been assumed to be 40-km/h accounting for the local environment of the access and noting that the actual speeds are lower):

- ROW (40-km/h) 35 metres minimum
- Abbotsfield Road (50-km/h) 40 metres minimum

The available sight distance exceeds the minimum Austroads requirements. The available sight distance is therefore acceptable along the ROW from the site, as well as along Abbotsfield Road from the ROW junction.

4.5 Pedestrian Impacts

The proposed development will generate a relatively small amount of pedestrian activity in the surrounding network. There are few pedestrian generating land uses near the site. It is likely that the majority of pedestrian movements will be associated with activity to and from bus stops in Abbotsfield Road near the site.

Within the car park a shared 1.0-metre wide pedestrian path connects between the ROW and the main internal pedestrian path connecting to the residential units. This is shown in Figure 5.

The Acceptable Solution A1 of Clause C2.6.5 of the Planning Scheme states:

> *"Uses that require 10 or more car parking spaces must:*
>
> *(a) have a 1m wide footpath that is separated from the access ways or parking aisles, excluding where crossing access ways or parking aisles, by:*
>
> *(i) a horizontal distance of 2.5m between the edge of the footpath and the access way or parking aisle; or*
>
> *(ii) protective devices such as bollards, guard rails or planters between the footpath and the access way or parking aisle; and*
>
> *(b) be signed and line marked at points where pedestrians cross access ways or parking aisles".*

In this case pedestrian footpath within the development are provided but it is not located 2.5 metres from the access way and is not protected by bollards or other protective devices. On this basis the Acceptable Solution A1 of Clause C2.6.5 of the Planning Scheme is not met.

The Performance Criteria P1 of Clause C2.6.5 of the Planning Scheme states:

> *"Safe and convenient pedestrian access must be provided within parking areas, having regard to:*
>
> *(a) the characteristics of the site;*
>
> *(b) the nature of the use;*
>
> *(c) the number of parking spaces;*
>
> *(d) the frequency of vehicle movements;*
>
> *(e) the needs of persons with a disability;*
>
> *(f) the location and number of footpath crossings;*
>
> *(g) vehicle and pedestrian traffic safety;*
>
> *(h) the location of any access ways or parking aisles; and*
>
> *(i) any protective devices proposed for pedestrian safety".*

The following is relevant with respect to the development:

a. Characteristics of site. The site is a small residential unit development with a private car park. Pedestrian paths are provided within the site connecting between the units and the car park. Vehicle speeds will be very low by virtue of the layout of the internal road network associated with the development (ie. Very short distance of the central aisle within the car park prevents high vehicle speeds). The layout of the development is consistent with contemporary residential unit design.

b. Nature of the use. The use is residential, which is consistent with land use in the surrounding area.

c. Number of parking spaces. A total of 24 on-site parking spaces are proposed, accessed via a single driveway access. Parking is well defined within the site.

d. Frequency of vehicle movements. The peak traffic generation will be 6 vehicles per hour, which is an average of 1 vehicle movement every 10 minutes. The low traffic generation coupled with the low vehicle speeds will result in an acceptable safety environment for shared use between pedestrians and vehicles.

e. Needs of persons with a disability. Not applicable.

f. Location and number of footpath crossings. Not applicable.

g. Vehicle and pedestrian safety. A 1.0-metre footpath is provided within the site. As noted in d above, the low traffic generation coupled with the low vehicle speeds will result in an acceptable safety environment for shared use between pedestrians and cars.

h. Location of access ways or parking aisles. The development has a relatively simple layout with a main circulation access running around the site. Parking is accessed at 90-degrees as internal driveways associated with each unit within the main area.

i. Protective devices. No pedestrian protective devices are included in the design. The low-speed and low volume environment associated with the site does not warrant the use of protective devices.

Based on the above assessment, the development meets the requirements of Performance Criteria P1 of Clause C2.6.5 of the Planning Scheme.

4.6 Road Safety Impacts

There are no significant detrimental road safety impacts foreseen for the proposed residential unit development. This is based on the following:

- The surrounding road network is able to adequately absorb the relatively low amount of traffic generated by the proposed development. Noting particularly that the peak hour flow increase in Abbotsfield Road is likely to be in the order of 6 vehicles per hour.

- The existing road safety performance of the road network does not indicate that there are any current road safety deficiencies that might be exacerbated by the proposed development.
- Adequate sight distance is available at the proposed site access at Abbotsfield Road in relation to the prevailing vehicle speeds and posted speed limit of 50-km/h.
- The proposed development is located in a predominantly residential area, and as such movements into and out of the subject site will not be seen as an uncommon event by other motorists.

5. Parking Assessment

5.1 Parking Provision

The proposed development provides a total of 24 on-site car parking spaces. This consists of 2 visitor spaces and 22 resident parking spaces.

The car parking layout is shown in Figure 5.

5.2 Empirical Parking Demand

The RMS Guide was utilised for calculating the parking demands associated with the residential unit component of the proposed development. The RMS Guide is a nationally recognised reference for traffic generation and parking demands associated with developments. The use of the RMS Guide has been extensively utilised for Tasmanian developments for many years.

It is common sense that medium-density and high-density housing developments will generate a lower parking demand than an equivalent number of stand-alone dwellings. This is due to the reduced floor area associated with units when compared to standalone dwellings.

The RMS Guide provides recommendations for various types of residential dwelling types. The most applicable to the proposed development is medium-density residential developments. The RMS Guide defines medium density as "*A medium density residential flat building is a building containing at least 2 but less than 20 dwellings. This includes villas, town houses, flats, semi-detached houses, terrace or row houses and other medium density developments*".

In this case the development consists of 12 standalone units and therefore satisfies the description of medium density housing under the RMS Guide. The parking demands associated with medium density housing is as follows:

- 1 space for each unit; plus
- 1 space for each 5 x 2-bedroom unit; plus
- 1 space for each 2 x 3-bedrom unit; plus
- 1 space per five units for visitor parking
- TOTAL – 17 spaces

This is a requirement for 17 spaces. The provision of 24 spaces therefore satisfies this likely parking demand.

5.3 Planning Scheme Requirements

The Acceptable Solution A1 of Clause C2.5.1 of the Planning Scheme states:

> *"The number of on-site car parking spaces must be no less than the number specified in Table C2.1, excluding if:*
>
> *(a) the site is subject to a parking plan for the area adopted by council, in which case parking provision (spaces or cash-in-lieu) must be in accordance with that plan;*
>
> *(b) the site is contained within a parking precinct plan and subject to Clause C2.7;*
>
> *(c) the site is subject to Clause C2.5.5; or*
>
> *(d) it relates to an intensification of an existing use or development or a change of use where:*
>
> *(i) the number of on-site car parking spaces for the existing use or development specified in Table C2.1 is greater than the number of car parking spaces specified in Table C2.1 for the proposed use or development, in which case no additional on-site car parking is required; or*
>
> *(ii) the number of on-site car parking spaces for the existing use or development specified in Table C2.1 is less than the number of car parking spaces specified in Table C2.1 for the proposed use or development, in which case on-site car parking must be calculated as follows:*
>
> *N = A + (C- B)*
>
> *N = Number of on-site car parking spaces required*
>
> *A = Number of existing on site car parking spaces*
>
> *B = Number of on-site car parking spaces required for the existing use or development specified in Table C2.1*
>
> *C= Number of on-site car parking spaces required for the proposed use or development specified in Table C2.1".*

In this case, sub-points (a), (b), (c), and (d) are not applicable.

The parking requirements of Table C2.1 are 2 spaces per unit plus 1 space per 3 units visitor parking (internal lot). This is a requirement for 28 spaces. The provision of 24 spaces therefore does not satisfy the requirements of Acceptable Solution A1 of Clause C2.5.1 of the Planning Scheme.

The Performance Criteria P1.2 of Clause C2.5.1 of the Planning Scheme states:

> *"The number of car parking spaces for dwellings must meet the reasonable needs of the use, having regard to:*
>
> *(a) the nature and intensity of the use and car parking required;*
>
> *(b) the size of the dwelling and the number of bedrooms; and*

(c) the pattern of parking in the surrounding area".

The following is relevant with respect to the proposed development:

a. Nature and intensity of use and car parking required. The likely parking demands of the proposed development is 17 spaces (refer to Section 5.2). The provision of 24 spaces therefore satisfies the likely parking demands of the proposed development.

b. Size of dwellings and number of bedrooms. The dwellings have a floor area of approximately 60-m^2 and have two bedrooms. The relatively small size of the units will likely result in low car ownership rates.

c. Pattern of parking in surrounding area. The surrounding area is residential in nature. Typically parking is provided on-site (driveway and garage parking) as well as on-street in Abbotsfield Road. General observations indicated that on-street parking demands are relatively low, with parking availability throughout the day.

Based on the above assessment the proposed development satisfies the requirements of Performance Criteria P1 of Clause C2.5.1 of the Planning Scheme. Specifically the likely parking demands associated with the small size of the units will be satisfied by the on-site parking provision.

5.4 Car Parking Layout

The car parking layout consists of two rows of 12 spaces connected by a single aisle that is accessed via a ROW. The car parking layout is shown in Figure 5.

The Acceptable Solution A1.1 of Clause C2.6.2 of the Planning Scheme states:

"Parking, access ways, manoeuvring and circulation spaces must either:

(a) comply with the following:

(i) have a gradient in accordance with Australian Standard AS 2890 - Parking facilities, Parts 1-6;

(ii) provide for vehicles to enter and exit the site in a forward direction where providing for more than 4 parking spaces;

(iii) have an access width not less than the requirements in Table C2.2;

(iv) have car parking space dimensions which satisfy the requirements in Table C2.3;

(v) have a combined access and manoeuvring width adjacent to parking spaces not less than the requirements in Table C2.3 where there are 3 or more car parking spaces;

(vi) have a vertical clearance of not less than 2.1m above the parking surface level; and

(vii) excluding a single dwelling, be delineated by line marking or other clear physical means; or

(b) comply with Australian Standard AS 2890- Parking facilities, Parts 1-6".

The development was assessed against A1.1(b). The relevant Australian Standards associated with the development is AS2890.1. The assessment is provided in the following sections.

5.4.1 Driveway Grade

Section 2.5.3(b) of AS2890.1 states the following regarding the maximum grade of straight ramps:

i. Longer than 20 metres – 1 in 5 (20%) maximum.

ii. Up to 20 metres long – 1 in 4 (25%) maximum. The allowable 20 m maximum length shall include any parts of the grade change transitions at each end that exceed 1 in 5 (20%).

The maximum grade of all driveways is well below the maximum AS2890.1 requirements.

5.4.2 Parking Grade

Section 2.4.6 of AS2890.1 states that the maximum grades within a car park shall be:

- Measured parallel to the angle of parking 1 in 20 (5%)
- Measured in any other direction 1 in 16 (6.25%)

The grades of the parking spaces are effectively level, thus complying with the AS2890.1 grade requirements.

5.4.3 Parking Dimensions

AS2890.1 defines the parking as User Class 1A, *Residential, Domestic and Employee Parking*. Parking dimension requirements for 90-degree parking for User Class 1A are:

- Space length 5.4 metres
- Space width 2.4 metres
- Aisle width 5.8 metres

All parking spaces comply with AS2890.1 dimensional requirements, noting that the aisle width and space widths exceed the minimum requirements.

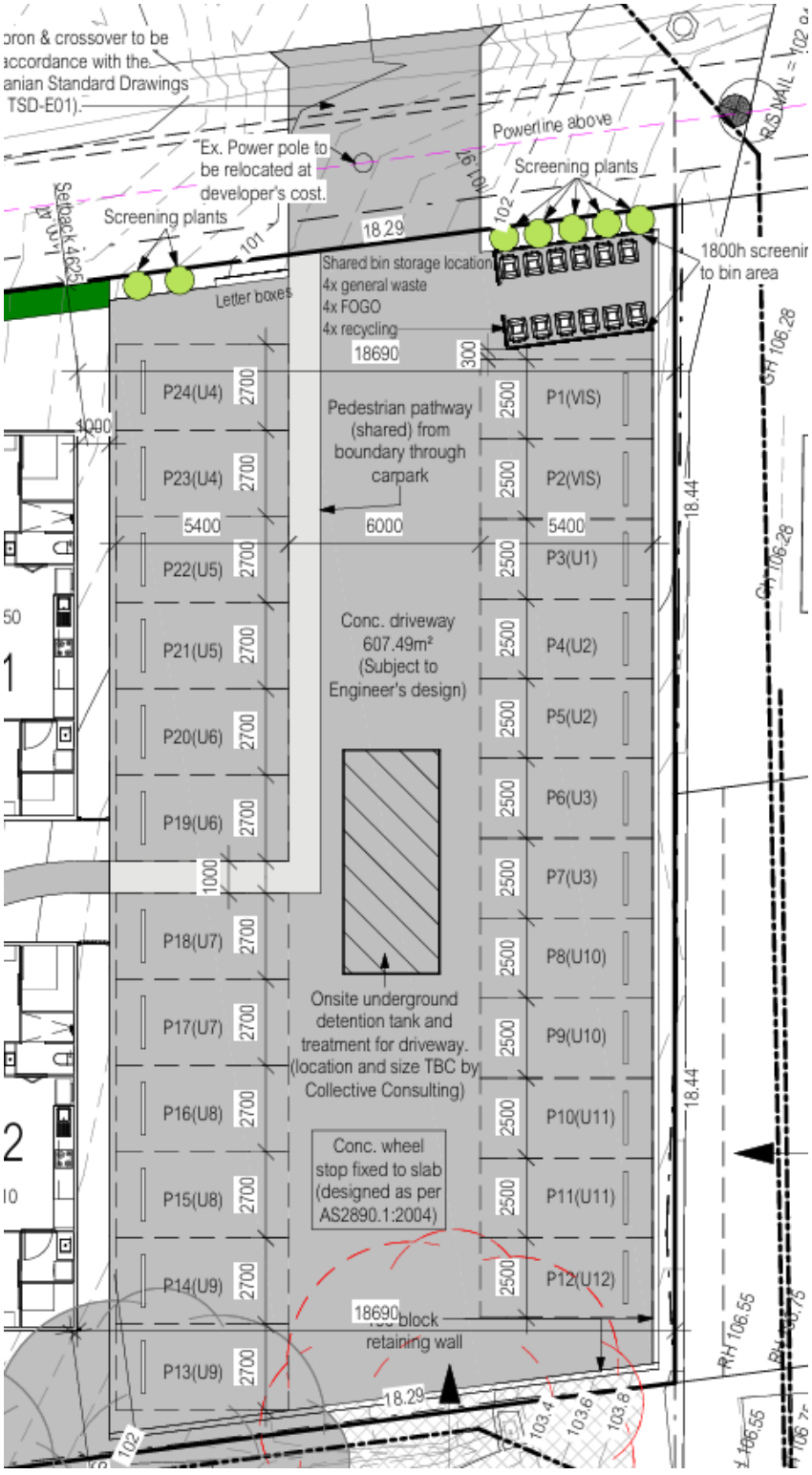
AS2890.1 defines both accesses servicing the proposed development as 'Category 1' access facility (Class 1A parking with less than 25 spaces fronting onto a local road). The AS2890.1 minimum driveway width requirement for a Category 1 access is 3.0 metres.

The available width at both access driveways complies with this requirement. The access width complies with the requirements of AS2890.1.

5.4.4 AS2890.1 Assessment Summary

The parking space dimensions and manoeuvring areas comply with the requirements of AS2890.1. The development therefore complies with the requirements of Acceptable Solution A1.1(b) of Clause C2.6.2 of the Planning Scheme.

Figure 5 Car Parking Layout



6. Conclusions

This traffic impact assessment (TIA) investigated the traffic and parking impacts of a proposed residential unit development at 168A Abbotsfield Road, Claremont.

The key findings of the TIA are summarised as follows:

- The development comprises of 12 x 2-bedroom units with 24 on-site car parking spaces.
- The traffic generation of the proposed development is likely to be 60 vehicles per day with a peak of 6 vehicles per hour.
- The traffic generation at the site's access on the ROW and Abbotsfield Road satisfies the requirements of Performance Criteria P1 of Clause C3.5.1 of the Planning Scheme.
- The pedestrian infrastructure within the site meets the requirements of Performance Criteria P1 of Clause C2.6.5 of the Planning Scheme.
- The car parking provision of 24 on-site parking spaces satisfies the requirements of Performance Criteria P1.2 of Clause C2.5.1 of the Planning Scheme. The relatively small size of the residential units will result in a lower parking demand than the requirements of Table C2.1 of the Planning Scheme. The likely parking demand of the proposed development will be 17 spaces, and therefore the provision of 24 spaces is considered acceptable.
- The car parking layout of the development meets the requirements of Acceptable Solution A1.1(b) of Clause C2.6.2 of the Planning Scheme.

Based on the findings of this report the proposed development is supported on traffic grounds.

Midson Traffic Pty Ltd ABN: 26 133 583 025

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T: 0437 366 040 E: admin@midsontraffic.com.au W: www.midsontraffic.com.au

Document Status

Revision	Author	Review	Date
0	Keith Midson	Zara Kacic-Midson	8 January 2025

CUNIC homes

Built for you.

U249 - PROPOSED CUNIC UNIT DEVELOPMENT
168a Abbotsfield Road
CLAREMONT

SHEET		DRAWING TITLE	SHEET		DRAWING TITLE
01	C	SITE PLAN	12	B	UNIT 6 FLOOR PLAN
01a	C	PERSPECTIVE VIEWS	13	B	UNIT 6 ELEVATIONS
01b	C	PRIVATE OPEN SPACE PLAN	14	B	UNIT 7 FLOOR PLAN
01c	C	COMMUNAL AREA PLAN	15	B	UNIT 7 ELEVATIONS
02	B	UNIT 1 FLOOR PLAN	16	B	UNIT 8 FLOOR PLAN
03	B	UNIT 1 ELEVATIONS	17	B	UNIT 8 ELEVATIONS
04	B	UNIT 2 FLOOR PLAN	18	B	UNIT 9 FLOOR PLAN
05	B	UNIT 2 ELEVATIONS	19	B	UNIT 9 ELEVATIONS
06	B	UNIT 3 FLOOR PLAN	20	B	UNIT 10 FLOOR PLAN
07	B	UNIT 3 ELEVATIONS	21	B	UNIT 10 ELEVATIONS
08	B	UNIT 4 FLOOR PLAN	22	B	UNIT 11 FLOOR PLAN
09	B	UNIT 4 ELEVATIONS	23	B	UNIT 11 ELEVATIONS
10	B	UNIT 5 FLOOR PLAN	24	B	UNIT 12 FLOOR PLAN
11	B	UNIT 5 ELEVATIONS	25	B	UNIT 12 ELEVATIONS

GLENORCHY CITY COUNCIL
PLANNING SERVICES

APPLICATION No. : PLN-24-270

DATE RECEIVED: 06/11/2025

SITE IS NOT BUSHFIRE PRONE AREA AS PER TASMANIAN PLANNING SCHEME OVERLAY - GLENORCHY.
No additional restrictions for construction methods / materials apply.

No.	Amendment	Date	Drawn	Checked	Sheet
C	Remove individual bin storage from each unit, provide screened communal bin storage area to parking area. Update all relevant plans.	5 November 2024	ST	RJ	01 - 01c
B	CLIENT CHANGES: Remove additional roof from container units, change laserlite roof to trimdek. Update all relevant plans and Shadow Diagrams (Separate drawing set)	2 August 2024	ST	CK	01a, 02 - 25
A	CLIENT CHANGES: Provide Private Open Space plan, show Flood Prone Hazard Area, provide details on Communal Area, provide Shadow Diagrams (Separate drawing set)	11 July 2024	ST	CK	01, 01b, 01c
	DA PLAN SET	25 June 2024	ST	CK	01 - 25

Notes
- Builder to verify all dimensions and levels on site prior to commencement of work
- All work to be carried out in accordance with the current National Construction Code.
- All materials to be installed according to manufacturers specifications.
- Do not scale from these drawings.
- No changes permitted without consultation with designer.

Designer:

ANOTHER PERSPECTIVE PTY LTD
PO BOX 21
NEW TOWN
LIC. NO. 685230609 (S. Turvey)
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Email:
info@anotherperspective.com.au

Client / Project info

PROPOSED CUNIC UNIT DEVELOPMENT

168a Abbotsfield Road
CLAREMONT

Soil Classification:	TBC
Title Reference:	CT61276/27
Floor Areas:	Refer to Plans
Porch / Deck Areas:	Refer to Plans
Wind Speed:	TBC
Climate Zone:	7
Alpine Zone:	N/A
Corrosion Environment:	LOW
Certified BAL:	NOT BUSHFIRE PRONE
Designed BAL:	NOT BUSHFIRE PRONE

(Refer to Standard Notes for Explanation)

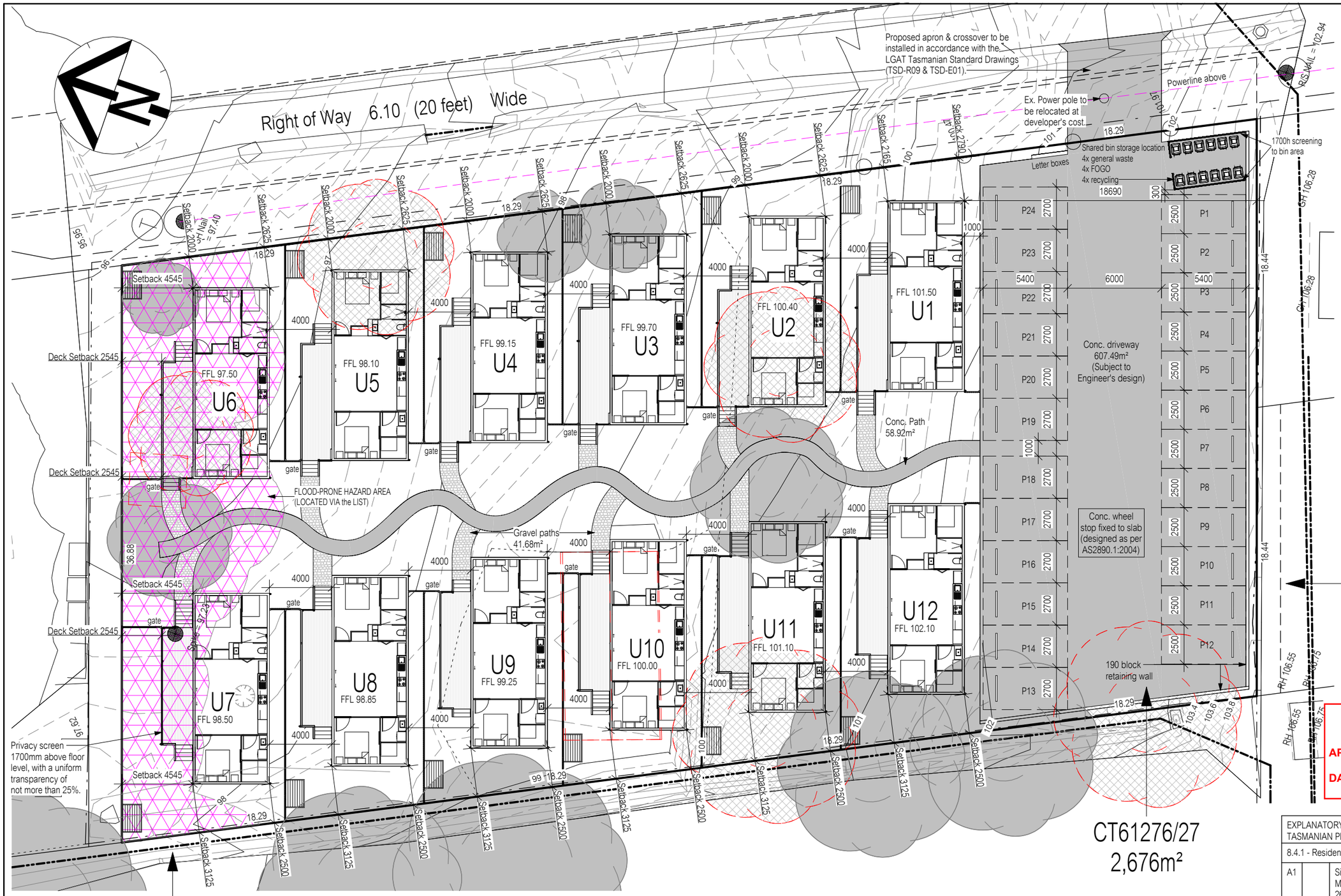
COVER SHEET

U249

Date 25 June 2024

Scale

Sheet 00/25



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GLENORCHY CITY COUNCIL PLANNING SERVICES

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EXPLANATORY NOTES:
TASMANIAN PLANNING SCHEME - GLENORCHY COUNCIL

8.4.1 - Residential density for multiple dwellings		
A1		Site Density: Min. 325m² per unit 2676m² / 12 (units) = 223m² provided
8.4.3 - Site coverage and private open space for all dwellings		
A1	(a)	Site Coverage: Max. 50% of site = 1338m² Proposed site coverage (excl. eaves up to 0.6m): 844.44m² (31.56%)

No.	Date	Int.
C	5 November 2024	ST
A	11 July 2024	ST

Amendment changes as per cover sheet

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PROPOSED CUNIC UNIT DEVELOPMENT

168a Abbotsfield Road
CLAREMONT

CUNIC homes
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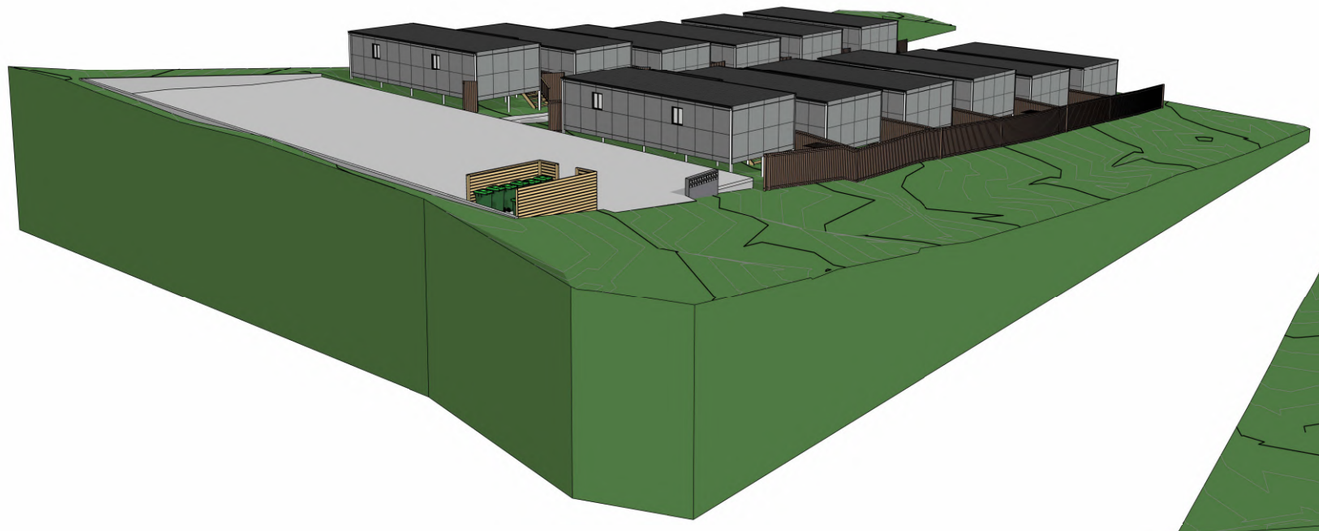
SITE PLAN

Drawn	ST	U249
Date	13 June 2024	Sheet
Scale	1 : 250	01/25



C	5 November 2024	ST
B	2 August 2024	ST
No.	Date	Int.

Amendment changes as per cover sheet

Shadows shown for stylisations purpose only

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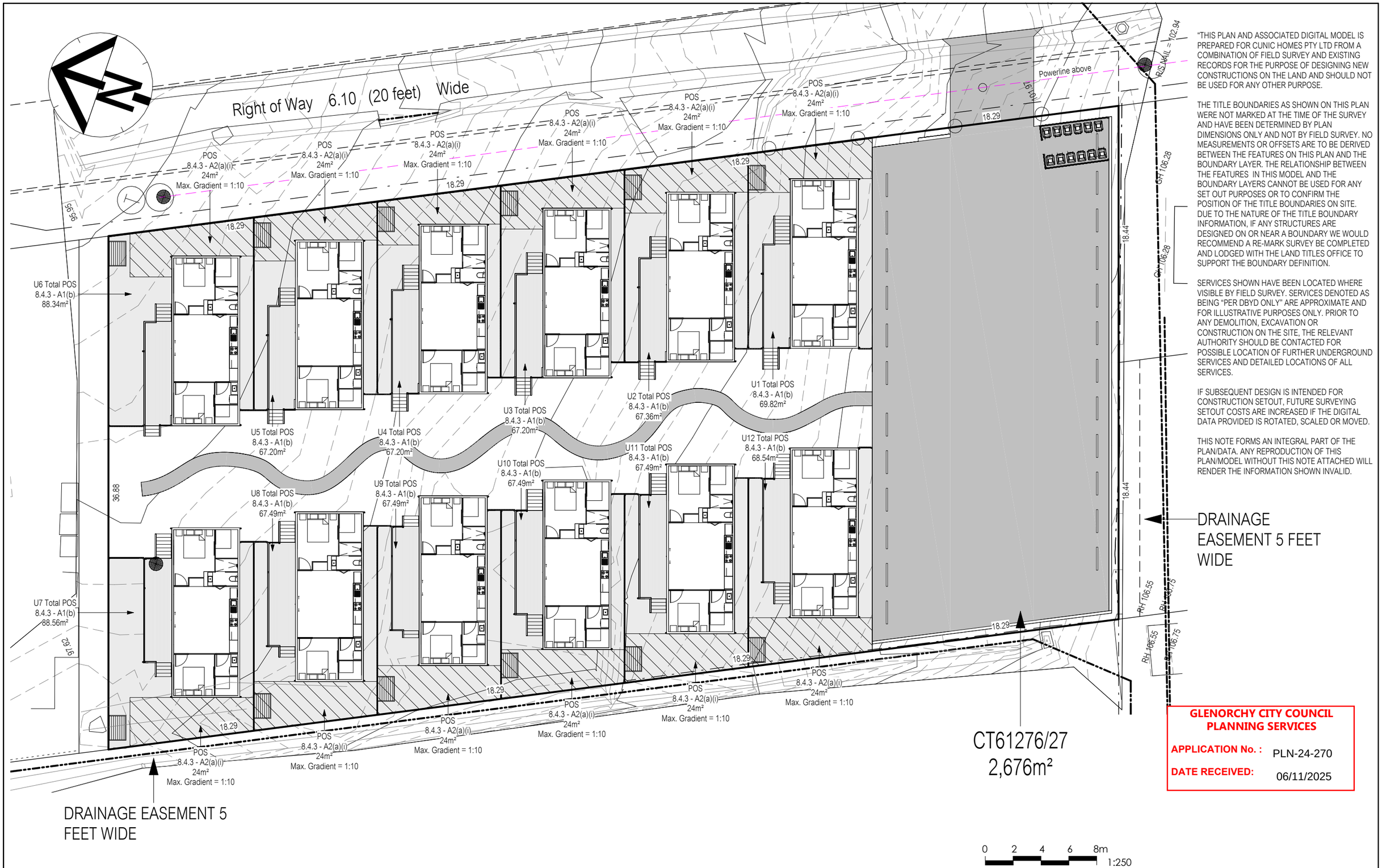
PERSPECTIVE VIEWS

Drawn	ST	U249
Date	13 June 2024	Sheet
Scale		01a/25



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CUNIC homes Built for you

PRIVATE OPEN SPACE PLAN

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Date	11 July 2024	Sheet
Scale	1 : 250	01b/25

"THIS PLAN AND ASSOCIATED DIGITAL MODEL IS PREPARED FOR CUNIC HOMES PTY LTD FROM A COMBINATION OF FIELD SURVEY AND EXISTING RECORDS FOR THE PURPOSE OF DESIGNING NEW CONSTRUCTIONS ON THE LAND AND SHOULD NOT BE USED FOR ANY OTHER PURPOSE.

THE TITLE BOUNDARIES AS SHOWN ON THIS PLAN WERE NOT MARKED AT THE TIME OF THE SURVEY AND HAVE BEEN DETERMINED BY PLAN DIMENSIONS ONLY AND NOT BY FIELD SURVEY. NO MEASUREMENTS OR OFFSETS ARE TO BE DERIVED BETWEEN THE FEATURES ON THIS PLAN AND THE BOUNDARY LAYER. THE RELATIONSHIP BETWEEN THE FEATURES IN THIS MODEL AND THE BOUNDARY LAYERS CANNOT BE USED FOR ANY SET OUT PURPOSES OR TO CONFIRM THE POSITION OF THE TITLE BOUNDARIES ON SITE. DUE TO THE NATURE OF THE TITLE BOUNDARY INFORMATION, IF ANY STRUCTURES ARE DESIGNED ON OR NEAR A BOUNDARY WE WOULD RECOMMEND A RE-MARK SURVEY BE COMPLETED AND LODGED WITH THE LAND TITLES OFFICE TO SUPPORT THE BOUNDARY DEFINITION.

SERVICES SHOWN HAVE BEEN LOCATED WHERE VISIBLE BY FIELD SURVEY. SERVICES DENOTED AS BEING "PER DBYD ONLY" ARE APPROXIMATE AND FOR ILLUSTRATIVE PURPOSES ONLY. PRIOR TO ANY DEMOLITION, EXCAVATION OR CONSTRUCTION ON THE SITE, THE RELEVANT AUTHORITY SHOULD BE CONTACTED FOR POSSIBLE LOCATION OF FURTHER UNDERGROUND SERVICES AND DETAILED LOCATIONS OF ALL SERVICES.

IF SUBSEQUENT DESIGN IS INTENDED FOR CONSTRUCTION SETOUT, FUTURE SURVEYING SETOUT COSTS ARE INCREASED IF THE DIGITAL DATA PROVIDED IS ROTATED, SCALED OR MOVED.

THIS NOTE FORMS AN INTEGRAL PART OF THE PLAN/DATA. ANY REPRODUCTION OF THIS PLAN/MODEL WITHOUT THIS NOTE ATTACHED WILL RENDER THE INFORMATION SHOWN INVALID.

GLENORCHY CITY COUNCIL PLANNING SERVICES

APPLICATION No. : PLN-24-270

DATE RECEIVED: 06/11/2025

0 2 4 6 8m
1:250

No.	Date	Int.	Amendment changes as per cover sheet
C	5 November 2024	ST	
A	11 July 2024	ST	

Notes
- Builder to verify all dimensions and levels on site prior to commencement of work
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Designer:

ANOTHER PERSPECTIVE PTY LTD
PO BOX 21
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LIC. NO. 685230609 (S. Turvey)
Ph: (03) 6231 4122
Fx: (03) 6231 4166
Email:
info@anotherperspective.com.au

Client / Project info

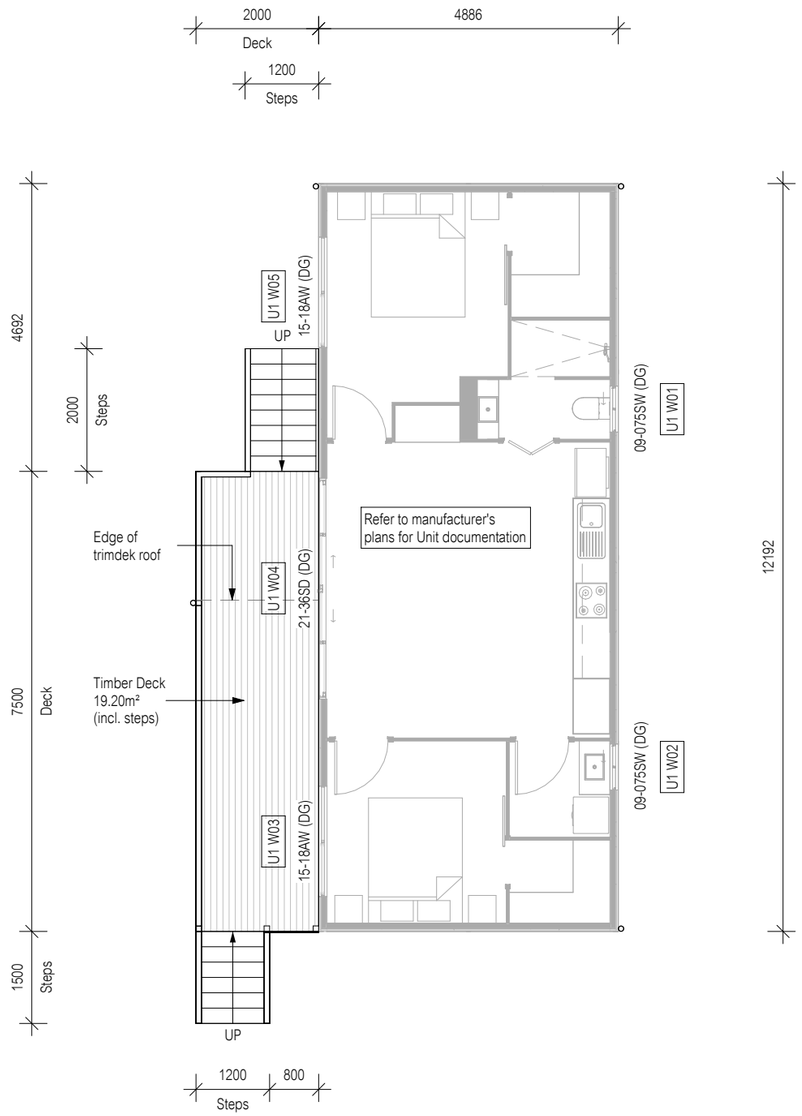
PROPOSED CUNIC UNIT DEVELOPMENT

168a Abbotsfield Road
CLAREMONT

CUNIC homes
Built for you

COMMUNAL AREA PLAN

Drawn	ST	U249
Date	11 July 2024	Sheet
Scale	1 : 250	01c/25



GLENORCHY CITY COUNCIL
PLANNING SERVICES

APPLICATION No. : PLN-24-270

DATE RECEIVED: 06/11/2025

0 1 2 3 4m
1:100

Floor Area = Refer to Manufacturer's Documentation

Articulation joints

Smoke Alarm (interconnected where more than 1)

All window sizes to be checked and/or confirmed on site prior to ordering glazing units

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Client / Project info

PROPOSED CUNIC UNIT DEVELOPMENT

168a Abbotsfield Road
CLAREMONT

CUNIC homes
Built for you

UNIT 1 FLOOR PLAN

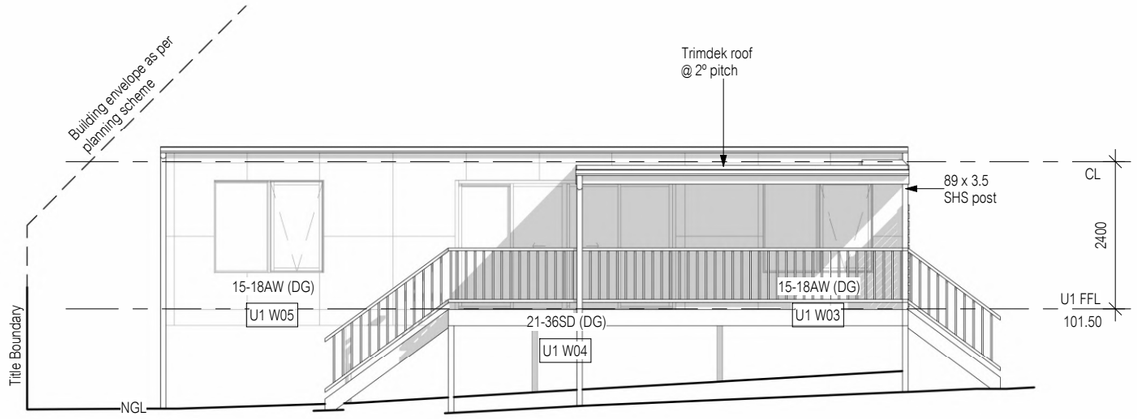
Drawn	ST	U249	
Date	11 April 2024	Sheet	02/25
Scale	1 : 100		

No.	Date	Int.
B	2 August 2024	ST

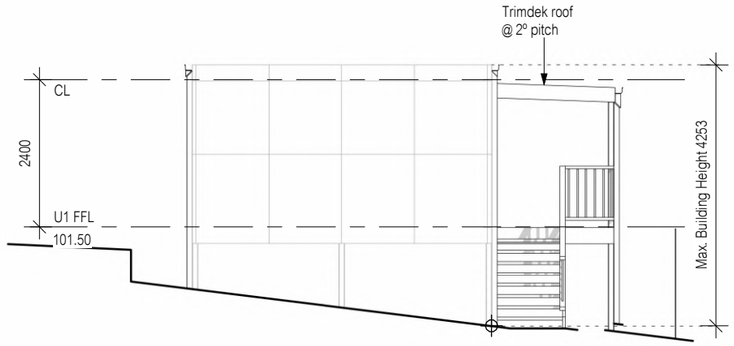
Amendment changes as per cover sheet

Material	Colour
Trimdek Roof	tbc

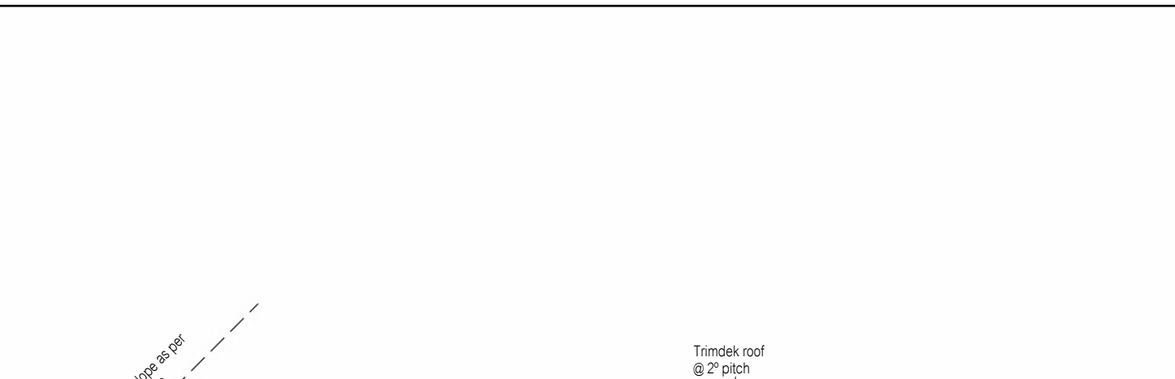
All lightweight cladding to be installed to manufacturer's guidelines. Refer to manufacturer's documentation.



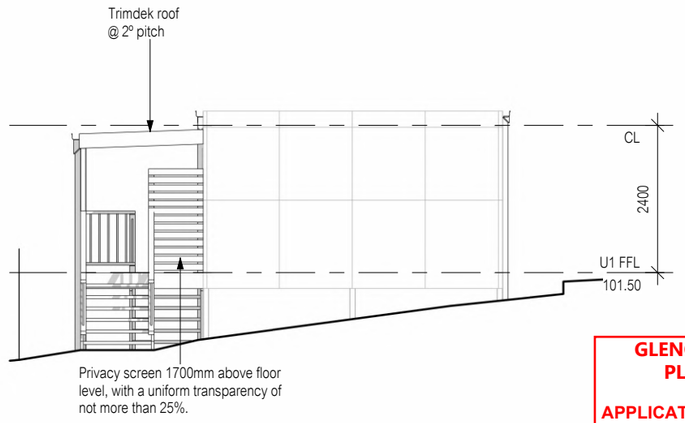
North Elevation



East Elevation



South Elevation



West Elevation

GLENORCHY CITY COUNCIL
PLANNING SERVICES

APPLICATION No. : PLN-24-270

DATE RECEIVED: 06/11/2025

No.	Date	Int.
B	2 August 2024	ST

Amendment changes as per cover sheet

All window sizes to be checked and/or confirmed on site prior to ordering glazing units

LEGEND:
AJ - Articulation Joint
BV - Brick Vent

Shadows shown for stylisation purposes only

Notes
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Client / Project info

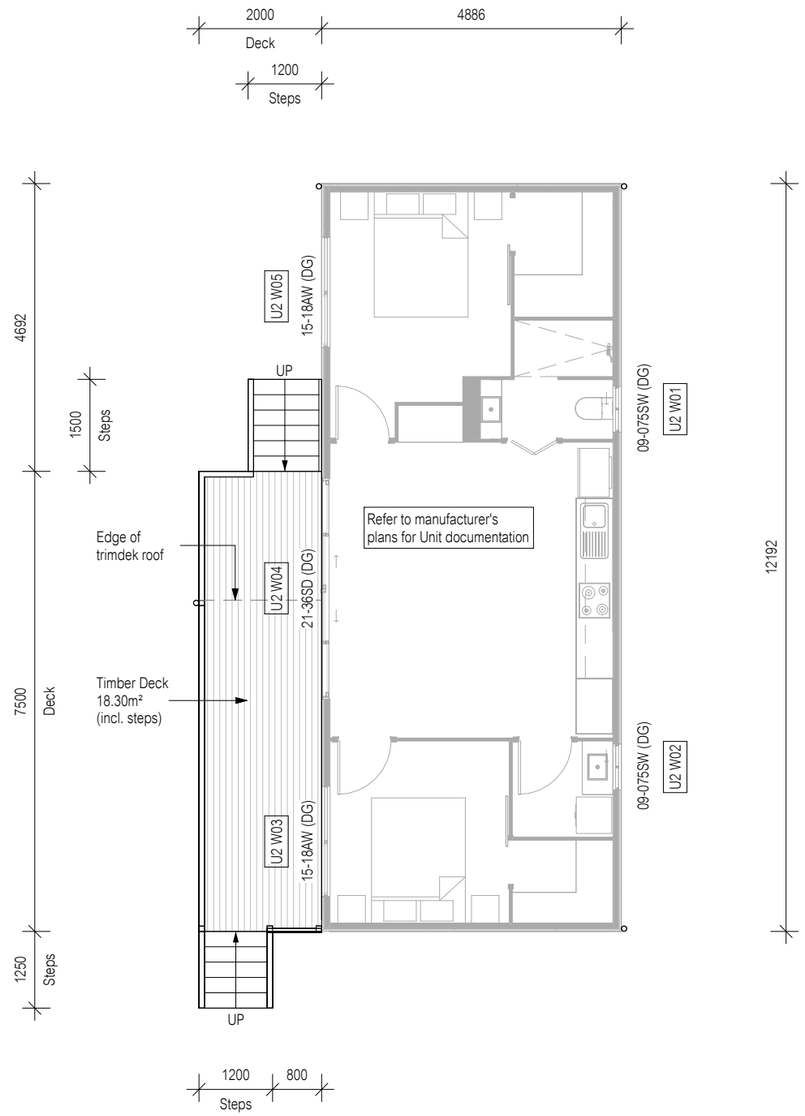
PROPOSED CUNIC UNIT DEVELOPMENT

168a Abbotsfield Road
CLAREMONT

CUNIC homes
Built for you

UNIT 1 ELEVATIONS

Drawn	ST	U249
Date	12 April 2024	Sheet 03/25
Scale	1 : 100	



GLENORCHY CITY COUNCIL
PLANNING SERVICES

APPLICATION No. : PLN-24-270

DATE RECEIVED: 06/11/2025

0 1 2 3 4m
1:100

Floor Area = Refer to Manufacturer's Documentation

Articulation joints

Smoke Alarm (interconnected where more than 1)

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PROPOSED CUNIC UNIT DEVELOPMENT

168a Abbotsfield Road
CLAREMONT

CUNIC homes
Built for you

UNIT 2 FLOOR PLAN

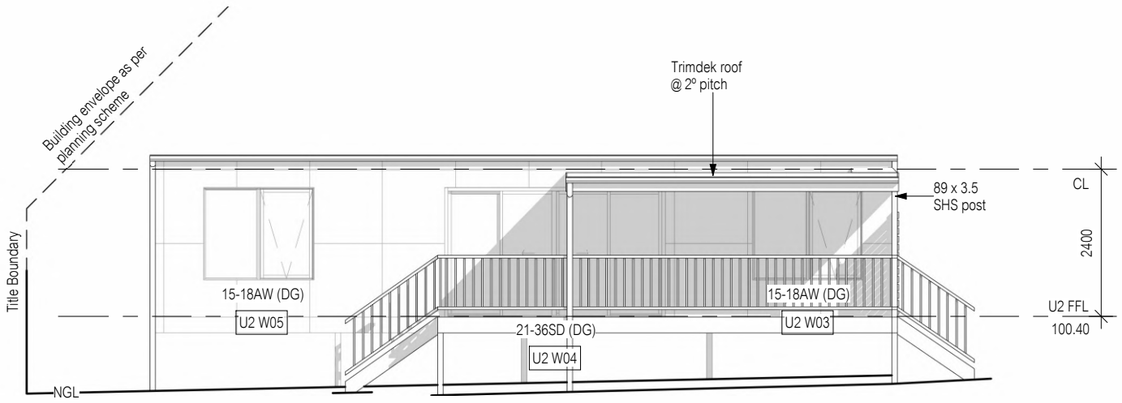
Drawn	ST	U249	
Date	13 June 2024	Sheet	04/25
Scale	1 : 100		

No.	Date	Int.
B	2 August 2024	ST

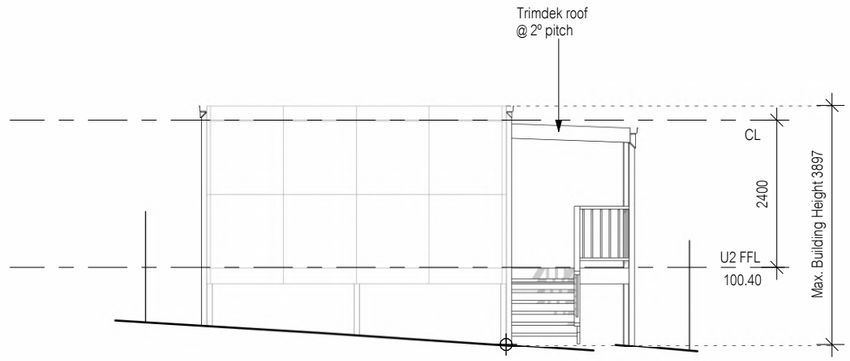
Amendment changes as per cover sheet

Material	Colour
Trimdek Roof	tbc

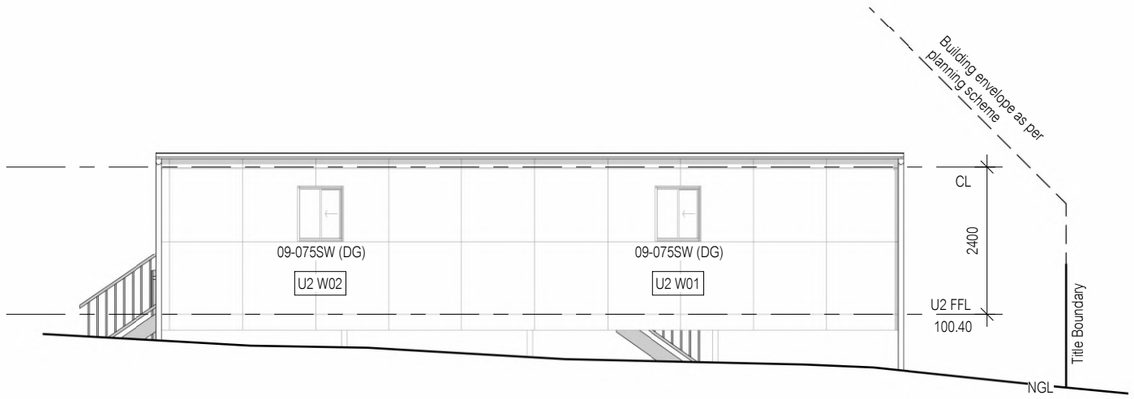
All lightweight cladding to be installed to manufacturer's guidelines. Refer to manufacturer's documentation.



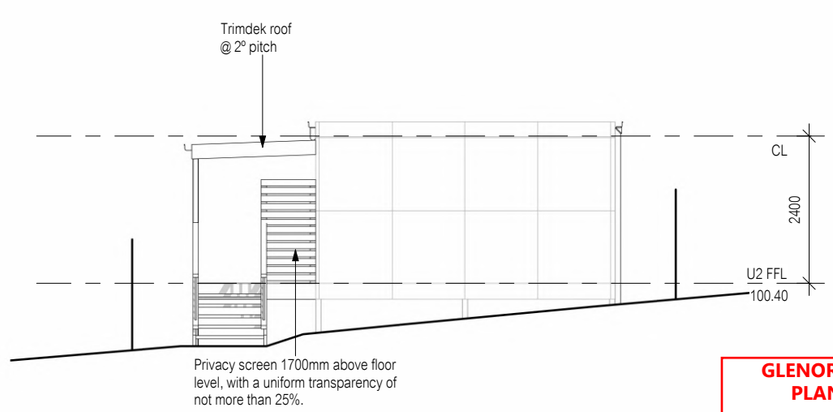
North Elevation



East Elevation



South Elevation



West Elevation

GLENORCHY CITY COUNCIL
PLANNING SERVICES

APPLICATION No. : PLN-24-270

DATE RECEIVED: 06/11/2025

No.	Date	Int.
B	2 August 2024	ST

Amendment changes as per cover sheet

All window sizes to be checked and/or confirmed on site prior to ordering glazing units

LEGEND:
AJ - Articulation Joint
BV - Brick Vent

Shadows shown for stylisation purposes only

Notes
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Client / Project info

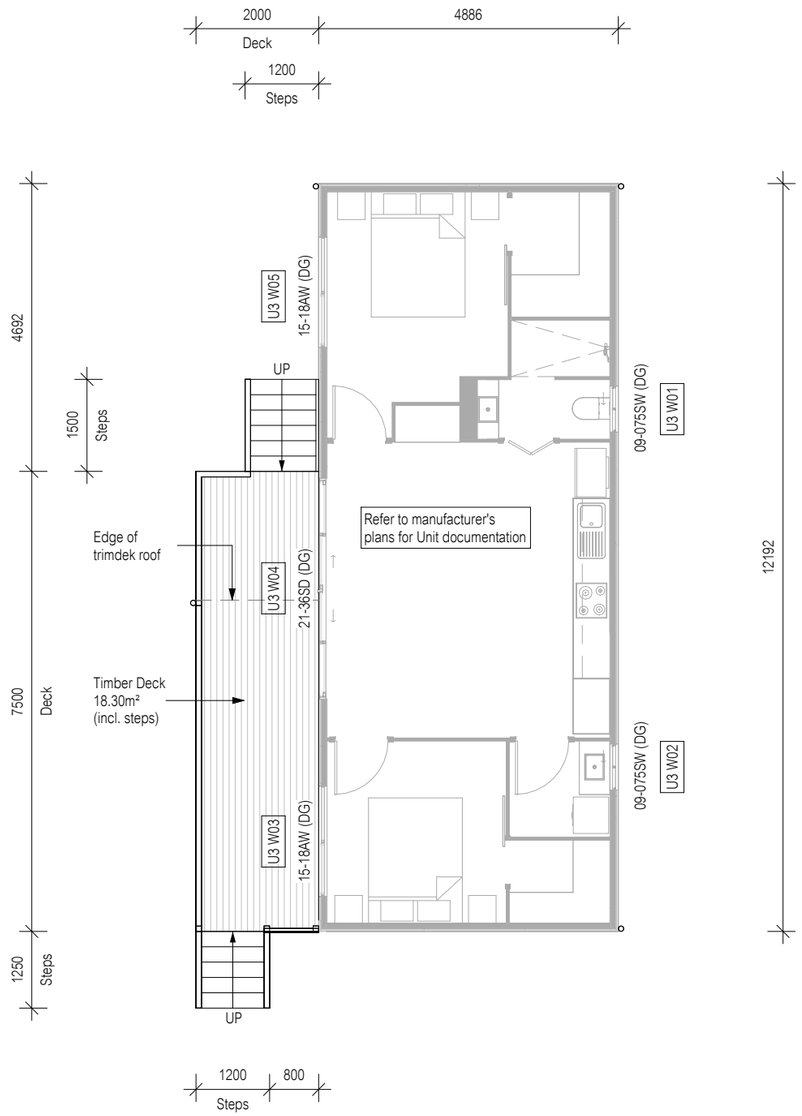
PROPOSED CUNIC UNIT DEVELOPMENT

168a Abbotsfield Road
CLAREMONT

CUNIC homes
Built for you

UNIT 2 ELEVATIONS

Drawn	ST	U249
Date	13 June 2024	Sheet 05/25
Scale	1 : 100	



GLENORCHY CITY COUNCIL
PLANNING SERVICES

APPLICATION No. : PLN-24-270

DATE RECEIVED: 06/11/2025

0 1 2 3 4m
1:100

Floor Area = Refer to Manufacturer's Documentation

Articulation joints

Smoke Alarm (interconnected where more than 1)

All window sizes to be checked and/or confirmed on site prior to ordering glazing units

No.	Date	Int.
B	2 August 2024	ST

Amendment changes as per cover sheet

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Client / Project info

PROPOSED CUNIC UNIT DEVELOPMENT

168a Abbotsfield Road
CLAREMONT

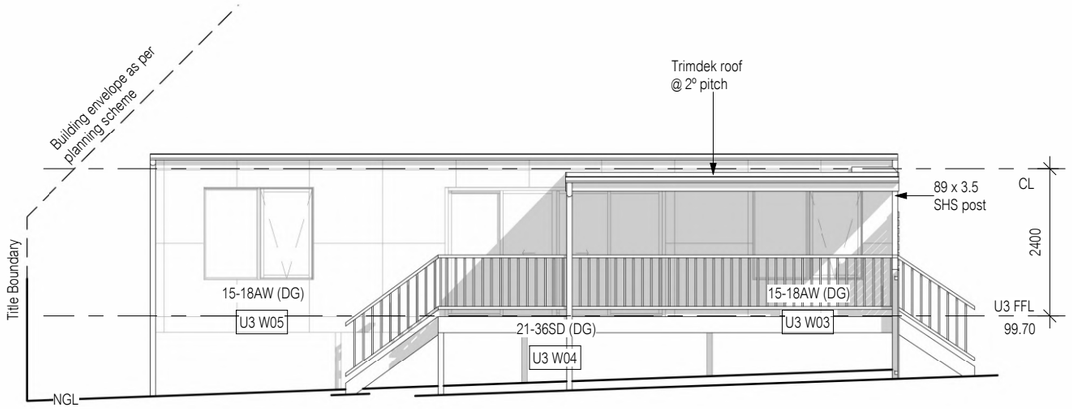
CUNIC homes
Built for you

UNIT 3 FLOOR PLAN

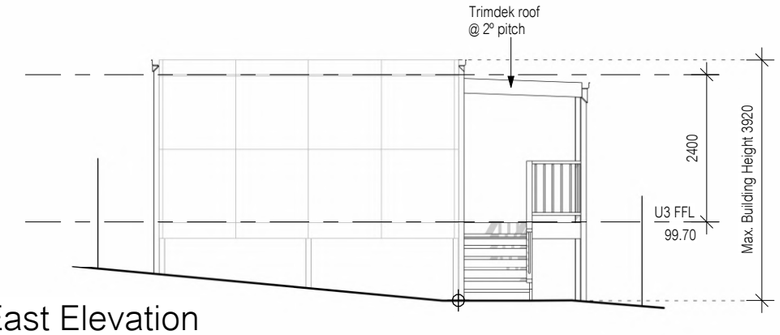
Drawn	ST	U249	
Date	13 June 2024	Sheet	06/25
Scale	1 : 100		

Material	Colour
Trimdek Roof	tbc

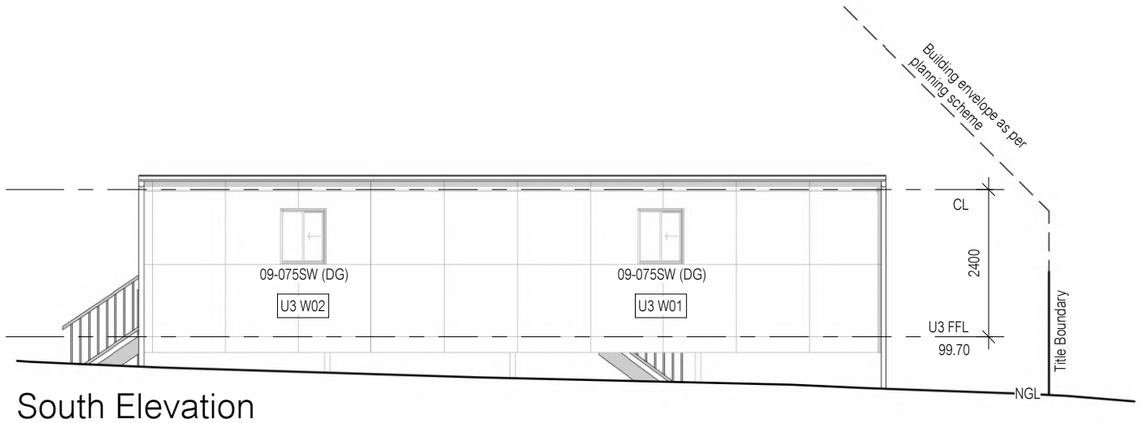
All lightweight cladding to be installed to manufacturer's guidelines. Refer to manufacturer's documentation.



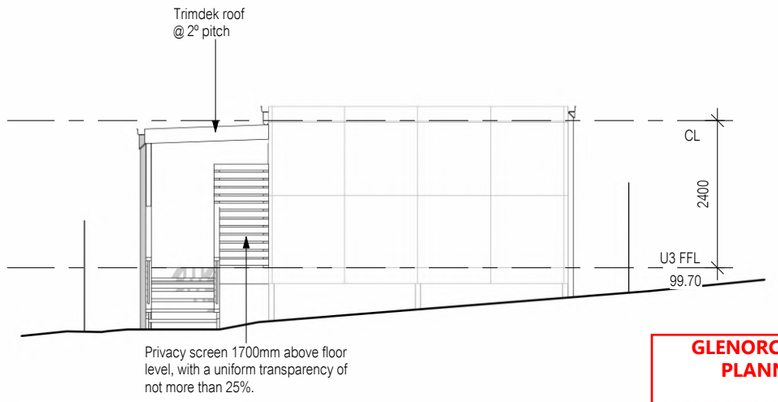
North Elevation



East Elevation



South Elevation



West Elevation

GLENORCHY CITY COUNCIL
PLANNING SERVICES

APPLICATION No. : PLN-24-270

DATE RECEIVED: 06/11/2025

No.	Date	Int.
B	2 August 2024	ST

Amendment changes as per cover sheet

All window sizes to be checked and/or confirmed on site prior to ordering glazing units

LEGEND:
AJ - Articulation Joint
BV - Brick Vent

Shadows shown for stylisation purposes only

Notes
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Client / Project info

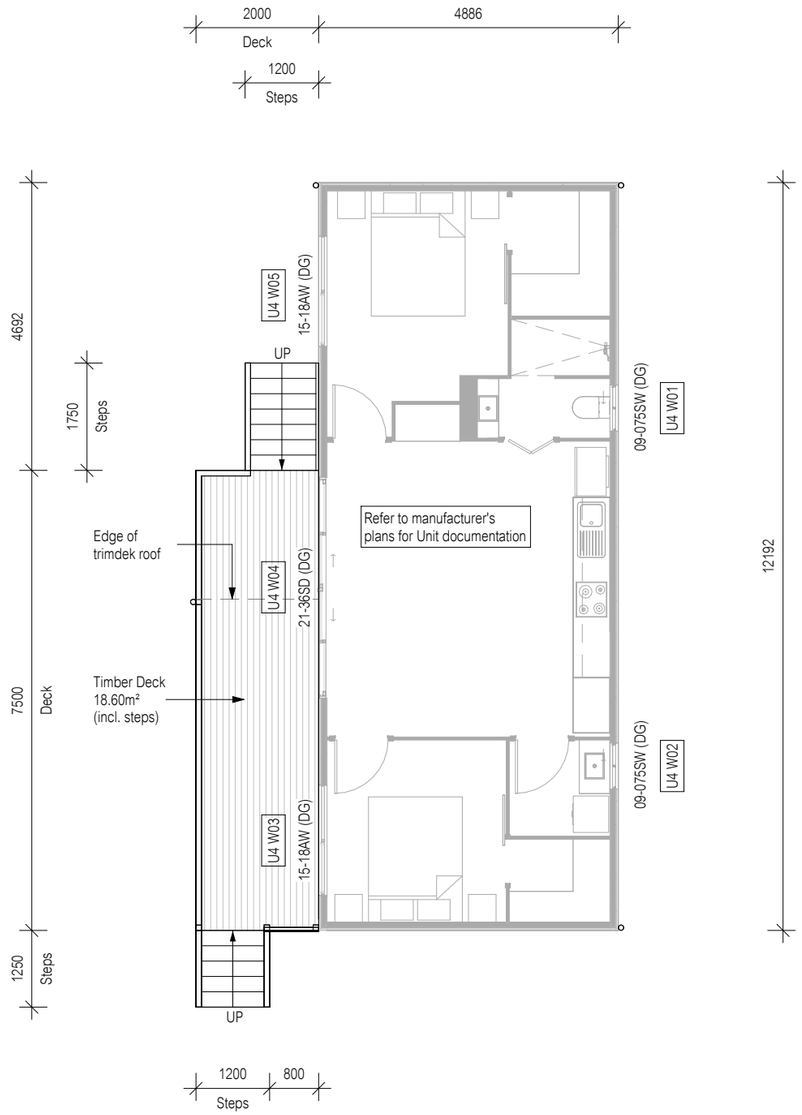
PROPOSED CUNIC UNIT DEVELOPMENT

168a Abbotsfield Road
CLAREMONT

CUNIC homes
Built for you

UNIT 3 ELEVATIONS

Drawn	ST	U249
Date	13 June 2024	Sheet
Scale	1 : 100	07/25



GLENORCHY CITY COUNCIL
PLANNING SERVICES

APPLICATION No. : PLN-24-270

DATE RECEIVED: 06/11/2025

0 1 2 3 4m
1:100

Floor Area = Refer to Manufacturer's Documentation

Articulation joints

Smoke Alarm (interconnected where more than 1)

All window sizes to be checked and/or confirmed on site prior to ordering glazing units

No.	Date	Int.
B	2 August 2024	ST

Amendment changes as per cover sheet

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PROPOSED CUNIC UNIT DEVELOPMENT

168a Abbotsfield Road
CLAREMONT

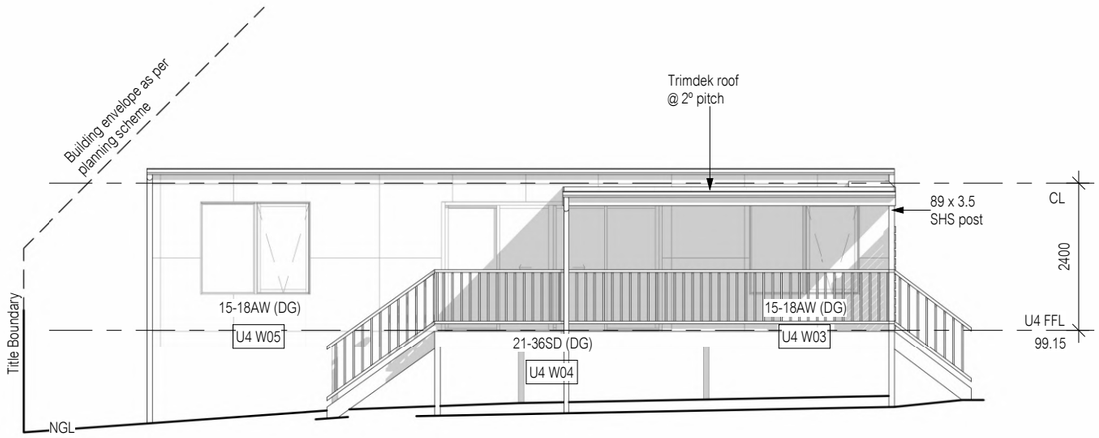
CUNIC homes
Built for you

UNIT 4 FLOOR PLAN

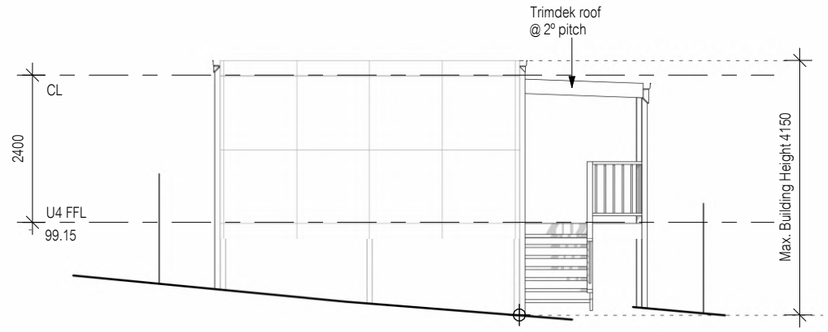
Drawn	ST	U249	
Date	13 June 2024	Sheet	08/25
Scale	1 : 100		

Material	Colour
Trimdek Roof	tbc

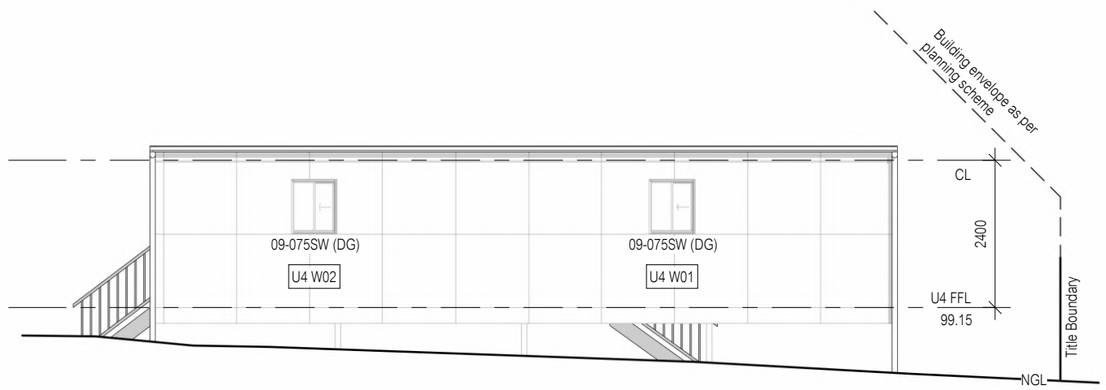
All lightweight cladding to be installed to manufacturer's guidelines. Refer to manufacturer's documentation.



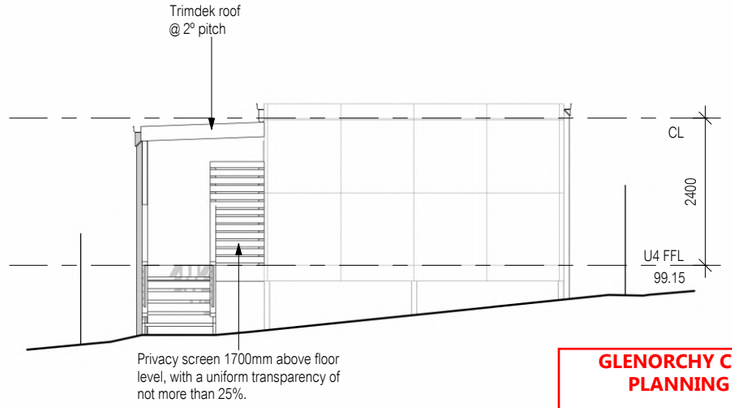
North Elevation



East Elevation



South Elevation



West Elevation

GLENORCHY CITY COUNCIL PLANNING SERVICES

APPLICATION No. : PLN-24-270

DATE RECEIVED: 06/11/2025

No.	Date	Int.
B	2 August 2024	ST

Amendment changes as per cover sheet

All window sizes to be checked and/or confirmed on site prior to ordering glazing units

LEGEND:
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BV - Brick Vent

Shadows shown for stylisation purposes only

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Client / Project info

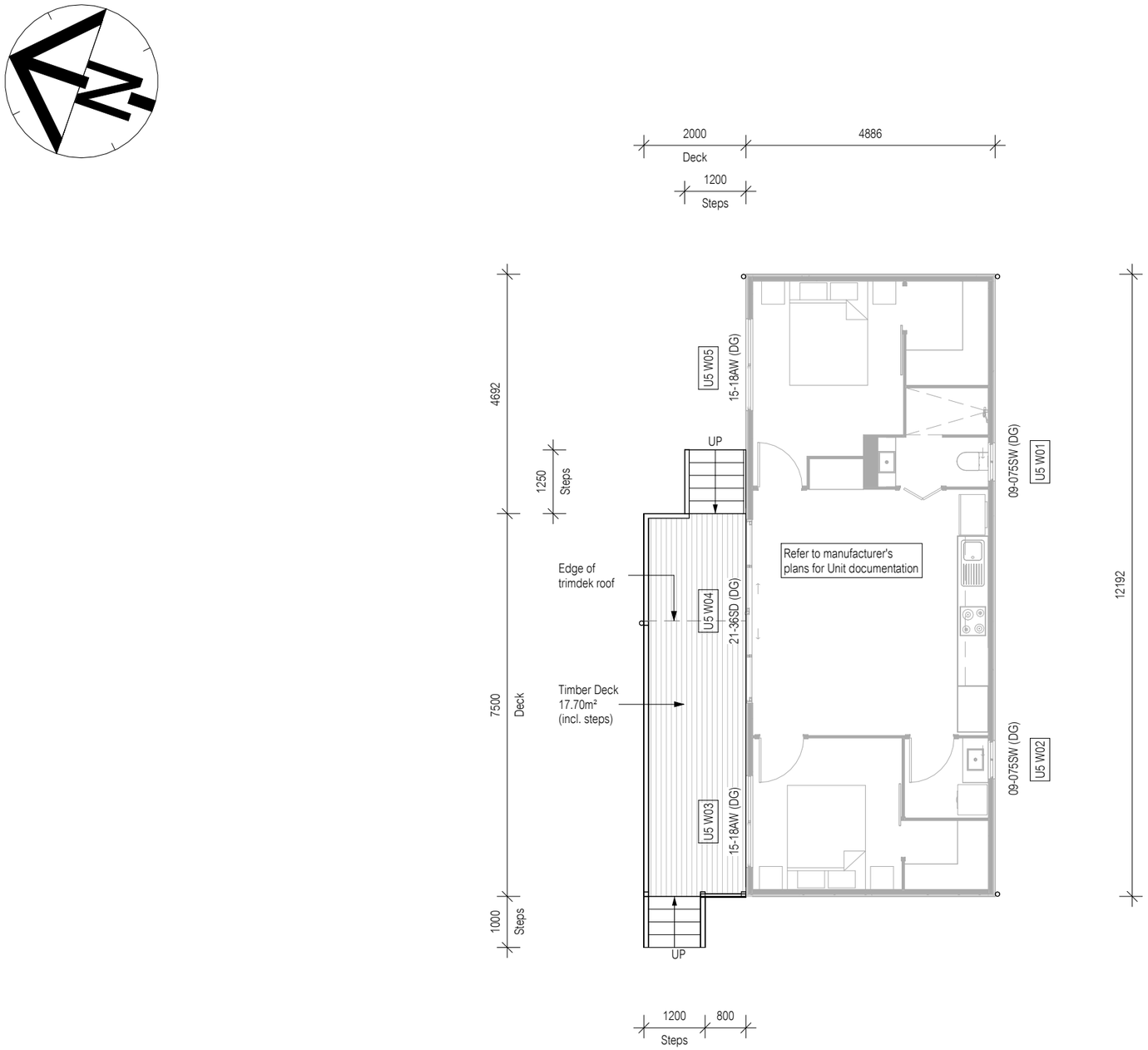
PROPOSED CUNIC UNIT DEVELOPMENT

168a Abbotsfield Road
CLAREMONT

CUNIC homes
Built for you

UNIT 4 ELEVATIONS

Drawn	ST	U249
Date	13 June 2024	Sheet 09/25
Scale	1 : 100	



GLENORCHY CITY COUNCIL
PLANNING SERVICES

APPLICATION No. : PLN-24-270

DATE RECEIVED: 06/11/2025

0 1 2 3 4m
1:100

Floor Area = Refer to Manufacturer's Documentation

Articulation joints

Smoke Alarm (interconnected where more than 1)

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No.	Date	Int.
B	2 August 2024	ST

Amendment changes as per cover sheet

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PROPOSED CUNIC UNIT DEVELOPMENT

168a Abbotsfield Road
CLAREMONT

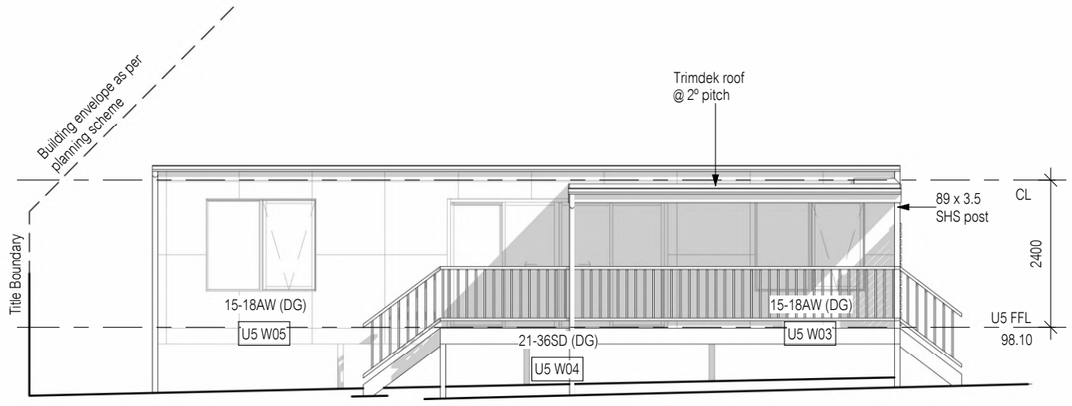
CUNIC homes
Built for you

UNIT 5 FLOOR PLAN

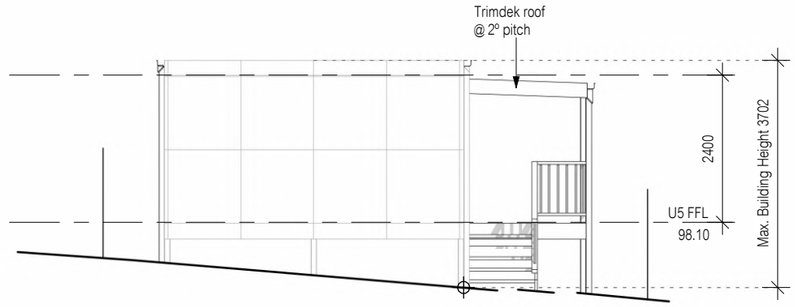
Drawn	ST	U249	
Date	13 June 2024	Sheet	10/25
Scale	1 : 100		

Material	Colour
Trimdek Roof	tbc

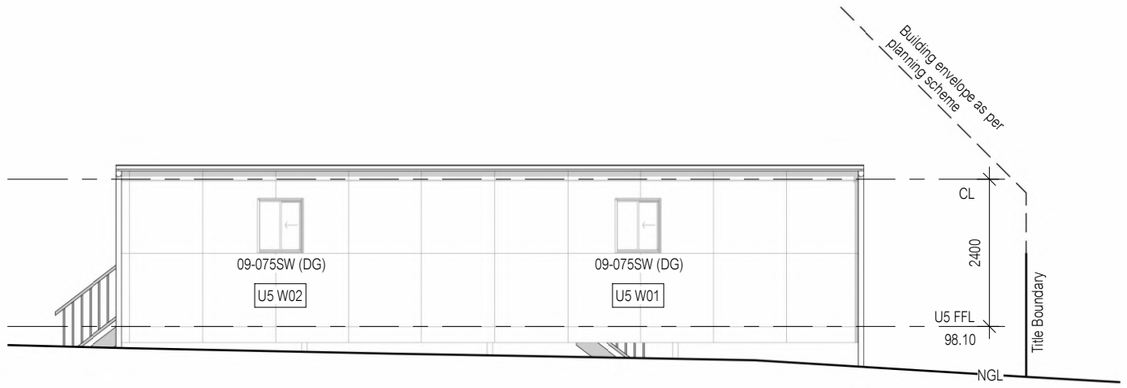
All lightweight cladding to be installed to manufacturer's guidelines. Refer to manufacturer's documentation.



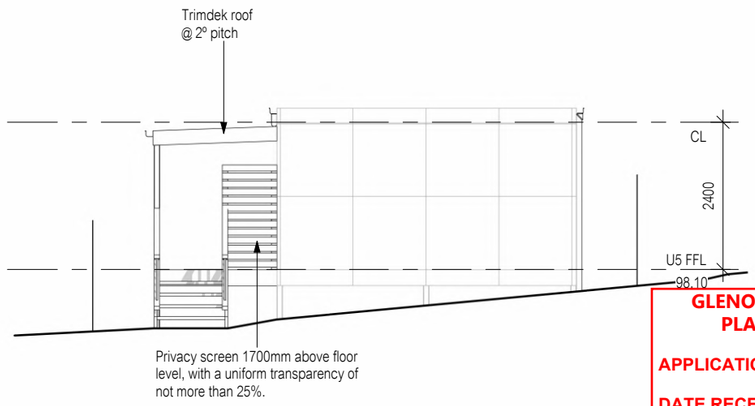
North Elevation



East Elevation



South Elevation



West Elevation

GLENORCHY CITY COUNCIL
PLANNING SERVICES
APPLICATION No. : PLN-24-270
DATE RECEIVED: 06/11/2025

No.	Date	Int.
B	2 August 2024	ST

Amendment changes as per cover sheet

All window sizes to be checked and/or confirmed on site prior to ordering glazing units

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BV - Brick Vent

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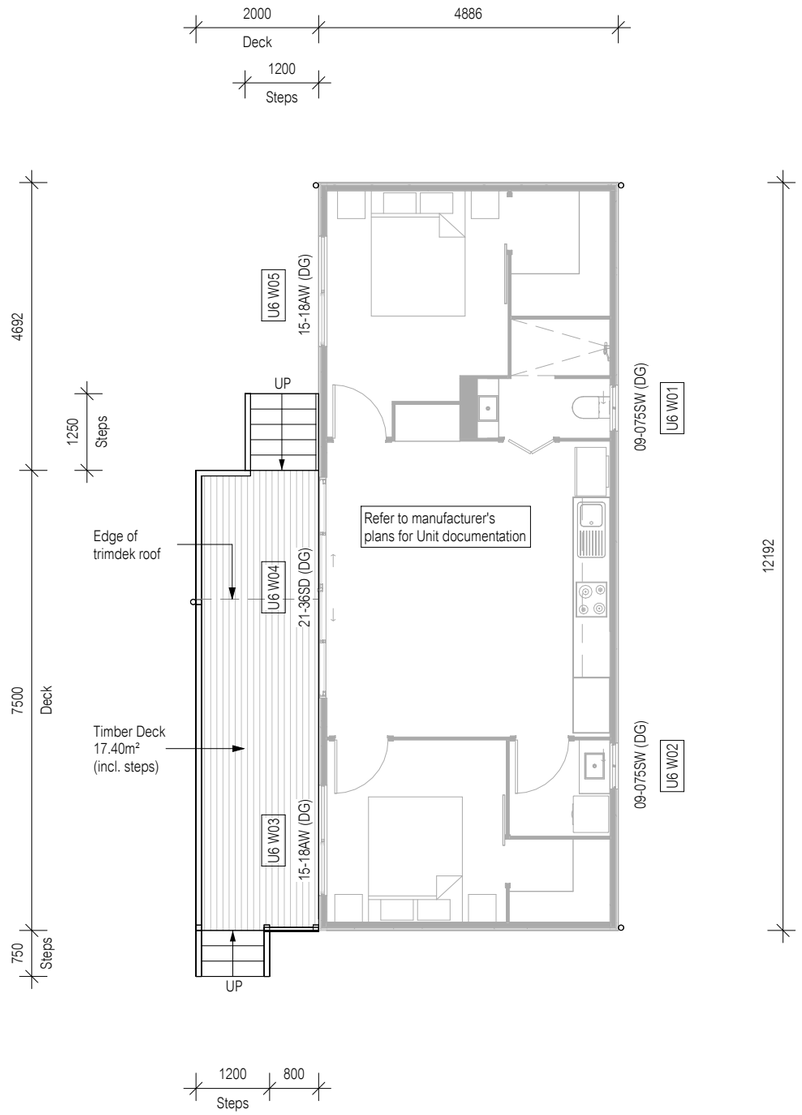
PROPOSED CUNIC UNIT DEVELOPMENT

168a Abbotsfield Road
CLAREMONT

CUNIC homes
Built for you

UNIT 5 ELEVATIONS

Drawn	ST	U249
Date	13 June 2024	Sheet 11/25
Scale	1 : 100	



GLENORCHY CITY COUNCIL
PLANNING SERVICES

APPLICATION No. : PLN-24-270

DATE RECEIVED: 06/11/2025

0 1 2 3 4m
1:100

Floor Area = Refer to Manufacturer's Documentation

Articulation joints

Smoke Alarm (interconnected where more than 1)

All window sizes to be checked and/or confirmed on site prior to ordering glazing units

No.	Date	Int.
B	2 August 2024	ST

Amendment changes as per cover sheet

Notes
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- Do not scale from these drawings.
- No changes permitted without consultation with designer.

Designer:

ANOTHER PERSPECTIVE PTY LTD
PO BOX 21
NEW TOWN
LIC. NO. 685230609 (S. Turvey)
Ph: (03) 6231 4122
Fx: (03) 6231 4166
Email:
info@anotherperspective.com.au

Client / Project info

PROPOSED CUNIC UNIT DEVELOPMENT

168a Abbotsfield Road
CLAREMONT

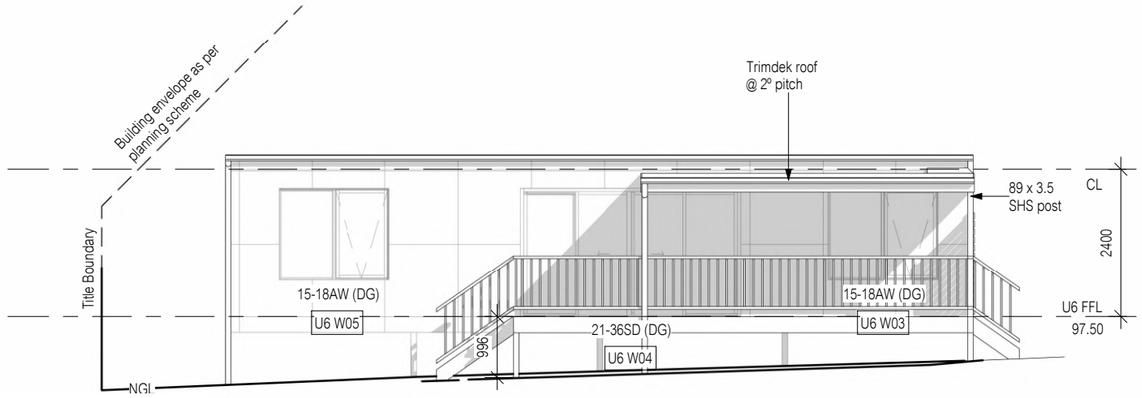
CUNIC homes
Built for you

UNIT 6 FLOOR PLAN

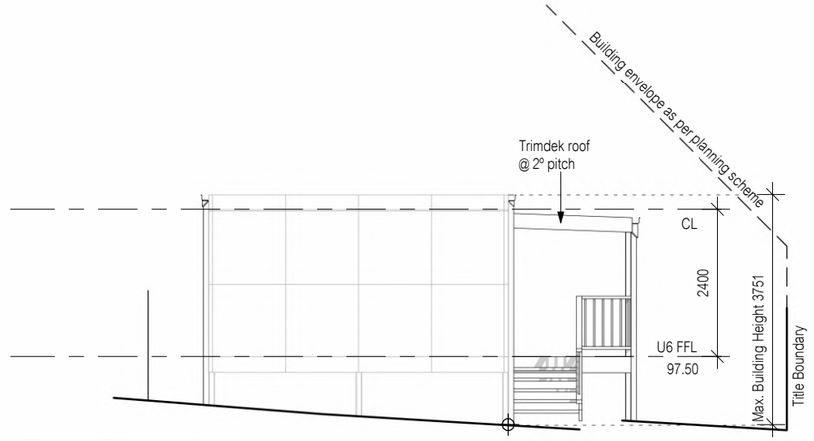
Drawn	ST	U249	
Date	13 June 2024	Sheet	12/25
Scale	1 : 100		

Material	Colour
Trimdek Roof	tbc

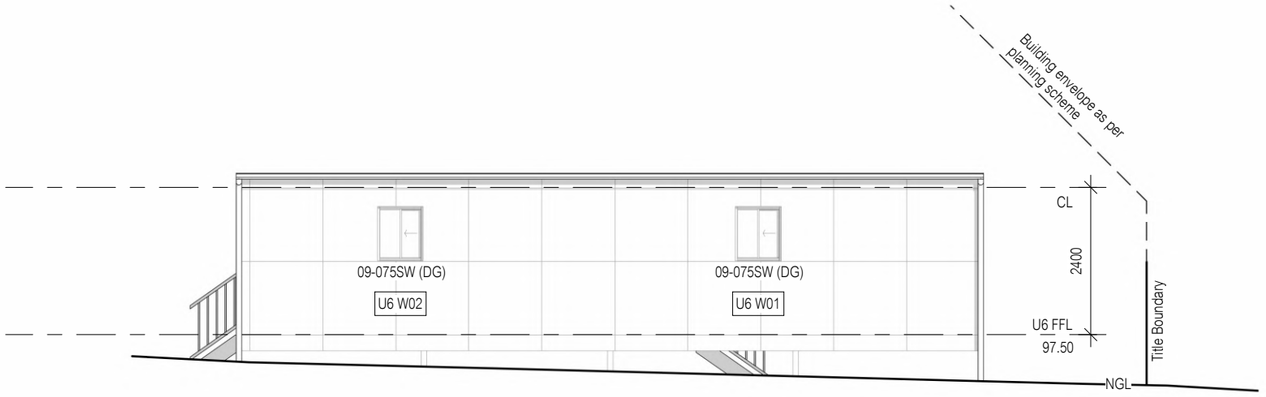
All lightweight cladding to be installed to manufacturer's guidelines. Refer to manufacturer's documentation.



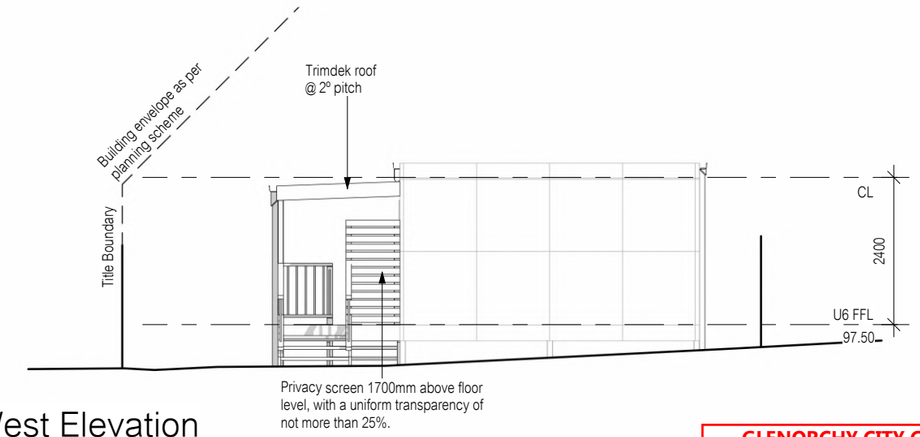
North Elevation



East Elevation



South Elevation



West Elevation

GLENORCHY CITY COUNCIL
PLANNING SERVICES

APPLICATION No. : PLN-24-270

DATE RECEIVED: 06/11/2025

No.	Date	Int.
B	2 August 2024	ST

Amendment changes as per cover sheet

All window sizes to be checked and/or confirmed on site prior to ordering glazing units

LEGEND:
AJ - Articulation Joint
BV - Brick Vent

Shadows shown for stylisation purposes only

Notes
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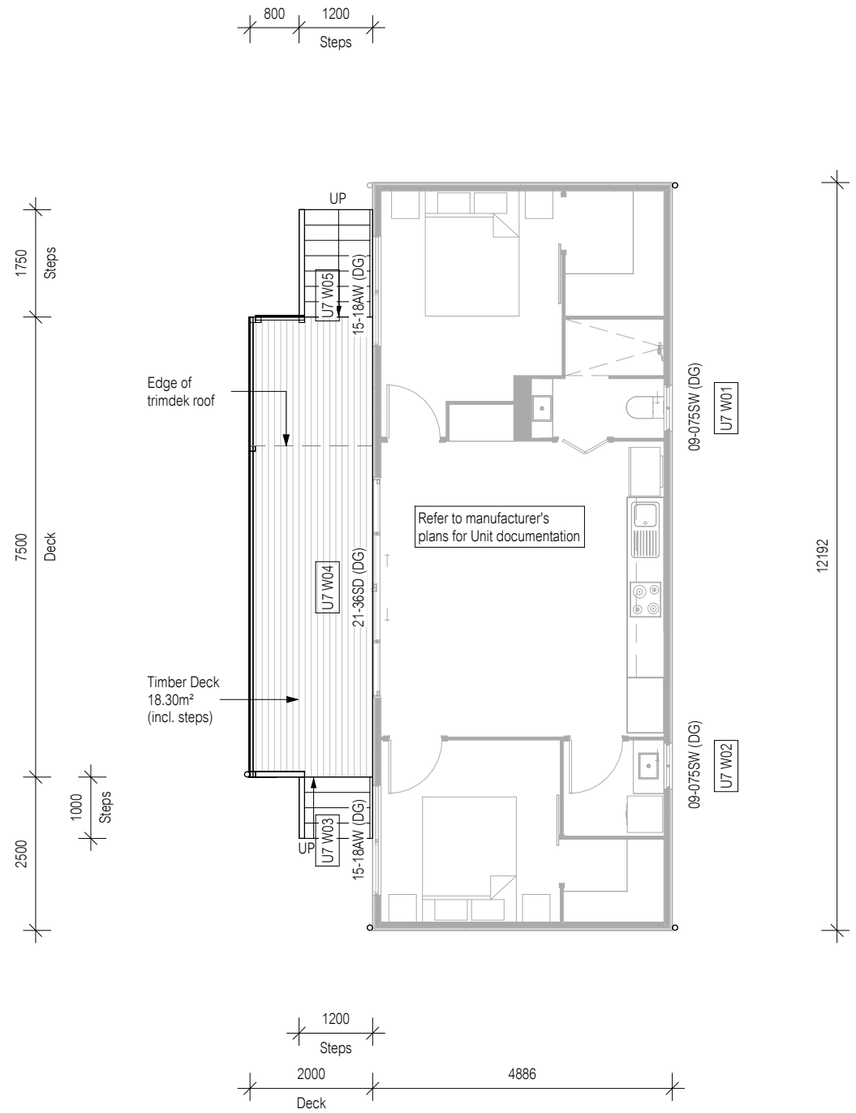
PROPOSED CUNIC UNIT DEVELOPMENT

168a Abbotsfield Road
CLAREMONT

CUNIC homes
Built for you

UNIT 6 ELEVATIONS

Drawn	ST	U249
Date	13 June 2024	Sheet
Scale	1 : 100	13/25



GLENORCHY CITY COUNCIL
PLANNING SERVICES

APPLICATION No. : PLN-24-270

DATE RECEIVED: 06/11/2025

0 1 2 3 4m
1:100

Floor Area = Refer to Manufacturer's Documentation

Articulation joints

Smoke Alarm (interconnected where more than 1)

All window sizes to be checked and/or confirmed on site prior to ordering glazing units

No.	Date	Int.
B	2 August 2024	ST

Amendment changes as per cover sheet

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PROPOSED CUNIC UNIT DEVELOPMENT

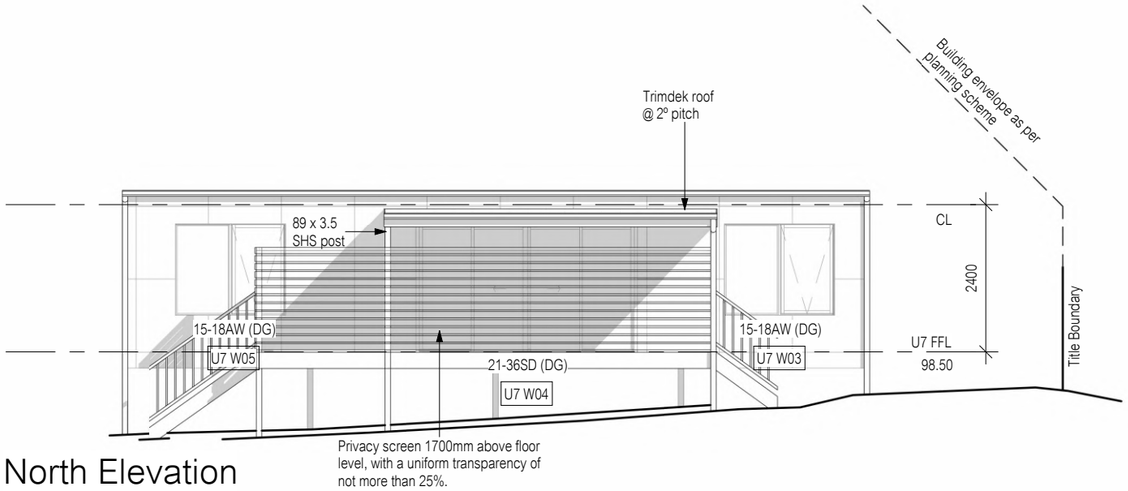
168a Abbotsfield Road
CLAREMONT

CUNIC homes
Built for you

UNIT 7 FLOOR PLAN

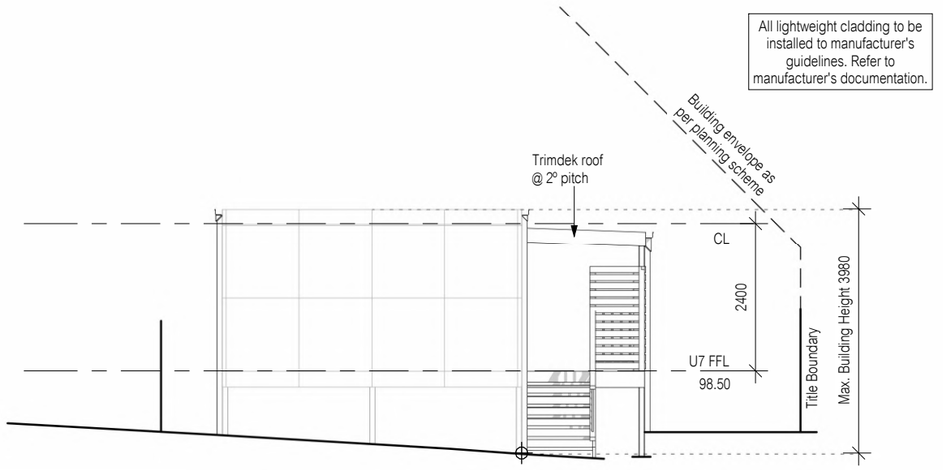
Drawn	ST	U249	
Date	13 June 2024	Sheet	14/25
Scale	1 : 100		

Material	Colour
Trimdek Roof	tbc

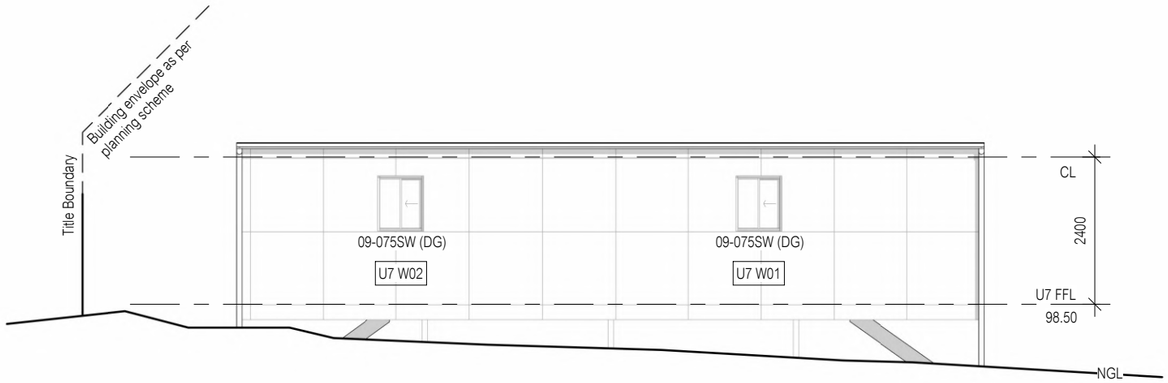


North Elevation

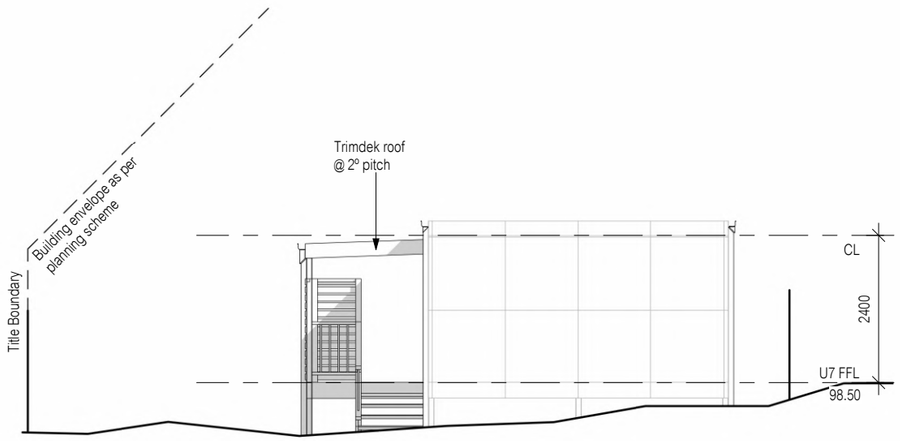
All lightweight cladding to be installed to manufacturer's guidelines. Refer to manufacturer's documentation.



East Elevation



South Elevation



West Elevation

GLENORCHY CITY COUNCIL
PLANNING SERVICES

APPLICATION No. : PLN-24-270

DATE RECEIVED: 06/11/2025

No.	Date	Int.
B	2 August 2024	ST

Amendment changes as per cover sheet

All window sizes to be checked and/or confirmed on site prior to ordering glazing units

LEGEND:
AJ - Articulation Joint
BV - Brick Vent

Shadows shown for stylisation purposes only

Notes
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PROPOSED CUNIC UNIT DEVELOPMENT

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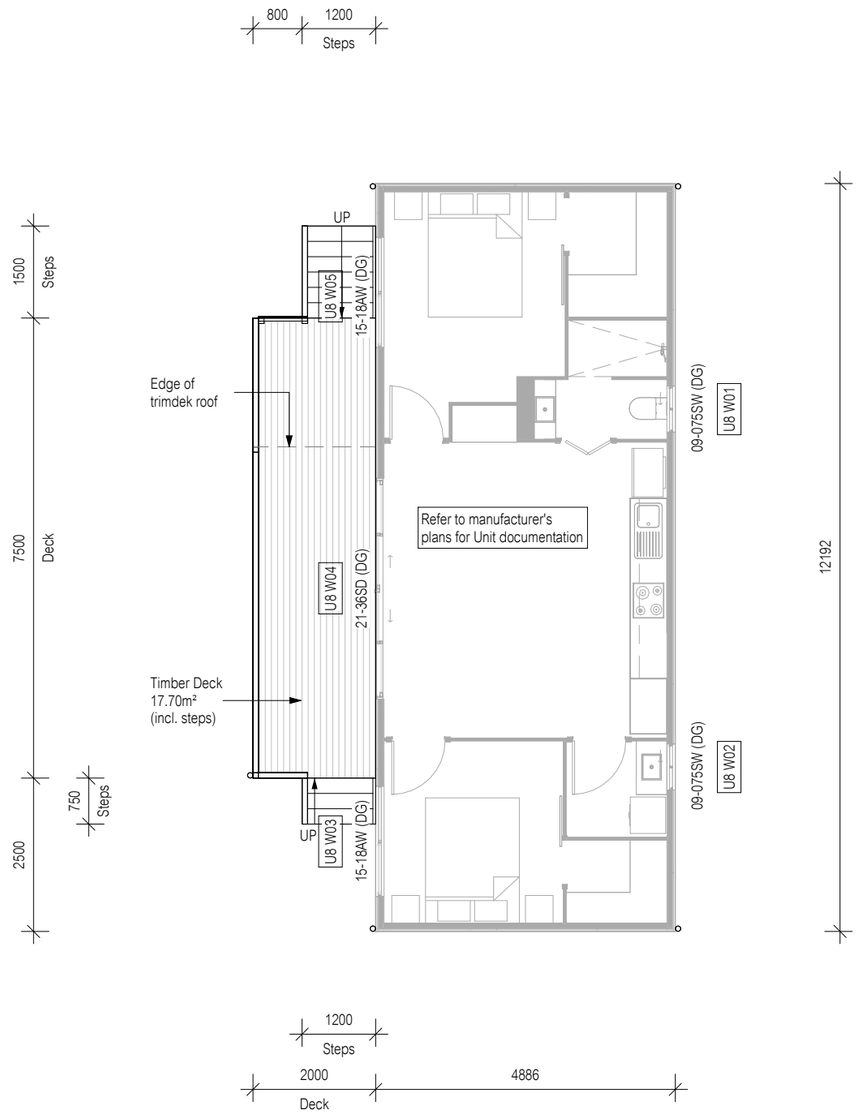
CLAREMONT

CUNIC homes
Built for you.

UNIT 7 ELEVATIONS

Drawn	ST	U249
Date	13 June 2024	Sheet 15/25
Scale	1 : 100	



GLENORCHY CITY COUNCIL
PLANNING SERVICES

APPLICATION No. : PLN-24-270

DATE RECEIVED: 06/11/2025

0 1 2 3 4m
1:100

Floor Area = Refer to Manufacturer's Documentation

Articulation joints

Smoke Alarm (interconnected where more than 1)

All window sizes to be checked and/or confirmed on site prior to ordering glazing units

No.	Date	Int.
B	2 August 2024	ST

Amendment changes as per cover sheet

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Client / Project info

PROPOSED CUNIC UNIT DEVELOPMENT

168a Abbotsfield Road
CLAREMONT

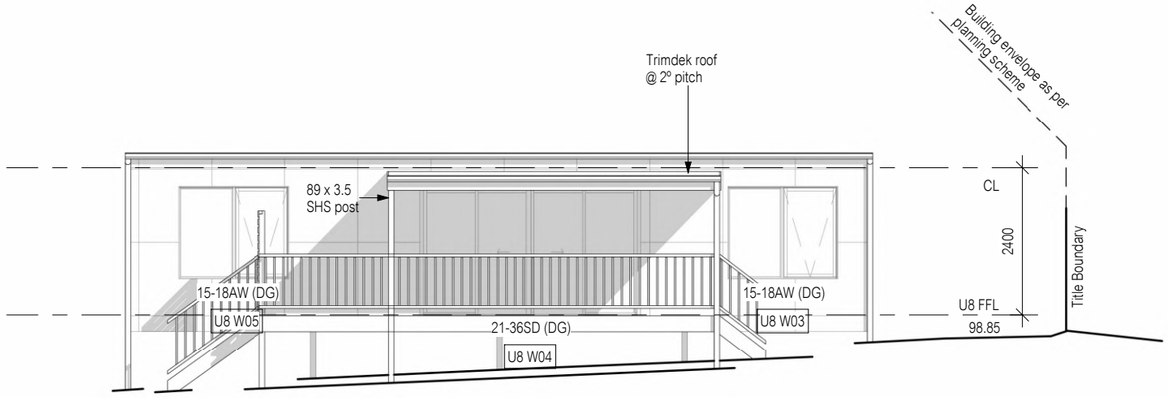
CUNIC homes
Built for you

UNIT 8 FLOOR PLAN

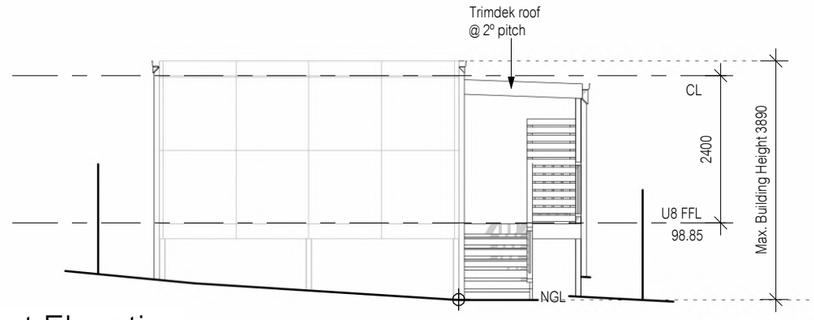
Drawn	ST	U249	
Date	13 June 2024	Sheet	16/25
Scale	1 : 100		

Material	Colour
Trimdek Roof	tbc

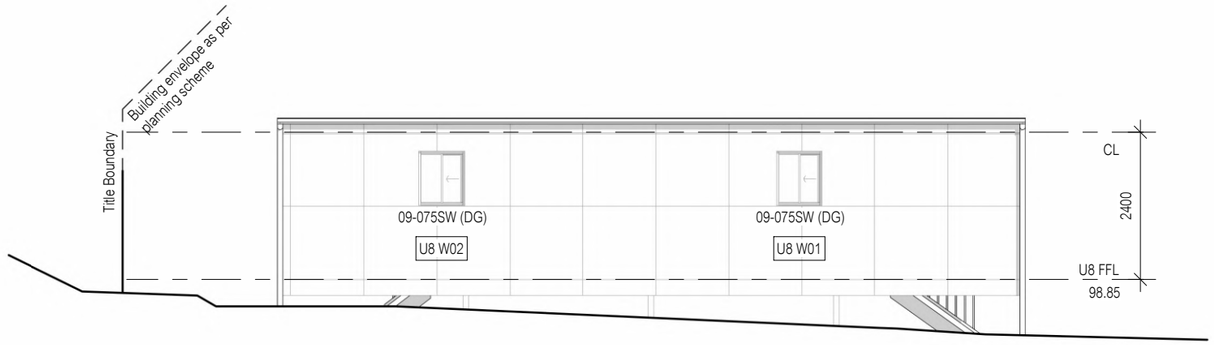
All lightweight cladding to be installed to manufacturer's guidelines. Refer to manufacturer's documentation.



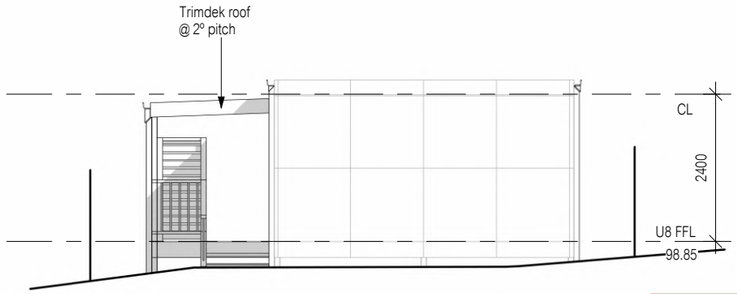
North Elevation



East Elevation



South Elevation



West Elevation

GLENORCHY CITY COUNCIL
PLANNING SERVICES

APPLICATION No. : PLN-24-270

DATE RECEIVED: 06/11/2025

No.	Date	Int.
B	2 August 2024	ST

Amendment changes as per cover sheet

All window sizes to be checked and/or confirmed on site prior to ordering glazing units

LEGEND:
AJ - Articulation Joint
BV - Brick Vent

Shadows shown for stylisation purposes only

Notes
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Client / Project info

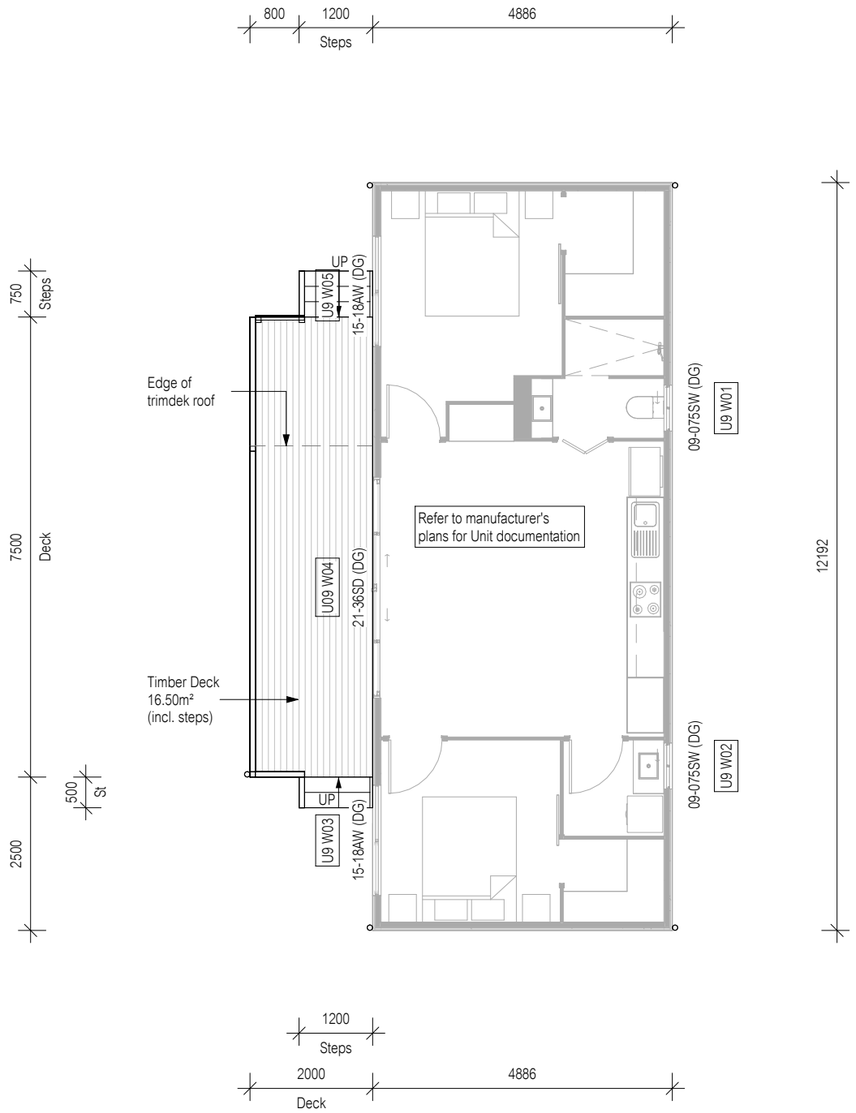
PROPOSED CUNIC UNIT DEVELOPMENT

168a Abbotsfield Road
CLAREMONT

CUNIC homes
Built for you

UNIT 8 ELEVATIONS

Drawn	ST	U249
Date	13 June 2024	Sheet
Scale	1 : 100	17/25



GLENORCHY CITY COUNCIL
PLANNING SERVICES

APPLICATION No. : PLN-24-270

DATE RECEIVED: 06/11/2025

0 1 2 3 4m
1:100

Floor Area = Refer to Manufacturer's Documentation

Articulation joints

Smoke Alarm (interconnected where more than 1)

All window sizes to be checked and/or confirmed on site prior to ordering glazing units

No.	Date	Int.
B	2 August 2024	ST

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Client / Project info

PROPOSED CUNIC UNIT DEVELOPMENT

168a Abbotsfield Road
CLAREMONT

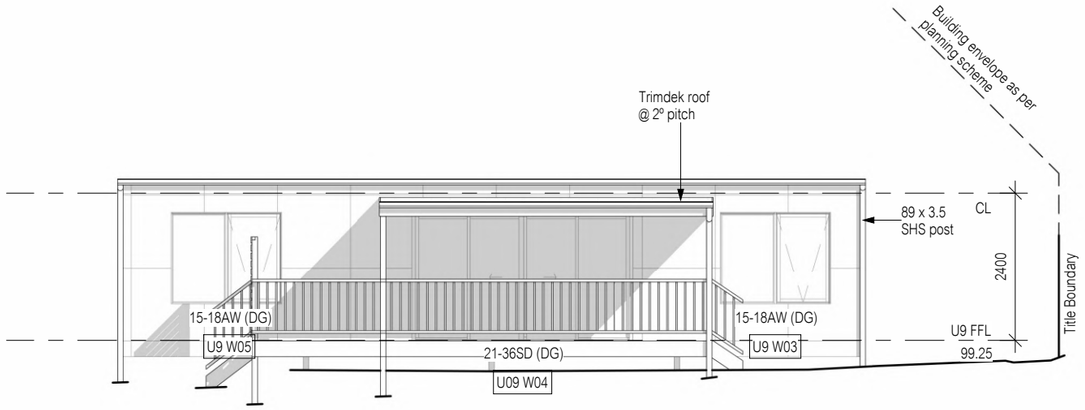
CUNIC homes
Built for you

UNIT 9 FLOOR PLAN

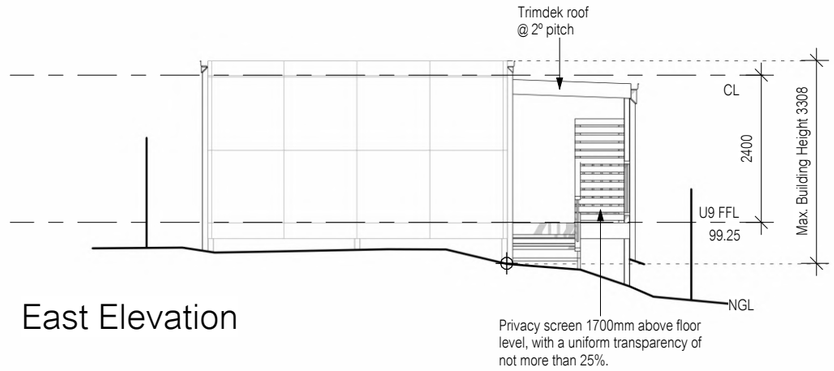
Drawn	ST	U249
Date	13 June 2024	Sheet
Scale	1 : 100	18/25
	Copyright ©	

Material	Colour
Trimdek Roof	tbc

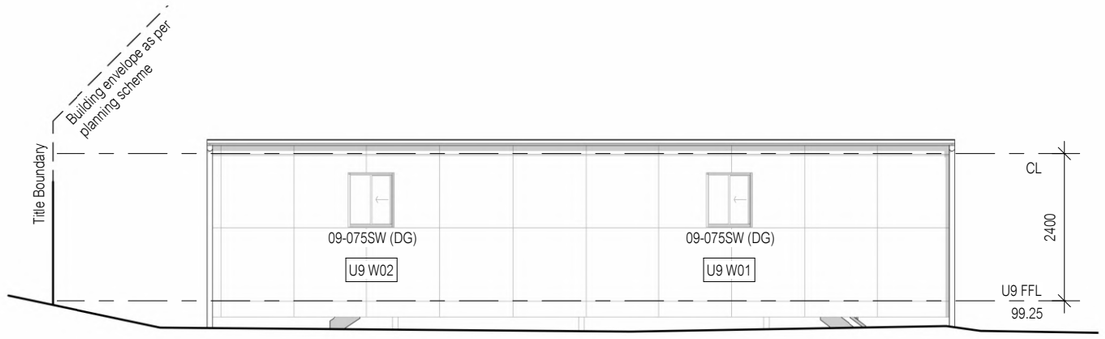
All lightweight cladding to be installed to manufacturer's guidelines. Refer to manufacturer's documentation.



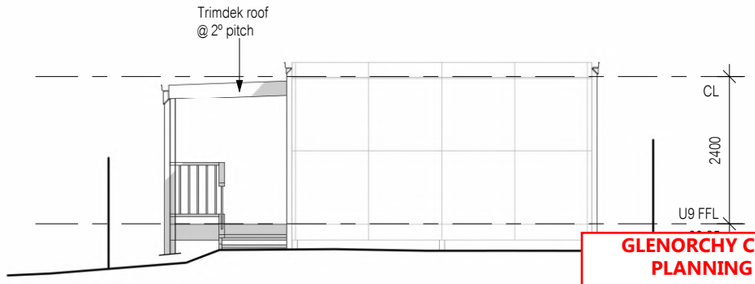
North Elevation



East Elevation



South Elevation



West Elevation

GLENORCHY CITY COUNCIL
PLANNING SERVICES

APPLICATION No. : PLN-24-270

DATE RECEIVED: 06/11/2025

No.	Date	Int.
B	2 August 2024	ST

Amendment changes as per cover sheet

All window sizes to be checked and/or confirmed on site prior to ordering glazing units

LEGEND:
AJ - Articulation Joint
BV - Brick Vent

Shadows shown for stylisation purposes only

Notes
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Client / Project info

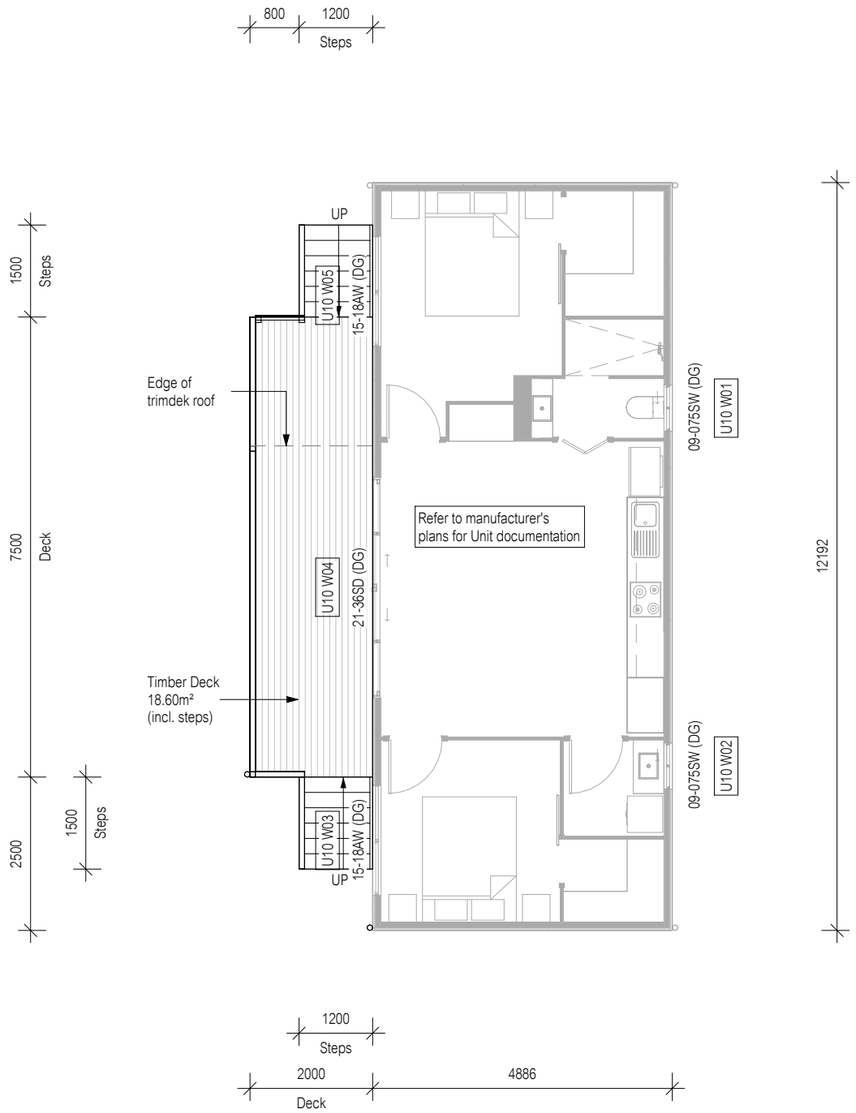
PROPOSED CUNIC UNIT DEVELOPMENT

168a Abbotsfield Road
CLAREMONT

CUNIC homes
Built for you

UNIT 9 ELEVATIONS

Drawn	ST	U249
Date	13 June 2024	Sheet
Scale	1 : 100	19/25



GLENORCHY CITY COUNCIL
PLANNING SERVICES

APPLICATION No. : PLN-24-270

DATE RECEIVED: 06/11/2025

0 1 2 3 4m
1:100

Floor Area = Refer to Manufacturer's Documentation

Articulation joints

Smoke Alarm (interconnected where more than 1)

All window sizes to be checked and/or confirmed on site prior to ordering glazing units

No.	Date	Int.
B	2 August 2024	ST

Amendment changes as per cover sheet

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PROPOSED CUNIC UNIT DEVELOPMENT

168a Abbotsfield Road
CLAREMONT

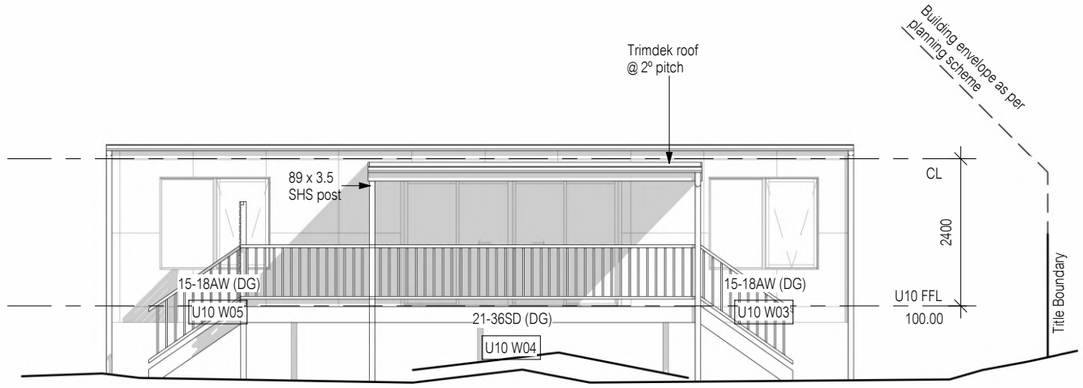
CUNIC homes
Built for you

UNIT 10 FLOOR PLAN

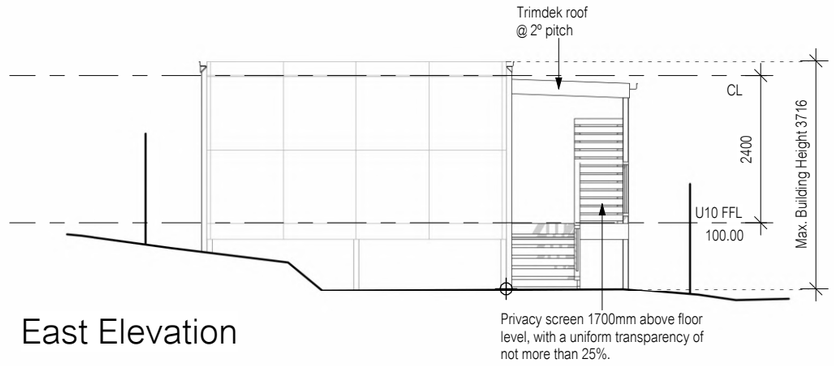
Drawn	ST	U249	
Date	13 June 2024	Sheet	20/25
Scale	1 : 100		

Material	Colour
Trimdek Roof	tbc

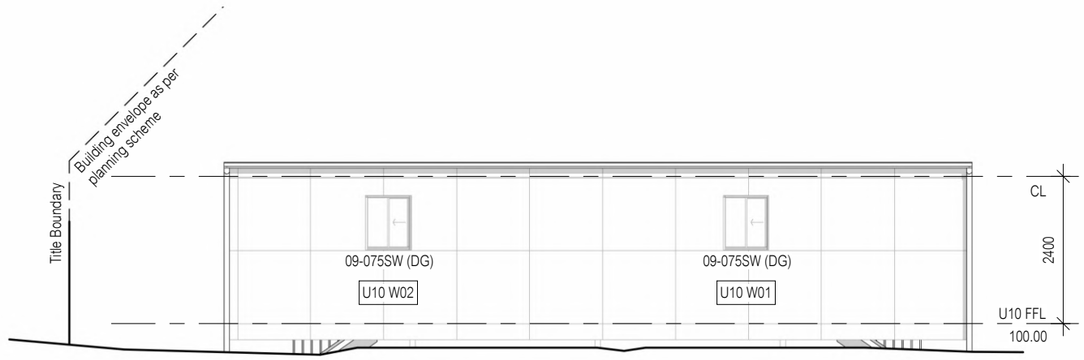
All lightweight cladding to be installed to manufacturer's guidelines. Refer to manufacturer's documentation.



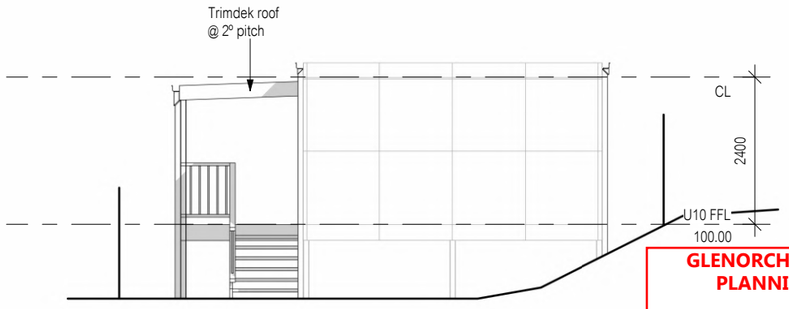
North Elevation



East Elevation



South Elevation



West Elevation

GLENORCHY CITY COUNCIL
PLANNING SERVICES

APPLICATION No. : PLN-24-270

DATE RECEIVED: 06/11/2025

No.	Date	Int.
B	2 August 2024	ST

Amendment changes as per cover sheet

All window sizes to be checked and/or confirmed on site prior to ordering glazing units

LEGEND:
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BV - Brick Vent

Shadows shown for stylisation purposes only

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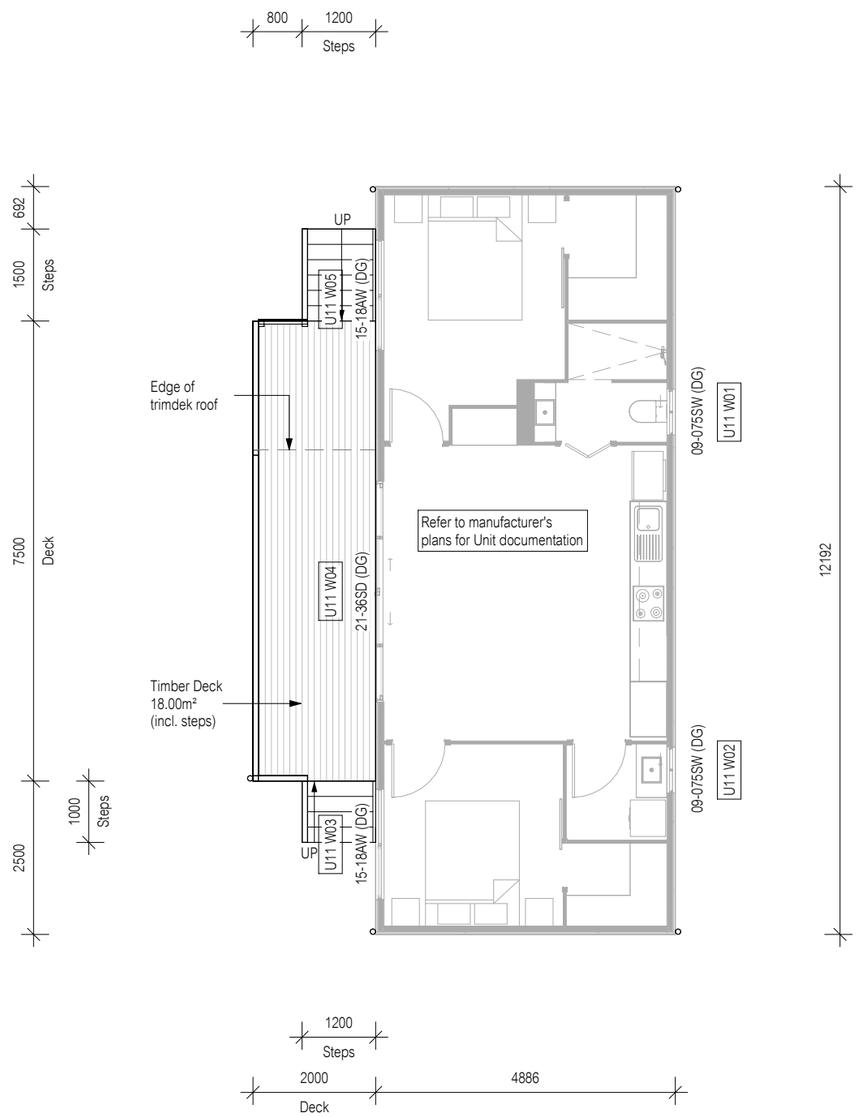
PROPOSED CUNIC UNIT DEVELOPMENT

168a Abbotsfield Road
CLAREMONT

CUNIC homes
Built for you

UNIT 10 ELEVATIONS

Drawn	ST	U249
Date	13 June 2024	Sheet
Scale	1 : 100	21/25



GLENORCHY CITY COUNCIL
PLANNING SERVICES

APPLICATION No. : PLN-24-270

DATE RECEIVED: 06/11/2025

0 1 2 3 4m
1:100

Floor Area = Refer to Manufacturer's Documentation

Articulation joints

Smoke Alarm (interconnected where more than 1)

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PROPOSED CUNIC UNIT DEVELOPMENT

168a Abbotsfield Road
CLAREMONT

CUNIC homes
Built for you

UNIT 11 FLOOR PLAN

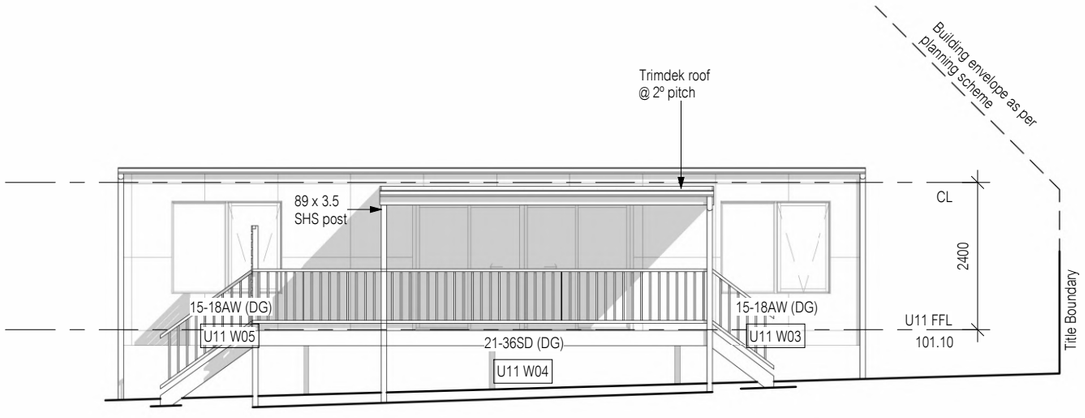
Drawn	ST	U249	
Date	13 June 2024	Sheet	22/25
Scale	1 : 100		

No.	Date	Int.
B	2 August 2024	ST

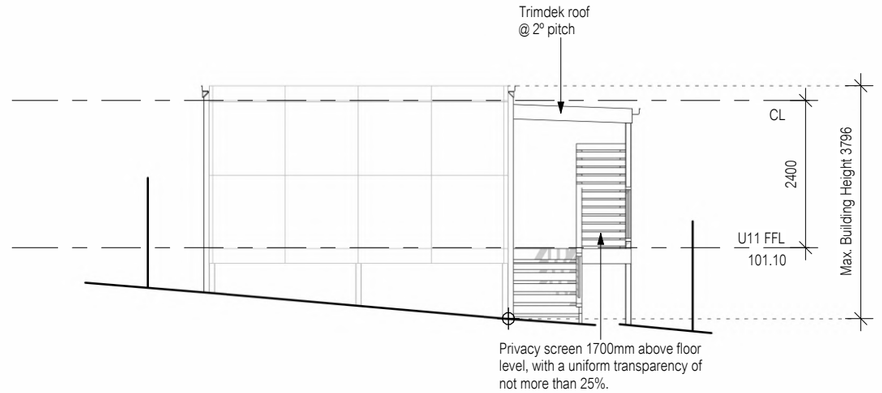
Amendment changes as per cover sheet

Material	Colour
Trimdek Roof	tbc

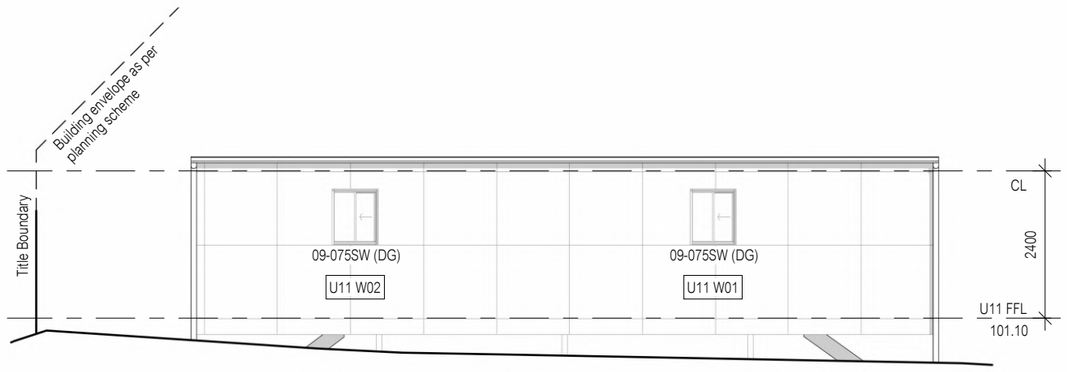
All lightweight cladding to be installed to manufacturer's guidelines. Refer to manufacturer's documentation.



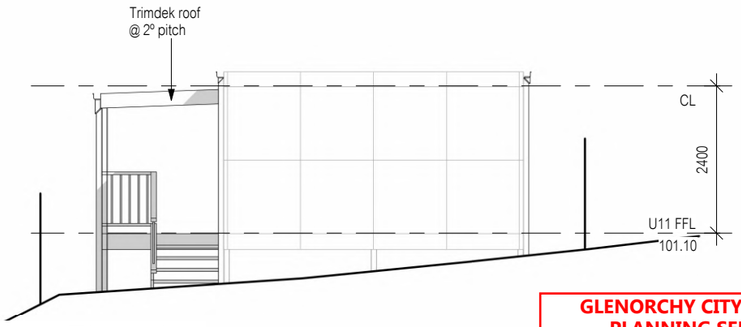
North Elevation



East Elevation



South Elevation



West Elevation

GLENORCHY CITY COUNCIL
PLANNING SERVICES

APPLICATION No. : PLN-24-270

DATE RECEIVED: 06/11/2025

No.	Date	Int.
B	2 August 2024	ST

Amendment changes as per cover sheet

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- No changes permitted without consultation with designer.

Designer:

ANOTHER PERSPECTIVE PTY LTD
PO BOX 21
NEW TOWN
LIC. NO. 685230609 (S. Turvey)
Ph: (03) 6231 4122
Fx: (03) 6231 4166
Email:
info@anotherperspective.com.au

Client / Project info

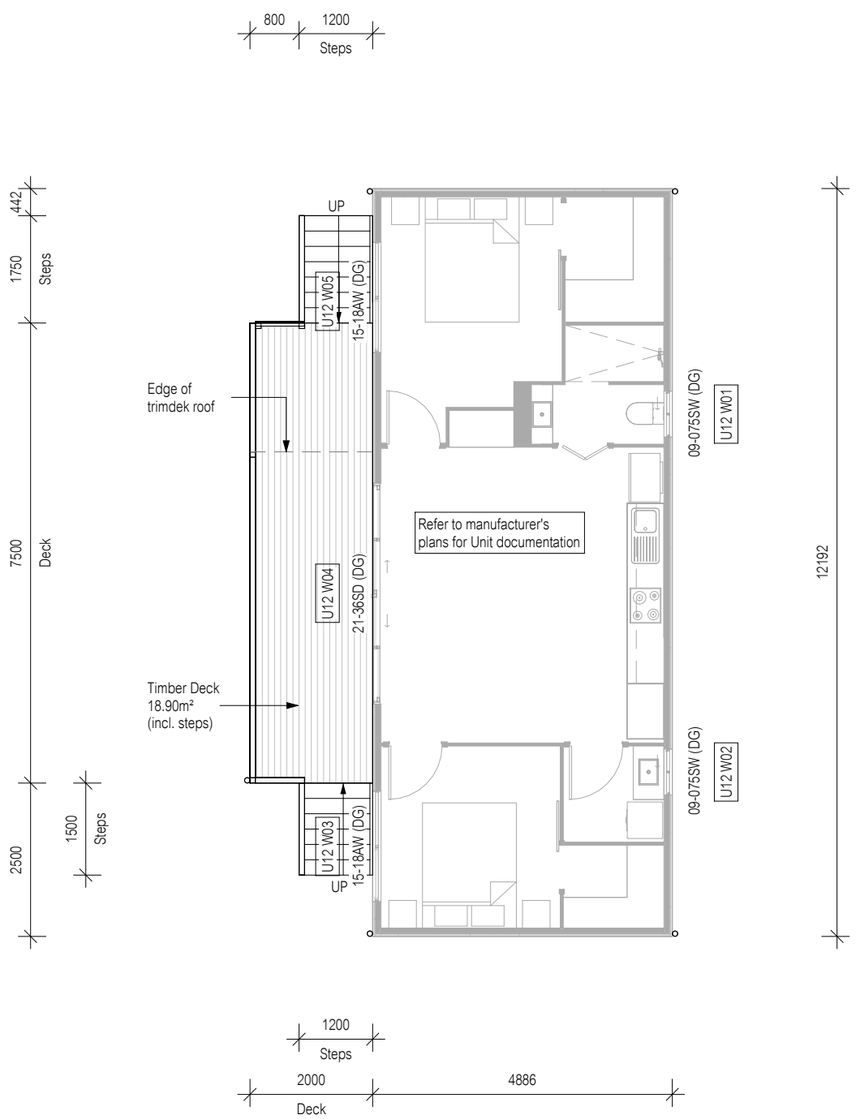
PROPOSED CUNIC UNIT DEVELOPMENT

168a Abbotsfield Road
CLAREMONT

CUNIC homes
Built for you

UNIT 11 ELEVATIONS

Drawn	ST	U249
Date	13 June 2024	Sheet 23/25
Scale	1 : 100	



GLENORCHY CITY COUNCIL
PLANNING SERVICES

APPLICATION No. : PLN-24-270

DATE RECEIVED: 06/11/2025

0 1 2 3 4m
1:100

Floor Area = Refer to Manufacturer's Documentation

Articulation joints

Smoke Alarm (interconnected where more than 1)

All window sizes to be checked and/or confirmed on site prior to ordering glazing units

No.	Date	Int.
B	2 August 2024	ST

Amendment changes as per cover sheet

Notes
- Builder to verify all dimensions and levels on site prior to commencement of work
- All work to be carried out in accordance with the current National Construction Code.
- All materials to be installed according to manufacturers specifications.
- Do not scale from these drawings.
- No changes permitted without consultation with designer.

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PROPOSED CUNIC UNIT DEVELOPMENT

168a Abbotsfield Road
CLAREMONT

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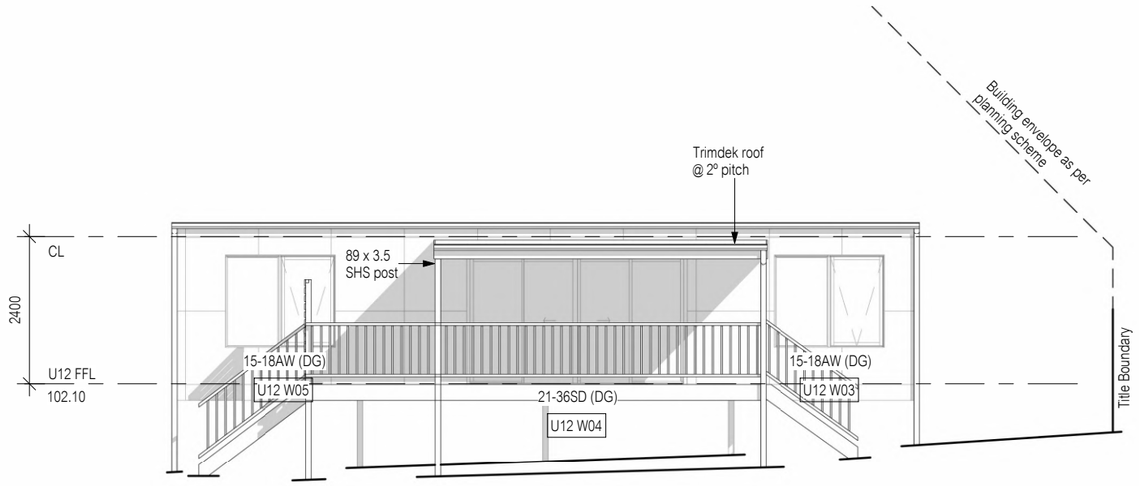
Built for you

UNIT 12 FLOOR PLAN

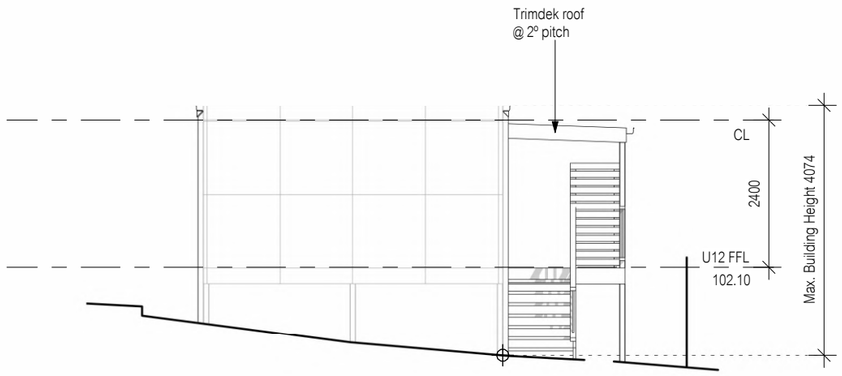
Drawn	ST	U249	
Date	13 June 2024	Sheet	24/25
Scale	1 : 100		

Material	Colour
Trimdek Roof	tbc

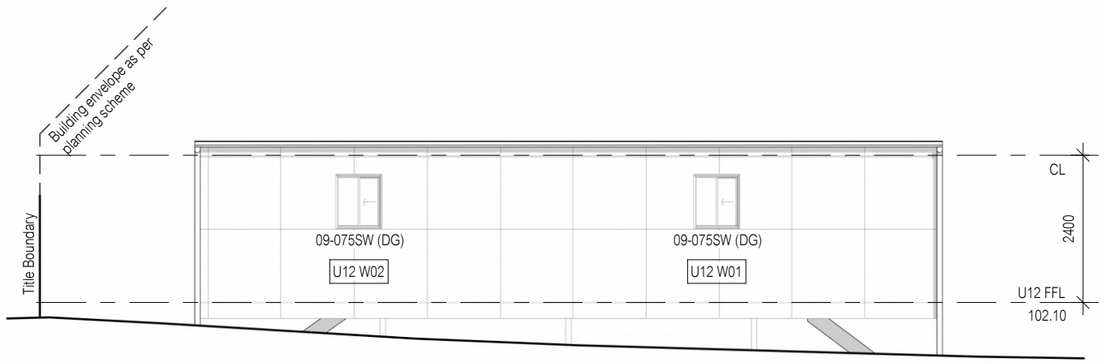
All lightweight cladding to be installed to manufacturer's guidelines. Refer to manufacturer's documentation.



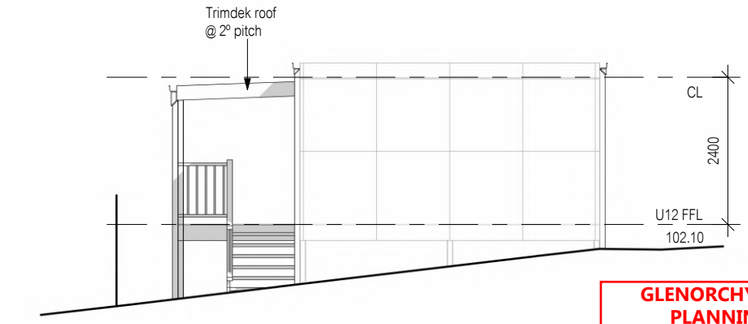
North Elevation



East Elevation



South Elevation



West Elevation

GLENORCHY CITY COUNCIL
PLANNING SERVICES
APPLICATION No. : PLN-24-270
DATE RECEIVED: 06/11/2025

No.	Date	Int.
B	2 August 2024	ST

Amendment changes as per cover sheet

All window sizes to be checked and/or confirmed on site prior to ordering glazing units

LEGEND:
AJ - Articulation Joint
BV - Brick Vent

Shadows shown for stylisation purposes only

Notes
- Builder to verify all dimensions and levels on site prior to commencement of work
- All work to be carried out in accordance with the current National Construction Code.
- All materials to be installed according to manufacturers specifications.
- Do not scale from these drawings.
- No changes permitted without consultation with designer.

Designer:

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NEW TOWN
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Client / Project info

PROPOSED CUNIC UNIT DEVELOPMENT

168a Abbotsfield Road
CLAREMONT

CUNIC homes
Built for you

UNIT 12 ELEVATIONS

Drawn	ST	U249
Date	13 June 2024	Sheet 25/25
Scale	1 : 100	

CLIENT / ARCHITECT:

CUNIC HOMES

PROJECT DETAILS:

UNITS DEVELOPMENT
168A ABBOTSFIELD ROAD, CLAREMONT

PROJECT No:

241043

DISCIPLINE:

CIVIL

DRAWINGS:

GLENORCHY CITY COUNCIL
PLANNING SERVICES

APPLICATION No. : PLN-24-270

DATE RECEIVED: 12/06/2025

REV:	ISSUED FOR / DESCRIPTION:	BY:	DATE:
F	REVISED DEVELOPMENT APPLICATION	SCP	29-05-25
E	REVISED DEVELOPMENT APPLICATION	OWM	18-02-25
D	REVIEW / INFORMATION	OWM	07-02-25
C	REVISED DEVELOPMENT APPLICATION	OWM	07-11-24
B	DEVELOPMENT APPLICATION	OWM	27-09-24
A	REVIEW / INFORMATION	OWM	26-09-24

COLLECTIVE CONSULTING

E admin@collectiveconsulting.com.au
Level 1, 10-14 Paterson Street
Launceston TAS 7250
P (03) 6334 0834
collectiveconsulting.com.au

CLIENT / ARCHITECT: CUNIC HOMES

PROJECT DETAILS: 168a ABBOTSFIELD ROAD, CLAREMONT UNITS DEVELOPMENT

DRAWING TITLE: COVER SHEET

DESIGN BY:	DESIGN CHECK:	DRAWN BY:	DRAFT CHECK:	CERTIFIER:
AJL	JTA	OWM	JTA	

SCALE:	PROJECT No:	DRAWING No:	REVISION:
-	241043	COV	F

GENERAL NOTES

1 // GENERAL

A. THESE DRAWINGS AND NOTES SHALL BE READ IN CONJUNCTION WITH ARCHITECTURAL, LANDSCAPE ARCHITECTS, STRUCTURAL, BUILDING SERVICES AND OTHER DISCIPLINES' DRAWINGS AND SPECIFICATIONS AND WITH ANY WRITTEN ENGINEER'S INSTRUCTIONS ISSUED DURING THE CONTRACT.
B. THE CONTRACTOR SHALL ENSURE THAT ALL CIVIL WORKS, MATERIALS, INFRASTRUCTURE AND WORKMANSHIP COMPLY WITH PLANNING AND BUILDING PERMITS, THE NATIONAL CONSTRUCTION CODE OF AUSTRALIA (NCC), AUSTRALIAN STANDARDS (AS), DEPARTMENT OF STATE GROWTH (DSG), INSTITUTE OF PUBLIC WORKS ENGINEERING AUSTRALIA (IPWEA) - TAS DIVISION, LOCAL GOVERNMENT ASSOCIATION TASMANIA (LGAT), WATER SERVICES ASSOCIATION OF AUSTRALIA (WSAA) AND ANY OTHER STATE / TERRITORY / LOCAL GOVERNMENT REGULATIONS.
C. ALL AUSTRALIAN STANDARDS REFERENCED IN THESE DRAWINGS ARE TO BE NOTED AS THE CURRENT VERSIONS.
D. ANY DISCREPANCIES ARE TO BE REPORTED TO THE SUPERINTENDENT BEFORE PROCEEDING WITH THE WORK.
E. THESE GENERAL NOTES DO NOT HAVE PRECEDENCE OVER THE SPECIFICATION OR DRAWING NOTES.
F. ALL SET OUT DIMENSIONS ON THE DRAWINGS ARE TO BE VERIFIED BY THE CONTRACTOR ON SITE BEFORE COMMENCING WORK.
G. DO NOT SCALE FOR DIMENSIONS OFF THESE DRAWINGS.
H. UNLESS NOTED OTHERWISE, ALL DIMENSIONS SHOWN ARE IN MILLIMETRES WITH THE EXCEPTION OF SURVEY LEVELS, WHICH ARE IN METRES.
I. THE CONTRACTOR IS TO ENSURE THAT ANY PROFESSIONALS, TRADESMEN OR SUPPLIERS ENGAGED THROUGHOUT THE DURATION OF THE CONTRACT ARE ACCREDITED AND QUALIFIED FOR THEIR DUTY OF WORK AND CARRY ALL NECESSARY PERMITS REQUIRED BY ANY STATUTORY AUTHORITY.
J. INSTALL ANY AND ALL PROPRIETARY ITEMS IN ACCORDANCE WITH SPECIFIC MANUFACTURERS REQUIREMENTS, SPECIFICATIONS AND RECOMMENDATIONS.

2 // NOTICE TO CONTRACTOR / TENDERER

A. THE CONTRACTOR / TENDERER IS TO MAKE THEMSELVES AWARE OF THE LOCAL COUNCIL AND THE DEPARTMENT OF STATE GROWTH (DSG) STANDARDS FOR CIVIL WORKS. TENDERER IS TO ALLOW FOR THESE STANDARDS DURING PRICING.
B. CONSTRUCTION IS TO BE CARRIED OUT IN ACCORDANCE WITH THESE STANDARDS THROUGHOUT THE DURATION OF THE CONTRACT.
C. COPIES OF THESE STANDARDS ARE AVAILABLE UPON REQUEST FROM THE LOCAL COUNCIL AND DSG'S WEBSITE.

3 // DESIGN LEVELS

A. CONFIRM / DETERMINE FINISHED FLOOR LEVELS ON SITE TO ACHIEVE DESIGN INTENT. REFER ARCHITECT FOR ANY DISCREPANCIES, ISSUES OR CHANGES TO FLOOR LEVELS. GENERALLY, SURFACES ARE TO BE SLOPED AWAY FROM BUILDINGS.

4 // SCOPE OF WORKS

A. THE SCOPE OF WORKS ARE SHOWN IN THESE DOCUMENTS AND THE SPECIFICATION.
B. THE CONTRACTOR IS EXPECTED TO RESOLVE ALL ISSUES UNCOVERED ON SITE THAT ARE NOT DETAILED IN THESE DOCUMENTS, IN CONJUNCTION WITH THE SUPERINTENDENT / PRINCIPAL.

5 // DISPOSAL OF EXCAVATED MATERIAL

A. DISPOSE OF EXCAVATED MATERIAL TO A LICENSED WASTE FACILITY OR APPROVED LAND FILL SITE.

6 // APPROVALS

ALL WORKS ARE TO BE CARRIED OUT IN ACCORDANCE WITH THE FOLLOWING APPROVALS;

- NIL

7 // LINETYPE LEGEND

eCOM	EXISTING COMMUNICATIONS LINE - CONFIRM EXACT LOCATION
COM	PROPOSED COMMUNICATIONS LINE
eFM	EXISTING FIRE WATER LINE / MAIN - CONFIRM EXACT LOCATION
FM	PROPOSED FIRE WATER LINE / MAIN
eGAS	EXISTING GAS LINE / MAIN - CONFIRM EXACT LOCATION
GAS	NEW GAS LINE / MAIN
ePWR	EXISTING POWER LINE - CONFIRM EXACT LOCATION
PWR	PROPOSED POWER LINE
▬ ▬ ▬	EXISTING SEWER SERVICE LINE / MAIN - CONFIRM EXACT LOCATION
▬ ▬ ▬	PROPOSED SEWER SERVICE LINE / MAIN
eS-RM	EXISTING SEWER RISING MAIN - CONFIRM EXACT LOCATION
S-RM	PROPOSED SEWER RISING MAIN
▬ ▬ ▬	EXISTING STORMWATER LINE / MAIN - CONFIRM EXACT LOCATION
▬ ▬ ▬	PROPOSED STORMWATER LINE / MAIN
eSW-RM	EXISTING STORMWATER RISING MAIN - CONFIRM EXACT LOCATION
SW-RM	PROPOSED STORMWATER RISING MAIN
eAG	EXISTING AGRICULTURAL DRAIN (AG DRAIN)
AG	PROPOSED SLOTTED AGRICULTURAL DRAIN (AG DRAIN)
eW	EXISTING WATER SERVICE LINE / MAIN - CONFIRM EXACT LOCATION
W	PROPOSED WATER SERVICE LINE / MAIN
— · · — · · —	PROPOSED VEE DRAIN - REFER SECTIONS AND DETAILS
—·—·—·—	EXISTING SERVICE LINE / MAIN TO BE DEMOLISHED
- - - - - - -	EXISTING SURFACE / STRUCTURE TO BE DEMOLISHED
—— · ——	TITLE BOUNDARY
— —— —	DRAINAGE EASEMENT / RIGHT OF WAY
—— · ——	EROSION CONTROL BARRIER

EXISTING INFRASTRUCTURE

1 // LOCATION OF EXISTING INFRASTRUCTURE

A. LOCATE ALL EXISTING UNDERGROUND INFRASTRUCTURE PRIOR TO COMMENCING ANY SITE AND DEMOLITION WORKS WITH THE FOLLOWING METHODS:
  A.1. THE CONTRACTOR IS TO NOTIFY ALL RELEVANT STATUTORY AUTHORITIES PRIOR TO COMMENCING ANY WORK FOR THE POSSIBLE LOCATION OF ANY EXISTING INFRASTRUCTURE NOT SHOWN ON THESE PLANS, AND IS TO NOTIFY THE SUPERINTENDENT OF THE SAME.
  A.2. THE CONTRACTOR IS TO COMPLETE A 'BEFORE YOU DIG'.
  A.3. THE CONTRACTOR IS TO REVIEW ALL SURVEY AND UNDERGROUND ASSET DATA.
  A.4. THE CONTRACTOR IS TO ARRANGE AND PAY FOR THE ON SITE MARKING AND CONFIRMATION OF DEPTH OF SERVICES LOCATIONS FOR ALL UNDERGROUND INFRASTRUCTURE INCLUDING BUT NOT LIMITED TO; COMMUNICATIONS, TASNETWORKS, TASGAS, TASWATER AND COUNCIL INFRASTRUCTURE (IE SEWER, STORMWATER, WATER ETC.) IN THE AREA OF NEW WORKS. CONFIRM LOCATIONS USING CABLE LOCATORS, POT HOLING, SUCTION TRUCK, HAND DIGGING AND UNDERGROUND CCTV CAMERA INSPECTIONS.
  A.5. THE CONTRACTOR IS TO WALK SITE AND IDENTIFY ANY ASSETS THAT MAY HAVE BEEN MISSED AND REPORT TO SUPERINTENDENT.

2 // GENERAL

A. ANY CLASHES WITH DESIGNED INFRASTRUCTURE ON THE FOLLOWING DESIGN DRAWINGS ARE TO BE REPORTED TO DESIGN ENGINEER FOR DIRECTION.
B. ALL EXISTING INFRASTRUCURE IS TO BE PROTECTED DURING CONSTRUCTION. ANY DAMAGE TO EXISTING INFRASTRUCTURE IS TO BE MADE GOOD AT THE CONTRACTOR'S EXPENSE.
C. TRENCHES WHERE SERVICES ARE REMOVED ARE TO BE FILLED WITH AN APPROVED COMPACTED MATERIAL AND TO ENGINEERS COMPACTION SPECIFICATIONS. MATCH AND MAKE GOOD SURFACES TO MATCH EXISTING SURROUNDINGS.

SAFETY IN DESIGN

1 // GENERAL

A. THE 'SAFETY IN DESIGN' RISK MITIGATION MEASURES FOR THIS PROJECT DO NOT ACCOUNT FOR ALL DESIGN, CONSTRUCTION, OPERATION, MAINTENANCE AND DEMOLITION ASSESSMENTS.
B. THEY DO NOT REDUCE OR LIMIT THE OBLIGATIONS OF THE CONTRACTOR, CONSTRUCTOR, USER, OPERATOR, MAINTAINER OR DEMOLISHER TO PERFORM THEIR OWN SAFETY IN DESIGN RISK ASSESSMENTS.
C. CONSTRUCTION AND INSTALLATION SAFE WORK METHOD STATEMENTS ARE TO BE REVIEWED BY A QUALIFIED PERSON TO ELIMINATE AND MINIMISE INSTALLATION RISKS.

DEMOLITION WORKS

1 // GENERAL

A. DEMOLITION WORKS ARE TO BE READ IN CONJUNCTION WITH ARCHITECTS AND OTHER CONSULTANTS DEMOLITION PLANS. CONTRACTOR TO NOTIFY ARCHITECT AND CONFIRM ANY ISSUES / CONTRADICTIONS WITH ARCHITECT.
B. CONTRACTOR TO MAKE ALL NECESSARY ALLOWANCES FOR REQUIRED DEMOLITIONS, REMOVALS AND RELOCATIONS TO SUIT NEW WORKS.
C. ALL EXISTING INFRASTRUCTURE IS TO BE PROTECTED DURING CONSTRUCTION. ANY DAMAGE TO EXISTING INFRASTRUCTURE IS TO BE MADE GOOD AT THE CONTRACTOR'S EXPENSE.
D. CAP, TERMINATE AND REMOVE REDUNDANT DISUSED DRAINAGE SERVICES TO SATISFACTION OF ENGINEER, LOCAL AUTHORITIES AND IN ACCORDANCE WITH AS3500.
E. TRENCHES WHERE SERVICES ARE REMOVED ARE TO BE FILLED WITH AN APPROVED COMPACTED MATERIAL AND TO ENGINEERS COMPACTION SPECIFICATIONS. MATCH AND MAKE GOOD SURFACES TO MATCH EXISTING SURROUNDINGS.
F. CONTRACTOR TO ALLOW TO MAKE GOOD ALL SURFACES AFFECTED BY DEMOLITION WORKS TO SUPERINTENDENT / PRINCIPAL / LOCAL COUNCIL'S SATISFACTION.

EXISTING SURVEY

1 // EXISTING SURVEY DETAILS

A. THE FOLLOWING ARE THE SURVEY DETAILS USED AS A BASIS FOR THE DESIGN:

SURVEYOR:	ROGERSON & BIRCH SURVEYORS
SURVEY REFERENCE NUMBER:	61276/27
SURVEY DATE:	19/12/2023
SITE LOCATION:	168A ABBOTSFIELD ROAD, CLAREMONT
COORDINATION SYSTEM:	MGA2020
LEVEL DATUM:	AHD83
SERVICE MARKER:	SPM9546

SITE SETOUT

1 // GENERAL

A. SETOUT IS THE RESPONSIBILITY OF THE CONTRACTOR AND SURVEYOR.
B. THE CONTRACTOR IS TO ARRANGE AND PAY FOR A REGISTERED SURVEYOR TO SETOUT THE BUILDING/S, CIVIL WORKS AND ANY OTHER COMPONENT.
C. COLLECTIVE CONSULTING TAKE NO RESPONSIBILITY FOR THE SETOUT OF BUILDING/S, CIVIL WORKS AND ANY OTHER COMPONENT.
D. REFER ARCH. FOR SETOUT OF ALL BUILDING/S AND RELATED COMPONENTS.
E. ALL SETOUT DIMENSIONS ON THESE DRAWINGS ARE TO BE VERIFIED BY THE CONTRACTOR ON SITE BEFORE COMMENCING WORK.
F. DO NOT SCALE FOR DIMENSIONS OFF THESE DRAWINGS.
G. UNLESS NOTED OTHERWISE, ALL DIMENSIONS SHOWN ARE IN MILLIMETRES WITH THE EXCEPTION OF SURVEY LEVELS, WHICH ARE IN METRES.

EARTHWORKS

1 // GENERAL

A. GENERAL EARTHWORKS, MATERIAL AND WORKMANSHIP SHALL COMPLY WITH THE SPECIFICATION, THE CURRENT EDITION OF THE SAA CODE FOR EARTHWORKS, AS3798, THE NCC, SAFE WORK AUSTRALIA CODE OF PRACTICE FOR EXCAVATION WORK, TOGETHER WITH ANY CODES, STANDARDS OR REGULATIONS REFEREED TO THEREIN.
B. THE CONTRACTOR SHALL KEEP A COPY OF THE CURRENT VERSION OF AS3798 AND ANY OTHER REQUIRED CODES, STANDARDS AND REGULATIONS ON SITE.

2 // TESTING & INSPECTIONS

A. THE CONTRACTOR IS TO BE RESPONSIBLE FOR ENGAGING AND PAYING ALL COSTS FOR AN APPROVED CONSTRUCTION MATERIALS TESTING COMPANY TO CARRY OUT TESTING OF ALL EARTHWORKS INCLUDING, BUT NOT LIMITED TO:

TESTING TYPE:	**TESTING REQUIREMENTS:**
SUBGRADE	LEVEL 1 TESTING
BACKFILLING OF SERVICE TRENCHES	LEVEL 1 TESTING
FILLS	LEVEL 1 TESTING
PAVEMENTS	LEVEL 1 TESTING

CERTIFICATION OF THESE ELEMENTS ARE TO BE PROVIDED PRIOR TO PRACTICAL COMPLETION.

3 // AREAS OF CUT

A. STRIP EXISTING TOP SOIL, VEGETATION, HARD SURFACES AND OTHER MATERIAL TO SUBGRADE LEVEL.
B. PROOF ROLL SUBGRADE IN ACCORDANCE WITH AS1289 TO:
  - 98% STANDARD DRY DENSITY UNDER BUILDINGS
  - 98% STANDARD DRY DENSITY UNDER ROADS AND CARPARKS
  - REMOVE ANY SOFT SPOTS AND COMPACT WITH 2% OF OPTIMUM MOISTURE CONTENT TO STANDARD DRY DENSITY AS STATED ABOVE

4 // AREAS OF FILL

A. STRIP EXISTING TOP SOIL, VEGETATION, HARD SURFACES AND OTHER MATERIAL TO SUBGRADE LEVEL.
B. PROOF ROLL SUBGRADE IN ACCORDANCE WITH AS1289 TO:
  - 98% STANDARD DRY DENSITY UNDER BUILDINGS
  - 98% STANDARD DRY DENSITY UNDER ROADS AND CARPARKS
  - REMOVE ANY SOFT SPOTS AND COMPACT WITH 2% OF OPTIMUM MOISTURE CONTENT TO STANDARD DRY DENSITY AS STATED ABOVE
C. PLACE FILL AS SPECIFIED AND COMPACT WITHIN 2% OF OPTIMUM MOISTURE CONTENT TO STANDARD DRY DENSITY AS STATED ABOVE.

5 // DISPOSAL OF EXCAVATED MATERIAL

A. DISPOSE OF EXCAVATED MATERIAL TO A LICENSED WASTE FACILITY OR APPROVED LAND FILL SITE.

SOIL AND WATER MANAGEMENT

1 // GENERAL

A. ALL WORKS ARE TO BE CARRIED OUT IN ACCORDANCE WITH 'SOIL & WATER MANAGEMENT ON BUILDING & CONSTRUCTION SITES' GUIDELINES AVAILABLE FROM NORTHERN RESOURCE MANAGEMENT (NRM) AND DETAILS SUPPLIED IN THESE DESIGN DRAWINGS.
B. COMPLY WITH ALL REQUIREMENTS TO LIMIT STORMWATER RUNOFF FROM THE SITE DURING CONSTRUCTION.
C. IT IS STRONGLY RECOMMENDED THAT THE DEVELOPER RE-COVERS ANY DISTURBED AREAS WITH TOPSOIL AS QUICKLY AS POSSIBLE AFTER BULK EARTHWORKS ARE COMPLETED, TO PREVENT SOIL DISPERSION.

2 // SOIL EROSION CONTROL

A. CONTRACTOR TO ALLOW TO;
  A.1. LIMIT DISTURBANCE WHEN EXCAVATING BY PRESERVING VEGETATED AREAS AS MUCH AS POSSIBLE
  A.2. DIVERT UP-SLOPE WATER WHERE PRACTICAL
  A.3. INSTALL SEDIMENT FENCES DOWN SLOPE OF ALL DISTURBED LANDS TO FILTER LARGE PARTICLES PRIOR TO STORMWATER SYSTEM
  A.4. WASH EQUIPMENT IN DESIGNATED AREA THAT DOES NOT DRAIN TO STORMWATER SYSTEM OR NATURAL DRAINAGE LINES
  A.5. PLACE STOCK PILES AWAY FROM ON-SITE DRAINAGE & UP-SLOPE FROM SEDIMENT FENCES
  A.6. LEAVE AND MAINTAIN VEGETATED FOOTPATHS
  A.7. STORE ALL HARD WASTE AND LITTER IN A DESIGNATED AREA THAT WILL PREVENT IT FROM BEING BLOWN AWAY AND WASHED INTO THE STORMWATER SYSTEMS
  A.8. RESTRICT VEHICLE MOVEMENT TO A STABILISED ACCESS

3 // NRM GUIDELINES

A. CONTRACTOR TO COMPLETE ALL WORKS IN ACCORDANCE WITH NRM SOIL & WATER MANAGEMENT ON BUILDING & CONSTRUCTION SITE USING THE FOLLOWING FACT SHEETS;
  FACT SHEET 1:
  SOIL & WATER MANAGEMENT ON LARGE BUILDING & CONSTRUCTION SITES
  FACT SHEET 2:
  SOIL & WATER MANAGEMENT ON STANDARD BUILDING & CONSTRUCTION SITES
  FACT SHEET 3: SOIL & WATER MANAGEMENT PLANS
  FACT SHEET 4: DISPERSIVE SOILS - HIGH RISK OF TUNNEL EROSION
  FACT SHEET 5: MINIMISE SOIL DISTURBANCE
  FACT SHEET 6: PRESERVE VEGETATION
  FACT SHEET 7: DIVERT UP-SLOPE WATER
  FACT SHEET 8: EROSION CONTROL MATS & BLANKETS
  FACT SHEET 9: PROTECT SERVICE TRENCHES & STOCKPILES
  FACT SHEET 10: EARLY ROOF DRAINAGE CONNECTION
  FACT SHEET 11:
  SCOUR PROTECTION - STORMWATER PIPE OUTFALLS & CHECK DAMS
  FACT SHEET 12: STABILISED SITE ACCESS
  FACT SHEET 13: WHEEL WASH
  FACT SHEET 14: SEDIMENT FENCES & FIBRE ROLLS
  FACT SHEET 15: PROTECTION OF STORMWATER PITS
  FACT SHEET 16: MANAGE CONCRETE, BRICK & TILE CUTTING
  FACT SHEET 17: SEDIMENT BASINS
  FACT SHEET 18: DUST CONTROL
  FACT SHEET 19: SITE RE-VEGETATION

CIVIL WORKS

1 // GENERAL

A. THE CONTRACTOR SHALL ENSURE THAT ALL CIVIL WORKS, MATERIALS AND WORKMANSHIP COMPLY WITH PLANNING AND BUILDING PERMITS, THE NATIONAL CONSTRUCTION CODE OF AUSTRALIA (NCC), AUSTRALIAN STANDARDS (AS), DEPARTMENT OF STATE GROWTH (DSG), INSTITUTE OF PUBLIC WORKS ENGINEERING AUSTRALIA (IPWEA) - TAS DIVISION, LOCAL GOVERNMENT ASSOCIATION TASMANIA (LGAT), AND ANY OTHER STATE / TERRITORY / LOCAL GOVERNMENT REGULATIONS.
B. ANY DEPARTURE FROM THESE STANDARDS AND REGULATIONS REQUIRES THE PRIOR WRITTEN APPROVAL FROM THE SUPERINTENDENT AND THE WORKS SUPERVISOR / INSPECTOR.

2 // INSPECTIONS

A. THE CONTRACTOR IS TO BE RESPONSIBLE FOR ORGANISING INSPECTIONS WITH THE SUPERINTENDENT.
B. THE FOLLOWING SITE INSPECTIONS ARE REQUIRED DURING CONSTRUCTION / HOLD POINTS, AS A MINIMUM, BEFORE COMMENCEMENT OF FURTHER WORKS:

REQUIRED SITE INSPECTIONS:
SUBGRADE PREPARATION
SUBBASE FOR ROADS, CARPARKS AND KERBS
FINAL TRIM PRIOR TO PLACING KERBS
FINAL TRIM PRIOR TO SEALING

C. THE CONTRACTOR IS TO MAKE THEMSELVES AWARE OF ANY ADDITIONAL INSPECTIONS REQUIRED BY THE LOCAL COUNCIL TO ACHIEVE PRACTICAL COMPLETION AND NOTIFY COLLECTIVE CONSULTING.
D. COLLECTIVE CONSULTING REQUIRE MIN. 48 HOURS NOTICE PRIOR TO ALL REQUIRED INSPECTIONS.

3 // TESTING

A. THE CONTRACTOR IS TO BE RESPONSIBLE FOR ENGAGING AND PAYING ALL COSTS FOR AN APPROVED CONSTRUCTION MATERIALS TESTING COMPANY TO CARRY OUT TESTING IN ACCORDANCE WITH DSG SPEC. SECTION 173 - EXAMINATION AND TESTING OF MATERIALS AND WORK (ROADWORKS).

4 // HOTMIX ASPHALT

A. ALL HOTMIX ASPHALT IS TO BE BLACK IN COLOUR (U.N.O.) AND IS TO BE PLACED IN ACCORDANCE WITH AND MEET THE REQUIREMENTS OF DSG SPEC. SECTION 407 - HOTMIX ASPHALT.

5 // KERBS AND CHANNELS

A. ALL KERBS AND CHANNELS AND ACCESS RAMPS SHOWN ON THE DRAWINGS ARE TO BE IN ACCORDANCE WITH LGAT STANDARD DRAWINGS TSD-R14-v3 TO TSD-R18-v3.

6 // FOOTPATHS

A. CONSTRUCT FOOTPATHS (INCLUDING EXPANSION JOINTS, CONTROL JOINTS, WEAKENED PLANE JOINTS, ETC.) IN ACCORDANCE WITH LGAT STANDARD DRAWING TSD-R11-v3.

7 // LANDSCAPE / STREET FURNITURE

A. LANDSCAPE AND STREET FURNITURE DESIGN AND DETAILING BY OTHERS.

8 // ROAD RESERVE WORKS

A. ALL WORKS IN (OR REQUIRING OCCUPATION) IN THE ROAD RESERVE MUST BE UNDERTAKEN BY CONTRACTOR REGISTERED WITH COUNCIL'S REGISTERED CONTRACTORS OR AS APPROVED BY COUNCIL.

SIGNAGE AND LINE MARKING

1 // GENERAL

A. LINE MARKING AND SIGNAGE SHOWN ON THE DESIGN PLANS ARE FOR INFORMATION ONLY. REFER TO THE ARCHITECTURAL PLANS FOR DETAILS.
B. CONTRACTOR TO INSTALL ALL SIGNAGE AND LINE MARKING AS PER THE ARCHITECTURAL PLANS.
C. CAR PARKING SPACE/S AND LINE MARKING TO BE IN ACCORDANCE WITH AS2890.
D. ACCESS CAR PARKING SPACE/S, SIGNAGE. SHARED AREA, BOLLARD AND LINE MARKING TO BE IN ACCORDANCE AS2890.6.
E. ALL LINE MARKING TO BE WITH DULUX ROADMASTER (OR EQUIVALENT) U.N.O.
F. ALL SIGN WORKS AND INSTALLATION TO BE IN ACCORDANCE WITH CURRENT VERSION OF 'MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES' (MUTCD) & AUSTROADS FOR SIGNAGE DETAILS.

INFRASTRUCTURE IN EMBANKMENT FILL

1 // GENERAL

A. WHERE THE LOCATION OF SEWER OR STORMWATER INFRASTRUCTURE REQUIRING FILL OR CONSTRUCTION IN AN EMBANKMENT, ALONG THE ROUTE SHOWN IN THE DESIGN DRAWINGS, PROCEED AS FOLLOWS;
  A.1. PREPARE THE FOUNDATION FOR THE FILL BY CLEANING AWAY ALL DEBRIS, VEGETATION, ORGANIC MATERIAL AND TOPSOIL FOR THE FULL WIDTH OF THE FILL AREA.
  A.2. COMPACT THE CLEARED SOIL SURFACE TO NOT LESS THAN 95% OF IT'S STANDARD MAXIMUM DRY DENSITY (AS3798).
  A.3. PLACE THE FILL IN LAYERS NOT EXCEEDING 200MM THICKNESS AND COMPACT EACH LAYER TO NOT LESS THAN 95% OF IT'S STANDARD MAXIMUM DRY DENSITY (AS3798).
  A.4. BRING THE COMPACTED FILL LEVEL UP TO A HEIGHT OF AT LEAST 300MM ABOVE THE DESIGN LEVEL OF THE TOP OF THE PIPE.
  A.5. PLACE THE REMAINDER OF THE FILL IN LAYERS NOT EXCEEDING 300MM THICKNESS AND COMPACT EACH LAYER TO NOT LESS THAN 95% OF IT'S STANDARD MAXIMUM DRY DENSITY (AS3798).
B. NOTE THAT ALL EARTHWORKS ARE TO BE CONSTRUCTED IN ACCORDANCE WITH AS3798.

CONTRACTOR SPECIFIC WORKS - PLUMBING

1 // GENERAL

A. DUE TO THE AGE OF BUILDING AND THE REQUIREMENTS FOR NEW BUILDING WORKS TO COMPLY WITH THE NATIONAL CONSTRUCTION CODE OF AUSTRALIA (NCC), AUSTRALIAN STANDARDS (AS), DEPARTMENT OF STATE GROWTH (DSG), INSTITUTE OF PUBLIC WORKS ENGINEERING AUSTRALIA (IPWEA) - TAS DIVISION, LOCAL GOVERNMENT ASSOCIATION TASMANIA (LGAT), WATER SERVICES ASSOCIATION OF AUSTRALIA (WSAA) TASMANIA FIRE SERVICE REQUIREMENTS AND ANY OTHER STATE / TERRITORY / LOCAL GOVERNMENT REGULATIONS, CONTRACTORS ARE TO COMPLETE ALL NECESSARY CHECKS AND ASSESSMENTS LISTED BELOW TO ENSURE THE BUILDING WORKS ARE READY FOR CERTIFICATE OF OCCUPANCY & CERTIFICATE OF COMPLETION.
B. ON-SITE TESTING IS TO BE CARRIED OUT DURING ESTABLISHMENT AND PRIOR TO COMMENCEMENT OF NEW WORKS. PROVIDE LATENT CONDITIONS REPORT TO ENGINEER FOR PRICING. THIS REPORT IS TO IDENTIFY INFRASTRUCTURE UPGRADE WORKS AT THE FRONT END OF THE PROJECT.
C. NO VARIATION WILL BE PAID FOR LATENT PLUMBING CONDITIONS THAT HAVE NOT BEEN IDENTIFIED DURING THE ON-SITE TESTING & LATENT CONDITIONS REPORT.
D. THESE WORKS ARE TO BE INCLUDED IN CONTRACTORS TENDER AS AGREED WITH SUPERINTENDENT.
E. LOCATE EXISTING INFRASTRUCTURE USING CABLE LOCATORS, POT HOLING, SUCTION TRUCK, HAND DIGGING AND UNDERGROUND CCTV CAMERA INSPECTIONS.

2 // COMPLIANCE WORKS

A. CONTRACTORS ARE TO COMPLETE THE FOLLOWING WORK:
  A.1. FLOW TEST ALL EXTERNAL & INTERNAL FIRE HYDRANTS FOR COMPLIANCE WITH AS 2419.1. NOTIFY RESULTS TO ENGINEER FOR APPROVAL.
  A.2. FLOW TEST ALL FIRE HOSE REELS FOR COMPLIANCE WITH AS1851.9. NOTIFY RESULTS TO ENGINEER FOR APPROVAL.
  A.3. UPGRADE FIRE HOSE REEL & HYDRANTS TO ACHIEVE COMPLIANCE WITH CURRENT STANDARDS TO ALLOW ISSUE OF CERTIFICATE OF OCCUPANCY & COMPLETION.
  A.4. PRESSURE TEST & DIE TEST EXISTING SEWER SYSTEM PRIOR TO COMMENCING WORKS & CHECK FOR LEAKS OR DEFECTS. MAKE GOOD DEFECTIVE AREAS & PROVIDE CERTIFIED RESULTS TO ENGINEER FOR APPROVAL AS PART OF LATENT CONDITIONS REPORT PREPARED AT FRONT END OF PROJECT. RE-TEST AFTER COMPLETION OF WORKS & PROVIDE RESULTS PRIOR TO HAND OVER.

STORMWATER INFRASTRUCTURE

1 // GENERAL

A. ALL STORMWATER INFRASTRUCTURE WORKS TO BE IN ACCORDANCE WITH THE LOCAL COUNCIL AND DSG STANDARDS AND REGULATIONS.
B. ALL STORMWATER PLUMBING INFRASTRUCTURE AND DRAINAGE TO COMPLY WITH AS3500.3.
C. ANY DEPARTURE FROM THESE STANDARDS AND REGULATIONS REQUIRES THE PRIOR WRITTEN APPROVAL FROM THE SUPERINTENDENT AND LOCAL COUNCIL'S WORKS SUPERVISOR / INSPECTOR.

2 // TESTING

A. ALL DRAINAGE WORKS SHALL BE SUBJECT TO THE TESTS PRESCRIBED BY THE AUTHORITIES WHO HAVE JURISDICTION OVER THE VARIOUS SERVICES.
B. ANY SECTION FAILING SUCH TESTS SHALL BE REMOVED AND REINSTALLED AT THE CONTRACTOR'S EXPENSE.
C. ONCE DRAINAGE INFRASTRUCTURE HAS BEEN INSTALLED, CONTRACTORS SHALL CCTV ALL PIPES AND SUBMIT FOOTAGE TO LOCAL COUNCIL FOR APPROVAL.

3 // MANHOLES (MH)

A. MANHOLES ARE TO BE Ø1050 ID (U.N.O.) PRECAST CONCRETE, INSTALLED IN ACCORDANCE WITH WSAA STANDARDS.
B. ALL MANHOLES IN TRAFFICABLE AREAS ARE TO BE FITTED WITH HEAVY DUTY CLASS D GATIC COVERS AND SURROUNDS (U.N.O).
C. ALL MANHOLES IN NON-TRAFFICABLE AREAS ARE TO BE FITTED WITH MEDIUM DUTY CLASS B GATIC COVERS AND SURROUNDS (U.N.O.).
D. ALL MANHOLES ARE TO HAVE A 5m LENGTH OF Ø75mm MIN. AGRICULTURAL DRAIN CONNECTED TO MANHOLE AND LAID IN THE UPSTREAM PIPE TRENCH IMMEDIATELY ADJACENT TO AND AT THE INVERT OF THE LOWEST PIPEWORK.

4 // TRENCHING AND BACKFILLING

A. ALL TRENCHES ARE TO BE EXCAVATED AND BACKFILLED IN ACCORDANCE WITH THESE DRAWINGS AND LOCAL COUNCIL STANDARDS, INCLUDING ELECTROMAGNETIC METAL IMPREGNATED TAPE IN ALL NON METALLIC PIPE TRENCHES.

6 // INSPECTIONS

A. THE CONTRACTOR IS TO BE RESPONSIBLE FOR ORGANISING INSPECTIONS WITH THE SUPERINTENDENT - LIAISE WITH LOCAL COUNCIL.
B. THE FOLLOWING SITE INSPECTIONS ARE REQUIRED DURING CONSTRUCTION / HOLD POINTS, AS A MINIMUM, BEFORE COMMENCEMENT OF FURTHER WORKS:

REQUIRED SITE INSPECTIONS:
PIPEWORK BEDDING
INSTALLED PIPE PRIOR TO BACKFILLING
BACKFILLING

C. THE CONTRACTOR IS TO MAKE THEMSELVES AWARE OF ANY ADDITIONAL INSPECTIONS REQUIRED BY THE LOCAL COUNCIL TO ACHIEVE PRACTICAL COMPLETION AND NOTIFY COLLECTIVE CONSULTING.
D. COLLECTIVE CONSULTING REQUIRE MIN. 48 HOURS NOTICE PRIOR TO ALL REQUIRED INSPECTIONS.

7 // AS CONSTRUCTED DRAWINGS

A. THE CONTRACTOR WILL BE RESPONSIBLE FOR PRODUCING 'AS INSTALLED' DRAWINGS TO THE STANDARD REQUIRED BY THE LOCAL COUNCIL.
B. THE DRAWINGS SHALL BE CERTIFIED AS BEING CORRECT BY EITHER A CHARTERED CIVIL ENGINEER OR A REGISTERED SURVEYOR.
C. COLLECTIVE CONSULTING CAN PROVIDE THIS SERVICE, AT AN ADDITIONAL FEE. THIS HAS NOT BEEN ALLOWED FOR AS PART OF THESE WORKS / CONTRACT.

8 // REDUNDANT PIPEWORK

A. FILL ALL REDUNDANT SECTION OF PIPEWORK WITH 'LIQUIFILL' (GRADE PC.1 - 0.5-2.0 MPA), U.N.O

SEWER INFRASTRUCTURE

1 // GENERAL

A. ALL SEWER INFRASTRUCTURE WORKS TO BE IN ACCORDANCE WITH THE WSAA SEWER CODE 02-2014-3.1 GRAVITY SEWERAGE CODE OF AUSTRALIA - MELBOURNE RETAIL WATER AGENCIES INTEGRATED (MRWA) VERSION 2.0 AND AS AMENDED BY THE TASWATER SUPPLEMENT.
B. TASWATER APPROVED PRODUCTS CAN BE FOUND AT THE FOLLOWING WEBSITE:
  https://mrwa.com.au/Pages/Products.aspx
C. ANY DEPARTURE FROM THESE STANDARDS AND REGULATIONS REQUIRES THE PRIOR WRITTEN APPROVAL FROM THE SUPERINTENDENT AND TASWATER FIELD SERVICES OFFICER.

2 // TESTING

A. ALL DRAINAGE WORKS SHALL BE SUBJECT TO THE TESTS PRESCRIBED BY THE AUTHORITIES WHO HAVE JURISDICTION OVER THE VARIOUS SERVICES.
B. ANY SECTION FAILING SUCH TESTS SHALL BE REMOVED AND REINSTALLED AT THE CONTRACTOR'S EXPENSE.
C. ONCE DRAINAGE INFRASTRUCTURE HAS BEEN INSTALLED, CONTRACTORS SHALL CCTV ALL PIPES AND SUBMIT FOOTAGE TO TASWATER FOR APPROVAL.

3 // SEWER MAIN CONNECTIONS

A. ALL NEW 'LIVE' CONNECTIONS TO EXISTING TASWATER SEWER INFRASTRUCTURE INCLUDING, BUT NOT LIMITED TO SEWER MAINS AND MANHOLES, ARE TO BE COMPLETED BY TASWATER (UNLESS PRIOR WRITTEN APPROVAL) AT OWNERS COST.
B. INSTALL PROPERTY SEWER CONNECTIONS (STANDARD OR SLOPED) WITH A SURFACE INSPECTION OPENING (IO) NOM. 1.0m WITHIN EACH NEW LOT IN ACCORDANCE WITH SECTION 5 OF WSAA SEWER CODE 02-2014-3.1 GRAVITY SEWERAGE CODE OF AUSTRALIA (MRWA) VERSION 2.0.

4 // MANHOLES (MH)

A. MANHOLES ARE TO BE Ø1050 ID (U.N.O.) PRECAST CONCRETE, INSTALLED IN ACCORDANCE WITH WSAA STANDARDS.
B. CONSTRUCTION AND INSTALLATION OF ALL MANHOLES AND MANHOLE COVERS TO BE IN ACCORDANCE WITH THE WSAA SEWER CODE 02-2014-3.1 GRAVITY SEWERAGE CODE OF AUSTRALIA - MELBOURNE RETAIL WATER AGENCIES INTEGRATED (MRWA) VERSION 2.0 AND AS AMENDED BY THE TASWATER SUPPLEMENT.
C. ALL MANHOLES IN TRAFFICABLE AREAS ARE TO BE FITTED WITH HEAVY DUTY CLASS D GATIC COVERS AND SURROUNDS (U.N.O).
D. ALL MANHOLES IN NON-TRAFFICABLE AREAS ARE TO BE FITTED WITH MEDIUM DUTY CLASS B GATIC COVERS AND SURROUNDS (U.N.O.).
E. BENCHING TO BE FULL DEPTH OF PIPE DIA. AS PER DETAILS IN WSAA SEWER CODE 02-2014-3.1 GRAVITY SEWERAGE CODE OF AUSTRALIA - MELBOURNE RETAIL WATER AGENCIES INTEGRATED (MRWA) VERSION 2.0 AND AS AMENDED BY THE TASWATER SUPPLEMENT.

5 // TRENCHING AND BACKFILLING

A. ALL TRENCHES ARE TO BE EXCAVATED AND BACKFILLED IN ACCORDANCE WITH THESE DRAWINGS AND TASWATER STANDARDS, INCLUDING ELECTROMAGNETIC METAL IMPREGNATED TAPE IN ALL NON METALLIC PIPE TRENCHES.
B. CEMENT STABILISED EMBEDMENT:
  FOR SEWER MAINS, INA CCORDANCE WITH MRWA SEWERAGE STANDARDS DRAWING RWA-S AND AS AMENDED BY THE TASWATER SUPPLEMENT.

6 // INSPECTIONS

A. THE CONTRACTOR IS TO BE RESPONSIBLE FOR ORGANISING INSPECTIONS WITH THE SUPERINTENDENT - LIAISE WITH TASWATER.
B. THE FOLLOWING SITE INSPECTIONS ARE REQUIRED DURING CONSTRUCTION / HOLD POINTS, AS A MINIMUM, BEFORE COMMENCEMENT OF FURTHER WORKS:

REQUIRED SITE INSPECTIONS:
PIPEWORK BEDDING
INSTALLED PIPE PRIOR TO BACKFILLING
BACKFILLING

C. THE CONTRACTOR IS TO MAKE THEMSELVES AWARE OF ANY ADDITIONAL INSPECTIONS REQUIRED BY TASWATER TO ACHIEVE PRACTICAL COMPLETION AND NOTIFY COLLECTIVE CONSULTING.
D. COLLECTIVE CONSULTING REQUIRE MIN. 48 HOURS NOTICE PRIOR TO ALL REQUIRED INSPECTIONS.

7 // AS CONSTRUCTED DRAWINGS

A. THE CONTRACTOR WILL BE RESPONSIBLE FOR PRODUCING 'AS INSTALLED' DRAWINGS TO THE STANDARD REQUIRED BY TASWATER.
B. THE DRAWINGS SHALL BE CERTIFIED AS BEING CORRECT BY EITHER A CHARTERED CIVIL ENGINEER OR A REGISTERED SURVEYOR.
C. COLLECTIVE CONSULTING CAN PROVIDE THIS SERVICE, AT AN ADDITIONAL FEE. THIS HAS NOT BEEN ALLOWED FOR AS PART OF THESE WORKS / CONTRACT.

8 // REDUNDANT PIPEWORK

A. FILL ALL REDUNDANT SECTION OF PIPEWORK WITH 'LIQUIFILL' (GRADE PC.1 - 0.5-2.0 MPa), U.N.O.

WATER RETICULATION INFRASTRUCTURE

1 // GENERAL

A. ALL WATER INFRASTRUCTURE WORKS TO BE IN ACCORDANCE WITH THE FOLLOWING:
  A.1. WSAA WATER SUPPLY CODE 03-2011-3.1 WATER SUPPLY CODE OF AUSTRALIA - MELBOURNE RETAIL WATER AGENCIES INTEGRATED (MRWA) VERSION 2.0 AND AS AMENDED BY THE TASWATER SUPPLEMENT.
  A.2. TASWATER'S STANDARD DRAWINGS TWS-W-0002 SERIES.
  A.3. WATER METERING POLICY / METERING GUIDELINES.
  A.4. TASWATER'S STANDARD DRAWINGS TWS-W-0003 SERIES - FOR PROPERTY SERVICE CONNECTIONS - CAGE FOR WATER METER ASSEMBLY.
  A.5. BOUNDARY BACKFLOW CONTAINMENT REQUIREMENTS AND AS3500.1.
B. ANY DEPARTURE FROM THESE STANDARDS AND REGULATIONS REQUIRES THE PRIOR WRITTEN APPROVAL FROM THE SUPERINTENDENT AND TASWATER'S FIELD SERVICES OFFICER.

2 // TESTING

A. ALL DRAINAGE WORKS SHALL BE SUBJECT TO THE TESTS PRESCRIBED BY THE AUTHORITIES WHO HAVE JURISDICTION OVER THE VARIOUS SERVICES.
B. ANY SECTION FAILING SUCH TESTS SHALL BE REMOVED AND REINSTALLED AT THE CONTRACTOR'S EXPENSE.

3 // FIRE HYDRANTS (FH)

A. INSTALLATION, COMMISSIONING AND TESTING OF FIRE HYDRANTS TO BE IN ACCRORDANCE WITH AS2419.
B. FIRE HYDRANTS ARE TO BE AS SHOWN ON THE DRAWINGS. THE CONTRACTOR IS TO ALLOW TO PLACE STANDARD MARKERS AS REQUIRED BY THE LOCAL AUTHORITY.

4 // THRUST AND ANCHOR BLOCKS

A. THRUST AND ANCHOR BLOCKS ARE TO BE PROVIDED AT BENDS, VALVES, HYDRANTS AND LINE ENDS IN ACCORDANCE WITH TASWATER STANDARDS.

5 // TRENCHING AND BACKFILLING

A. ALL TRENCHES ARE TO BE EXCAVATED AND BACKFILLED IN ACCORDANCE WITH THESE DRAWINGS AND TASWATER STANDARDS, INCLUDING ELECTROMAGNETIC METAL IMPREGNATED TAPE IN ALL NON METALLIC PIPE TRENCHES.
B. CEMENT STABILISED EMBEDMENT:
  B.1. THE LATEST VERSION OF DRAWING MRWA-W-208 (REV 3) INCLUDES TABLE 208_A WITH NOTE G INDICATING THAT WHEN TRENCHSTOPS OR BULKHEADS ARE USED (GRADES GREATER THAN 5%) CEMENT STABILISED EMBEDMENT MUST BE USED. THIS IS NOT TASWATER'S PREFERRED STANDARD.
  B.2. FOR PIPES UP TO 10% GRADE TASWATER WILL ACCEPT THE PREVIOUS REVISION OF MRWA (REV 2). IE PIPES UP TO 10% GRADE DO NOT REQUIRE CEMENT STABILISED EMBEDMENT UNLESS THE CONDITIONS OF NOTE H APPLY - 'WHEN SOCKETED MAINS ARE LAID AT >5% SLOPE IN AREAS THAT ARE LIKELY TO HAVE HIGH GROUND WATER, CEMENT STABILISED EMBEDMENT SHALL BE USED...'
  B.3. FOR PIPES AT GRADE GREATER THAN 10% MRWA-W-208 REV 3 REMAINS VALID.
  B.4. THE LATEST VERSION OF MRWA-W-203 (REV 2) EMBEDMENT SHALL BE ADOPTED NOTING THAT THE REQUIREMENT IDENTIFIED IN THE THIRD DOT POINT FOR TYPE B IN THE NOTES REGARDING TABLE 203-A SHALL BE AMENDED TO READ 'WHERE WATER MAIN GRADE >10%'.
  B.5. FURTHER TO THIS IT SHOULD BE NOTED THAT MOST WATER MAINS ARE LIKELY TO REQUIRE A TYPE A EMBEDMENT SYSTEM. THE VARIOUS MATERIALS AVAILABLE FOR THIS SYSTEM ARE IDENTIFIED IN TABLE 203-B.

6 // INSPECTIONS

A. THE CONTRACTOR IS TO BE RESPONSIBLE FOR ORGANISING INSPECTIONS WITH THE SUPERINTENDENT - LIAISE WITH LOCAL COUNCIL.
B. THE FOLLOWING SITE INSPECTIONS ARE REQUIRED DURING CONSTRUCTION / HOLD POINTS, AS A MINIMUM, BEFORE COMMENCEMENT OF FURTHER WORKS:

REQUIRED SITE INSPECTIONS:
PIPEWORK BEDDING
INSTALLED PIPE PRIOR TO BACKFILLING
BACKFILLING

C. THE CONTRACTOR IS TO MAKE THEMSELVES AWARE OF ANY ADDITIONAL INSPECTIONS REQUIRED BY TASWATER TO ACHIEVE PRACTICAL COMPLETION AND NOTIFY COLLECTIVE CONSULTING.
D. COLLECTIVE CONSULTING REQUIRE MIN. 48 HOURS NOTICE PRIOR TO ALL REQUIRED INSPECTIONS.

7 // PIPE CLEANING - 'DISINFECTION'

A. THE CONTRACTOR IS TO ALLOW TO CLEANSE WATER MAINS BY FLUSHING WITH SODIUM HYPOCHLORITE (OR SIMILAR), AS DIRECTED BY THE LOCAL AUTHORITY.

8 // AS CONSTRUCTED DRAWINGS

A. THE CONTRACTOR WILL BE RESPONSIBLE FOR PRODUCING 'AS INSTALLED' DRAWINGS TO THE STANDARD REQUIRED BY TASWATER.
B. THE DRAWINGS SHALL BE CERTIFIED AS BEING CORRECT BY EITHER A CHARTERED CIVIL ENGINEER OR A REGISTERED SURVEYOR.
C. COLLECTIVE CONSULTING CAN PROVIDE THIS SERVICE, AT AN ADDITIONAL FEE. THIS HAS NOT BEEN ALLOWED FOR AS PART OF THESE WORKS / CONTRACT.

9 // PROPERTY WATER CONNECTIONS

A. ALL PROPERTY CONNECTIONS SHALL BE CONSTRUCTED IN ACCORDANCE WITH MRWA-W-110, MRWA-W-111 AND TASWATER TW-W-0002 SERIES STANDARD DRAWINGS.
A. UNLESS NOTED OTHERWISE, PROPERTY WATER CONNECTIONS SHALL BE DN25 (Ø20 ID) HDPE (PE100) SDR 11 PN16 PIPES. WHERE INSTALLED UNDER ROADS, PIPES SHALL BE SLEEVED IN DN100 SN4 PIPES, FITTED WITH TRACE AND TIGHT FITTING RUBBER WRAPS AT 2M MAX. CENTRES TO PREVENT WATER HAMMER.

10 // WATER MAINS CONNECTIONS

A. ALL NEW 'LIVE' CONNECTIONS TO EXISTING TASWATER WATER INFRASTRUCTURE ARE TO BE COMPLETED BY TASWATER (UNLESS PRIOR WRITTEN APPROVAL) AT OWNERS COST.

11 // MINIMUM COVER

A. MINIMUM COVER FOR WATER LINES ARE TO BE:

CONDITIONS / POSITION:	**MINIMUM COVER:**
UNDER ROADWAYS AND VEHICLE CROSSOVERS (EXCLUDING MAJOR ROADWAYS)	750mm
RESIDENTIAL LAND	450mm
NON-RESIDENTIAL LAND	600mm

12 // TASWATER APPLICATIONS AND SIGN-OFF

A. THE CONTRACTOR IS RESPONSIBLE FOR LODGING ALL FURTHER APPLICATIONS FOR THE CONNECTION OR DISCONNECTION OF ANY LIVE TAPPINGS, PROPERTY CONNECTIONS, ETC.
B. THE CONTRACTOR IS RESPONSIBLE FOR ENSURING ALL FINAL SIGN OFF BY TASWATER.

GLENORCHY CITY COUNCIL
PLANNING SERVICES
APPLICATION No. : PLN-24-270
DATE RECEIVED: 12/06/2025

THESE DRAWINGS ARE TO BE PRINTED AND READ IN COLOUR.
A FULL SIZED PRINTED COLOUR COPY SHOULD BE RETAINED ON SITE AT ALL TIMES FOR CONTRACTORS AND SUB-CONSULTANTS COMPLETING THE WORKS.

REV:	ISSUED FOR / DESCRIPTION:	BY:	DATE:
E	REVISED DEVELOPMENT APPLICATION	OWM	18-02-25
D	REVIEW / INFORMATION	OWM	07-02-25
C	REVISED DEVELOPMENT APPLICATION	OWM	07-11-24
B	DEVELOPMENT APPLICATION	OWM	27-09-24
A	REVIEW / INFORMATION	OWM	26-09-24

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CLIENT / ARCHITECT:
CUNIC HOMES

PROJECT DETAILS:
168a ABBOTSFIELD ROAD, CLAREMONT
UNITS DEVELOPMENT

DRAWING TITLE:
CIVIL NOTES

DESIGN BY:	DESIGN CHECK:	DRAWN BY:	DRAFT CHECK:	CERTIFIER:
AJL	JTA	OWM	JTA	

SCALE:	PROJECT No:	DRAWING No:	REVISION:
-	241043	C001	E

REV:	ISSUED FOR / DESCRIPTION:	BY:	DATE:
B	REVISED DEVELOPMENT APPLICATION	OWM	18-02-25
A	REVIEW / INFORMATION	OWM	07-02-25

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168a ABBOTSFIELD ROAD, CLAREMONT
UNITS DEVELOPMENT

DESIGN BY:	DESIGN CHECK:	DRAWN BY:	DRAFT CHECK:	CERTIFIER:
AJL	JTA	OWM	JTA	

DRAWING TITLE:
OVERALL SITE PLAN

SCALE:	PROJECT No:	DRAWING No:	REVISION:
1:250 @ A1 1:500 @ A3	241043	C100	B

TRUE NORTH

PROJECT NORTH

CAUTION:
EXTG WATER LINE / MAIN

TITLE BOUNDARY: 73.15

DEMOLISH EXTG 20mm CONNECTION (CODE)
AND REPLACE WITH NEW 40mm WATER CONNECTION

DEMOLISH AND REMOVE EXTG FENCE
POSTS. PREPARE AREA FOR NEW WORKS

eMH-S
CL: 102.940
IL: 100.972

CAUTION:
EXTG SEWER LINE / MAIN

TITLE BOUNDARY: 36.88

168a ABBOTSFIELD ROAD, CLAREMONT TAS 7011
PROPERTY ID: 9550943
TITLE REFERENCE: 61276/27
AREA: 2675m²

TITLE BOUNDARY: 73.15

GLENORCHY CITY COUNCIL
PLANNING SERVICES
APPLICATION No. : PLN-24-270
DATE RECEIVED: 12/06/2025

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DANGER
OVERHEAD POWER LINES

EXISTING SITE / DEMOLITION NOTES:

1. THE FOLLOWING IS TO BE READ IN CONJUNCTION WITH NOTES ON DRAWING C001.
2. LOCATE ALL EXISTING UNDERGROUND INFRASTRUCTURE PRIOR TO COMMENCING ANY SITE AND DEMOLITION WORKS. REFER 'EXISTING INFRASTRUCTURE' NOTES ON DRAWING C001 FOR METHODS.
3. ANY CLASHES WITH DESIGNED INFRASTRUCTURE ON THE FOLLOWING DESIGN DRAWINGS ARE TO BE REPORTED TO DESIGN ENGINEER FOR DIRECTION.
4. ALL EXISTING INFRASTRUCURE IS TO BE PROTECTED DURING CONSTRUCTION. ANY DAMAGE TO EXISTING INFRASTRUCTURES IS TO BE MADE GOOD AT THE CONTRACTOR'S EXPENSE.
5. CONTRACTOR TO MAKE ALL NECESSARY ALLOWANCES FOR REQUIRED DEMOLITIONS, REMOVALS AND RELOCATIONS TO SUIT NEW WORKS. REFER 'DEMOLITION WORKS' NOTES ON DRAWING C001.
6. CAP, TERMINATE AND REMOVE REDUNDANT DISUSED DRAINAGE SERVICES TO SATISFACTION OF ENGINEER, LOCAL AUTHORITIES AND IN ACCORDANCE WITH AS3500.
7. TRENCHES WHERE SERVICES ARE REMOVED ARE TO BE FILLED WITH AN APPROVED COMPACTED MATERIAL AND TO ENGINEERS COMPACTION SPECIFICATIONS. MATCH AND MAKE GOOD EXISTING SURFACES TO MATCH EXISTING SURROUNDINGS.
8. CONTRACTOR TO ALLOW TO MAKE GOOD ALL SURFACES AFFECTED BY DEMOLITION WORKS TO SUPERINTENDENT / PRINCIPAL SATISFACTION.

EXISTING SITE LEGEND:

CL	COVER LEVEL
e / EXTG	EXISTING ITEM / ELEMENT
eEP	EXISTING ELECTRICAL / COMMS PIT
eFP	EXISTING FIRE PLUG
eGP	EXISTING GRATED / GULLY PIT
eHW	EXISTING HEADWALL
eMH	EXISTING MANHOLE
ePP	EXISTING POWER / LIGHT POLE
eSV	EXISTING STOP / SWITCH VALVE
eTP	EXISTING TELSTRA PIT
IL	INVERT LEVEL
S	SEWER
SW	STORMWATER
W	WATER

EXISTING SEWER PIPE SCHEDULE

MARK	EXISTING PIPE SIZE	EXISTING PIPE TYPE
eS-1	DN150	PVC

EXISTING WATER MAIN SCHEDULE

MARK	EXISTING PIPE SIZE	EXISTING PIPE TYPE
eW-1	DN100	DUCTILE IRON CEMENT LINED

DEMOLITION LEGEND:

D1 DEMOLISH EXTG BUILDINGS (INCLUDING ROOFS, WALLS, FLOORS, FOOTINGS, ETC.)
CAP AND TERMINATE REDUNDANT INFRASTRUCTURE CLEAR OF NEW WORKS
READ IN CONJUNCTION WITH ARCH. DEMOLITION PLANS - ALL WORKS TO A SAW CUT EDGE
MAKE GOOD AREAS AND EXTG SURFACES AND PREPARE FOR NEW WORKS

D2 DEMOLISH AND REMOVE EXTG FENCING / SCREENING / ETC. TO EXTENT SHOWN
READ IN CONJUNCTION WITH ARCH. DEMOLITION PLANS
MAKE GOOD AREAS AND EXTG SURFACES AND PREPARE FOR NEW WORKS

D3 DEMOLISH AND REMOVE EXTG LANDSCAPING / PLANTING / SOFT AREAS / ETC.
READ IN CONJUNCTION WITH ARCH. DEMOLITION PLANS
MAKE GOOD AREAS AND EXTG SURFACES AND PREPARE FOR NEW WORKS

D4 DEMOLISH AND REMOVE EXTG CONC. AND GRAVEL HARDSTANDS
READ IN CONJUNCTION WITH ARCH. DEMOLITION PLANS
MAKE GOOD AREAS AND EXTG SURFACES AND PREPARE FOR NEW WORKS

W1 DEMOLISH AND REMOVE EXISTING WATER INFRASTRUCTURE
WORKS TO BE COMPLETED BY TASWATER AT DEVELOPERS COST
MAKE GOOD AREAS ADJACENT AND PREPARE FOR NEW WORKS

P1 DEMOLISH AND RELOCATE EXISTING POWER POLE AND POWER LINES
WORKS TO BE COMPLETED BY TASNETWORKS AT DEVELOPERS COST
MAKE GOOD AREAS ADJACENT AND PREPARE FOR NEW WORKS

REV:	ISSUED FOR / DESCRIPTION:	BY:	DATE:
E	REVISED DEVELOPMENT APPLICATION	OWM	18-02-25
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AJL	JTA	OWM	JTA	

DRAWING TITLE:
EXISTING SITE / DEMOLITION PLAN

SCALE:	PROJECT No:	DRAWING No:	REVISION:
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Document Set ID: 3503389
Version: 2, Version Date: 26/06/2025

TRUE NORTH

PROJECT NORTH

1-1
eMH-S
CL: 101.439
IL: 100.599

eMH-S
CL: 102.940
IL: 100.972

DEMOLISH EXTG 20mm CONNECTION (CODE) AND REPLACE WITH NEW 40mm WATER CONNECTION

DEMOLISH AND REMOVE EXTG FENCE POSTS. PREPARE AREA FOR NEW WORKS

CAUTION: EXTG SEWER LINE / MAIN

CAUTION: EXTG SEWER LINE / MAIN

DEMOLISH EXTG DRIVEWAY, HEADWALL AND PIPE.

DEMOLISH EXTG HOUSE SWTR OUTLET REALIGN WITH NEW VEE DRAIN TYP

DEMOLISH EXTG HOUSE SWTR OUTLET REALIGN WITH NEW VEE DRAIN TYP

CAUTION: EXTG. OVERHEAD POWER LINES

TITLE BOUNDARY: 36.88

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DANGER OVERHEAD POWER LINES

GLENORCHY CITY COUNCIL PLANNING SERVICES

APPLICATION No. : PLN-24-270

DATE RECEIVED: 12/06/2025

EXISTING SITE / DEMOLITION NOTES:

1. THE FOLLOWING IS TO BE READ IN CONJUNCTION WITH NOTES ON DRAWING C001.
2. LOCATE ALL EXISTING UNDERGROUND INFRASTRUCTURE PRIOR TO COMMENCING ANY SITE AND DEMOLITION WORKS. REFER 'EXISTING INFRASTRUCTURE' NOTES ON DRAWING C001 FOR METHODS.
3. ANY CLASHES WITH DESIGNED INFRASTRUCTURE ON THE FOLLOWING DESIGN DRAWINGS ARE TO BE REPORTED TO DESIGN ENGINEER FOR DIRECTION.
4. ALL EXISTING INFRASTRUCURE IS TO BE PROTECTED DURING CONSTRUCTION. ANY DAMAGE TO EXISTING INFRASTRUCTURES IS TO BE MADE GOOD AT THE CONTRACTOR'S EXPENSE.
5. CONTRACTOR TO MAKE ALL NECESSARY ALLOWANCES FOR REQUIRED DEMOLITIONS, REMOVALS AND RELOCATIONS TO SUIT NEW WORKS. REFER 'DEMOLITION WORKS' NOTES ON DRAWING C001.
6. CAP, TERMINATE AND REMOVE REDUNDANT DISUSED DRAINAGE SERVICES TO SATISFACTION OF ENGINEER, LOCAL AUTHORITIES AND IN ACCORDANCE WITH AS3500.
7. TRENCHES WHERE SERVICES ARE REMOVED ARE TO BE FILLED WITH AN APPROVED COMPACTED MATERIAL AND TO ENGINEERS COMPACTION SPECIFICATIONS. MATCH AND MAKE GOOD EXISTING SURFACES TO MATCH EXISTING SURROUNDINGS.
8. CONTRACTOR TO ALLOW TO MAKE GOOD ALL SURFACES AFFECTED BY DEMOLITION WORKS TO SUPERINTENDENT / PRINCIPAL SATISFACTION.

EXISTING SITE LEGEND:

CL	COVER LEVEL
e / EXTG	EXISTING ITEM / ELEMENT
eEP	EXISTING ELECTRICAL / COMMS PIT
eFP	EXISTING FIRE PLUG
eGP	EXISTING GRATED / GULLY PIT
eHW	EXISTING HEADWALL
eMH	EXISTING MANHOLE
ePP	EXISTING POWER / LIGHT POLE
eSV	EXISTING STOP / SWITCH VALVE
eTP	EXISTING TELSTRA PIT
IL	INVERT LEVEL
S	SEWER
SW	STORMWATER
W	WATER

EXISTING SEWER PIPE SCHEDULE

MARK	EXISTING PIPE SIZE	EXISTING PIPE TYPE
eS-1	DN150	PVC

EXISTING WATER MAIN SCHEDULE

MARK	EXISTING PIPE SIZE	EXISTING PIPE TYPE
eW-1	DN100	DUCTILE IRON CEMENT LINED

DEMOLITION LEGEND:

D1 DEMOLISH EXTG BUILDINGS (INCLUDING ROOFS, WALLS, FLOORS, FOOTINGS, ETC.)
CAP AND TERMINATE REDUNDANT INFRASTRUCTURE CLEAR OF NEW WORKS
READ IN CONJUNCTION WITH ARCH. DEMOLITION PLANS - ALL WORKS TO A SAW CUT EDGE
MAKE GOOD AREAS AND EXTG SURFACES AND PREPARE FOR NEW WORKS

D2 DEMOLISH AND REMOVE EXTG FENCING / SCREENING / ETC. TO EXTENT SHOWN
READ IN CONJUNCTION WITH ARCH. DEMOLITION PLANS
MAKE GOOD AREAS AND EXTG SURFACES AND PREPARE FOR NEW WORKS

D3 DEMOLISH AND REMOVE EXTG LANDSCAPING / PLANTING / SOFT AREAS / ETC.
READ IN CONJUNCTION WITH ARCH. DEMOLITION PLANS
MAKE GOOD AREAS AND EXTG SURFACES AND PREPARE FOR NEW WORKS

D4 DEMOLISH AND REMOVE EXTG CONC. AND GRAVEL HARDSTANDS
READ IN CONJUNCTION WITH ARCH. DEMOLITION PLANS
MAKE GOOD AREAS AND EXTG SURFACES AND PREPARE FOR NEW WORKS

W1 DEMOLISH AND REMOVE EXISTING WATER INFRASTRUCTURE
WORKS TO BE COMPLETED BY TASWATER AT DEVELOPERS COST
MAKE GOOD AREAS ADJACENT AND PREPARE FOR NEW WORKS

P1 DEMOLISH AND RELOCATE EXISTING POWER POLE AND POWER LINES
WORKS TO BE COMPLETED BY TASNETWORKS AT DEVELOPERS COST
MAKE GOOD AREAS ADJACENT AND PREPARE FOR NEW WORKS

REV:	ISSUED FOR / DESCRIPTION:	BY:	DATE:
B	REVISED DEVELOPMENT APPLICATION	OWM	18-02-25
A	REVIEW / INFORMATION	OWM	07-02-25

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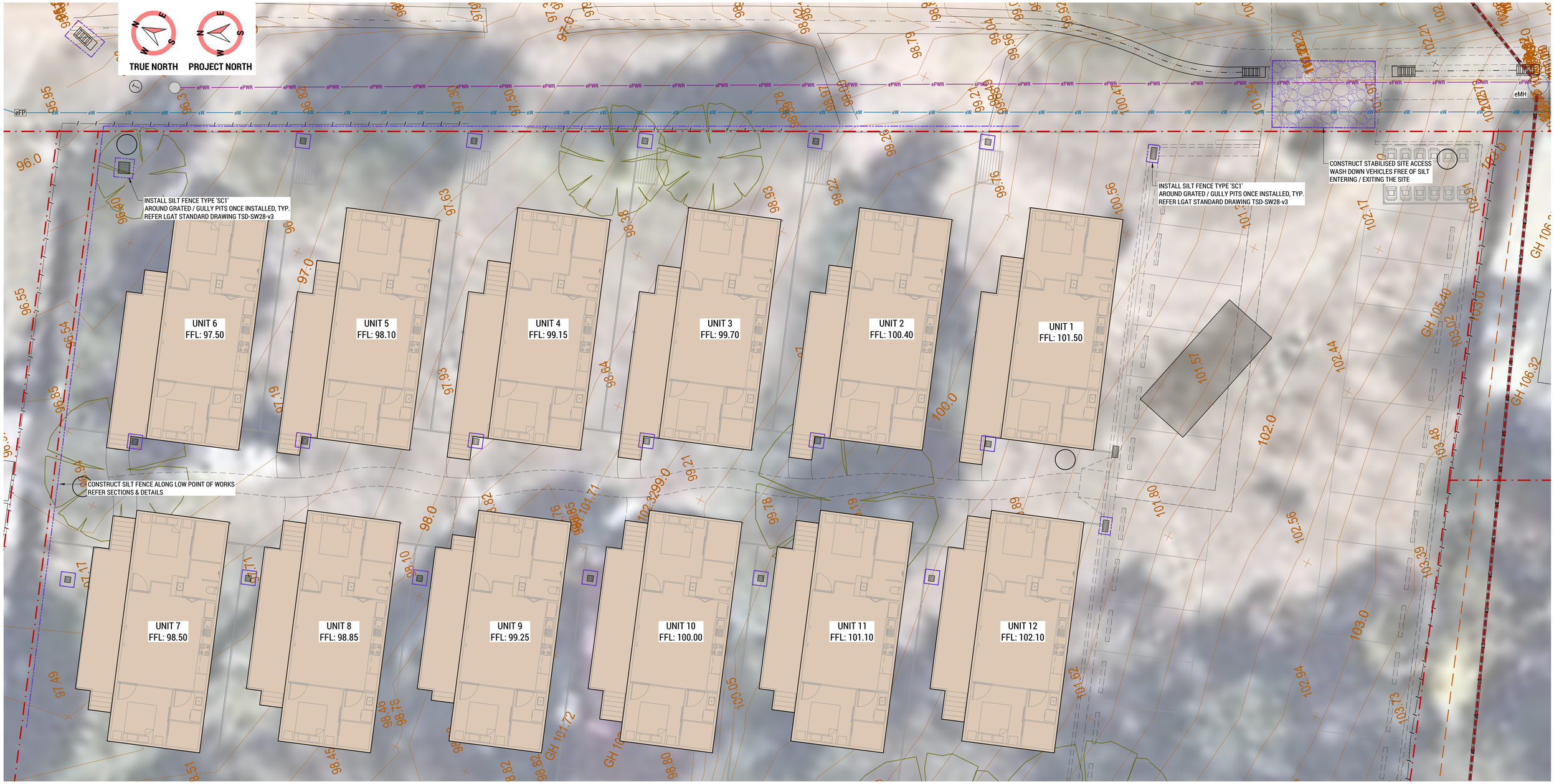
CLIENT / ARCHITECT:
CUNIC HOMES
CUNIC homes Built for you

PROJECT DETAILS:
168a ABBOTSFIELD ROAD, CLAREMONT
UNITS DEVELOPMENT

DESIGN BY:	DESIGN CHECK:	DRAWN BY:	DRAFT CHECK:	CERTIFIER:
AJL	JTA	OWM	JTA	

DRAWING TITLE:
EXISTING SITE / DEMOLITION PLAN

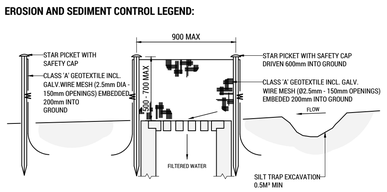
SCALE:	PROJECT No:	DRAWING No:	REVISION:
1:100 @ A1 (1:200 @ A3)	241043	C102	B



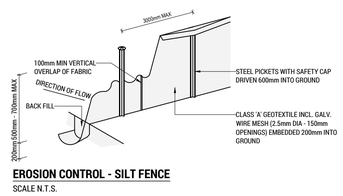
EROSION AND SEDIMENT CONTROL NOTES:

1. THE FOLLOWING IS TO BE READ IN CONJUNCTION WITH NOTES ON DRAWING C001.
2. COMPLY WITH ALL REQUIREMENTS TO LIMIT STORMWATER RUNOFF FROM THE SITE DURING CONSTRUCTION.
3. EROSION AND SEDIMENT CONTROL MEASURES TO BE CARRIED OUT IN ACCORDANCE WITH 'SOIL AND WATER MANAGEMENT ON BUILDING AND CONSTRUCTION SITES' GUIDELINES AVAILABLE FROM NORTHERN RESOURCE MANAGEMENT (NRM) AND DETAILS SUPPLIED IN THESE DESIGN DRAWINGS.
4. REFER 'SOIL AND WATER MANAGEMENT' NOTES ON DRAWING C001 FOR ADDITIONAL EROSION AND SEDIMENT CONTROL MEASURES.
5. INSTALL SILT FENCES AND TRAPS TO PREVENT SEDIMENTS AND POLLUTANTS ENTERING STORMWATER SYSTEM OR NATURAL DRAINAGE LINES.
6. REFER LGAT STANDARD DRAWING TSD-SW28-v3 FOR GUIDELINE / DETAILS FOR SEDIMENT CONTROL. CONSTRUCT AS DETAILED AND INSTALL CLASS 'A' GEOTEXTILE OR USE PROPRIETARY SILT FENCE, EG; MACCAFERRI 'SILT LOK'.
7. ALL RUNOFF AND SEDIMENT CONTROL STRUCTURES TO BE INSPECTED AND CLEANED DAILY TO PREVENT BREAKAGE / OVERTOPPING AND MAINTAINED IN A FUNCTIONING CONDITION.
8. IT IS THE RESPONSIBILITY OF THE DEVELOPER TO INSTALL, MAINTAIN AND (UPON COMPLETION) REMOVE ALL TEMPORARY SEDIMENT CONTROL MEASURES.
9. ALL VEGETATION OUTSIDE OF THE BUILDING ENVELOPE TO BE RETAINED WHERE ABLE.
10. IT IS STRONGLY RECOMMENDED THAT THE DEVELOPER RE-COVERS ANY DISTURBED AREAS WITH TOPSOIL AS QUICKLY AS POSSIBLE AFTER BULK EARTHWORKS ARE COMPLETED, TO PREVENT SOIL DISPERSION.
11. STOCK PILING OF SOILS OR OTHER MATERIALS AFFECTED BY WATER TO BE STORED CLEAR OF ANY DRAINAGE PATHS.
12. WASH DOWN VEHICLES FREE OF SILT ENTERING / EXITING THE SITE.

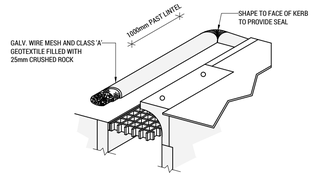
EROSION AND SEDIMENT CONTROL LEGEND:



EROSION CONTROL - SILT FENCE AT GRATE PIT - SC1
SCALE N.T.S.



EROSION CONTROL - SILT FENCE
SCALE N.T.S.



SILT FENCE
CONSTRUCT AS DETAILED AND INSTALL CLASS 'A' GEOTEXTILE OR USE PROPRIETARY SILT FENCE, EG; MACCAFERRI 'SILT LOK'.

OMIT SANDBAG WALL AND SILTTRAP WHEN PIT IS IN A LOW POINT.

GULLY PIT
GALVANIZED WIRE MESH 2mm DIA x 12mm OPENING.

GENERAL
SEDIMENT FENCES ARE TO BE CLEANED DAILY TO PREVENT BREAKAGE/OVERTOPPING.

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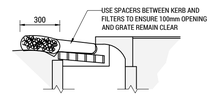
NOTE:
INSTALL SILT MANAGEMENT AS REQUIRED. LOCATIONS TO BE CONFIRMED ON SITE. ENSURE SILT MANAGEMENT COMPLIES WITH CURRENT COUNCIL STANDARDS AND REQUIREMENTS.

EROSION CONTROL - SILT FENCE AT SIDE ENTRY PIT - SC3
SCALE N.T.S.



EROSION CONTROL - SILT FENCE AT SIDE ENTRY PIT - SC3
SCALE N.T.S.

GLENORCHY CITY COUNCIL PLANNING SERVICES

APPLICATION No. : PLN-24-270

DATE RECEIVED: 12/06/2025

REV:	ISSUED FOR / DESCRIPTION:	BY:	DATE:
B	**REVISED DEVELOPMENT APPLICATION**	SCP	29-05-25
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PROJECT DETAILS:
168a ABBOTSFIELD ROAD, CLAREMONT
UNITS DEVELOPMENT

DRAWING TITLE:
EROSION CONTROL PLAN

DESIGN BY:	DESIGN CHECK:	DRAWN BY:	DRAFT CHECK:	CERTIFIER:
AJL	JTA	OWM	JTA	

SCALE:	PROJECT No:	DRAWING No:	REVISION:
1:100 @ A1 (1:200 @ A3)	241043	C201	B

TRUE NORTH

PROJECT NORTH

CONSTRUCT SILT FENCE ALONG LOW POINT OF WORKS
REFER SECTIONS & DETAILS

CONSTRUCT STABILISED SITE ACCESS
WASH DOWN VEHICLES FREE OF SILT
ENTERING / EXITING THE SITE

INSTALL SILT FENCE TYPE 'SC1'
AROUND GRATED / GULLY PITS ONCE INSTALLED, TYP.
REFER LGAT STANDARD DRAWING TSD-SW28-v3

CONSTRUCT STABILISED SITE ACCESS
WASH DOWN VEHICLES FREE OF SILT
ENTERING / EXITING THE SITE

eMH

EROSION AND SEDIMENT CONTROL NOTES:

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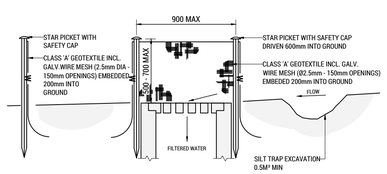
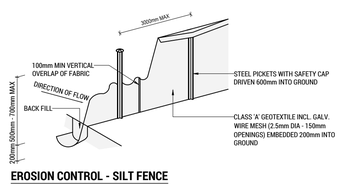
11. STOCK PILING OF SOILS OR OTHER MATERIALS AFFECTED BY WATER TO BE STORED CLEAR OF ANY DRAINAGE PATHS.
12. WASH DOWN VEHICLES FREE OF SILT ENTERING / EXITING THE SITE.

EROSION AND SEDIMENT CONTROL LEGEND:



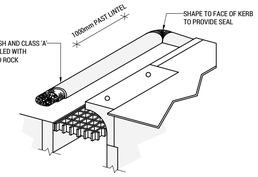
EROSION CONTROL - SILT FENCE AT GRATE PIT - SC1

SCALE N.T.S.



EROSION CONTROL - SILT FENCE

SCALE N.T.S.



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CONSTRUCT AS DETAILED AND INSTALL CLASS 'A' GEOTEXTILE OR USE PROPRIETARY SILT FENCE. EG; MACCAFERRI 'SILT LOK'.

OMIT SANDBAG WALL AND SILTTRAP WHEN PIT IS IN A LOW POINT.

GULLY PIT
GALVANIZED WIRE MESH 2mm DIA x 12mm OPENING.

GENERAL
SEDIMENT FENCES ARE TO BE CLEANED DAILY TO PREVENT BREAKAGE/OVERTOPPING.

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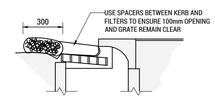
NOTE:
INSTALL SILT MANAGEMENT AS REQUIRED. LOCATIONS TO BE CONFIRMED ON SITE. ENSURE SILT MANAGEMENT COMPLIES WITH CURRENT COUNCIL STANDARDS AND REQUIREMENTS.

EROSION CONTROL - SILT FENCE AT SIDE ENTRY PIT - SC3

SCALE N.T.S.



EROSION CONTROL - SILT FENCE AT SIDE ENTRY PIT - SC3

SCALE N.T.S.

GLENORCHY CITY COUNCIL PLANNING SERVICES

APPLICATION No. : PLN-24-270

DATE RECEIVED: 12/06/2025

REV:	ISSUED FOR / DESCRIPTION:	BY:	DATE:
A	REVISED DEVELOPMENT APPLICATION	OWM	18-02-25

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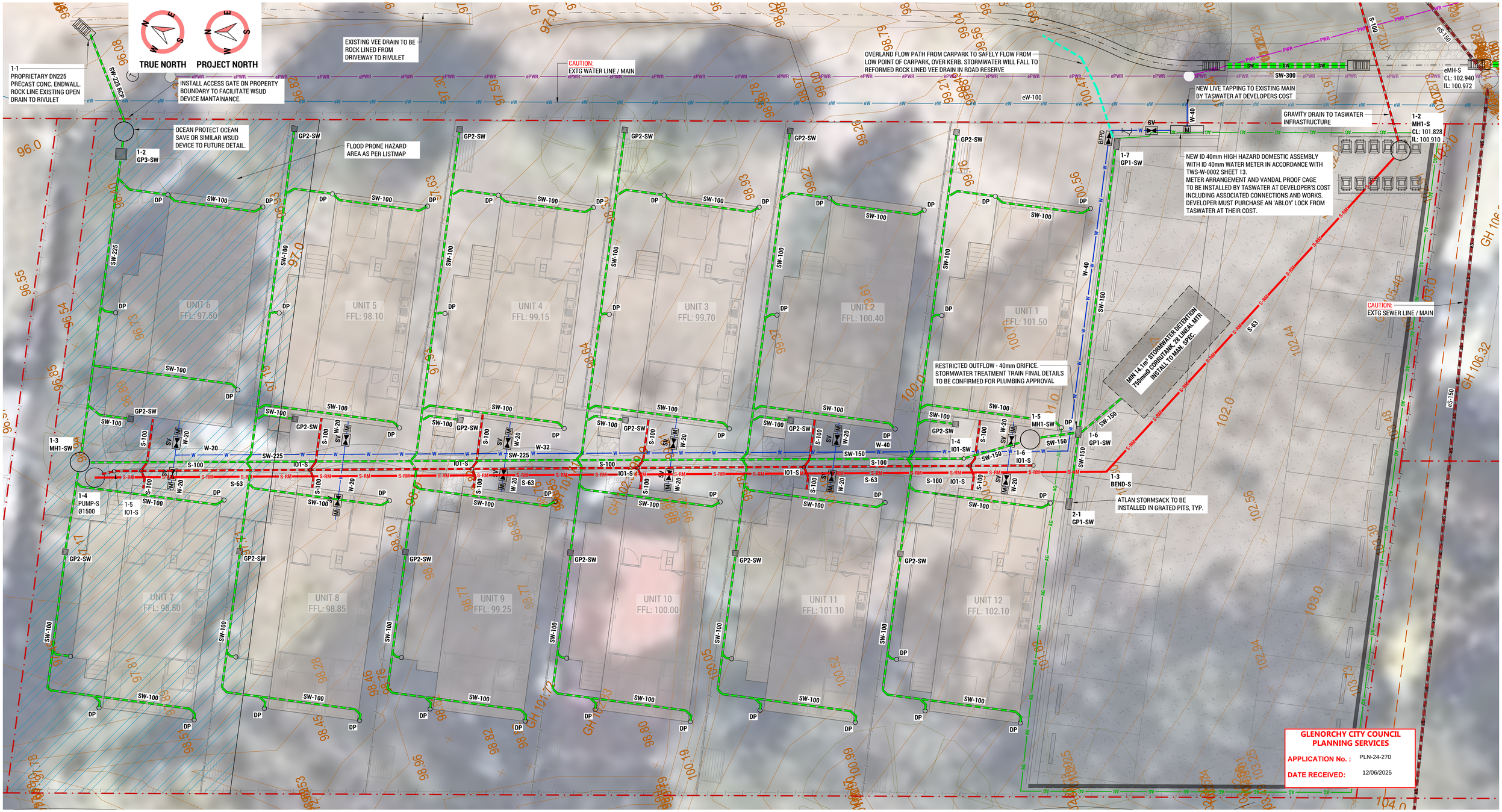
PROJECT DETAILS:
168a ABBOTSFIELD ROAD, CLAREMONT
UNITS DEVELOPMENT

DESIGN BY:	DESIGN CHECK:	DRAWN BY:	DRAFT CHECK:	CERTIFIER:
AJL	JTA	OWM	JTA	

DRAWING TITLE:
EROSION CONTROL PLAN

SCALE:	PROJECT No:	DRAWING No:	REVISION:
1:100 @ A1 (1:200 @ A3)	241043	C202	A

Document Set ID: 3503383
Version: 2, Version Date: 26/06/2025



INFRASTRUCTURE NOTES:

1. THE FOLLOWING IS TO BE READ IN CONJUNCTION WITH NOTES ON DRAWING C001.
2. STORMWATER PIPES SHALL BE INSTALLED WITH MIN. 0.5% GRADE FOR SIZES Ø225 AND ABOVE UNLESS NOTED / SCHEDULED OTHERWISE.
3. STORMWATER PIPES SHALL BE INSTALLED WITH MIN. 1.0% GRADE FOR PIPE SIZES Ø150 AND BELOW UNLESS NOTED / SCHEDULED OTHERWISE.
4. SEWER PIPES SHALL BE INSTALLED WITH MIN. 1.0% GRADE FOR PIPE SIZES Ø150 AND AND ABOVE UNLESS NOTED / SCHEDULED OTHERWISE.
5. SEWER PIPES SHALL BE INSTALLED WITH MIN. 1.65% GRADE FOR PIPE SIZES Ø100 AND BELOW UNLESS NOTED / SCHEDULED OTHERWISE.
6. ALL 'DN' SIZES SCHEDULED OR NOTED INDICATE INTERNAL DIAMETER.
7. REFER SECTIONS AND DETAILS FOR PIPE TRENCHING SPEC'S.
8. WATER LINES SHALL GENERALLY BE LAID ABOVE SEWER PIPES WHEREVER POSSIBLE.
9. ALL PIPES SHALL BE INSTALLED WITH MIN. 750mm COVER (U.N.O.)

INFRASTRUCTURE LEGEND:

CL	COVER LEVEL
DN	NOMINAL PIPE DIAMETER - INTERNAL DIAMETER (U.N.O.)
DP	DOWNPIPE - AS SCHEDULED
e / EXTG	EXISTING ITEM / ELEMENT
FH	FIRE HYDRANT - REFER SECTIONS AND DETAILS
FM	FIRE WATER SERVICE LINE / MAIN
FP	FIRE PLUG
GD	GRATED DRAIN - AS SCHEDULED / REFER SECTIONS AND DETAILS
GP	GRATED / GULLY PIT - AS SCHEDULED / REFER SECTIONS AND DETAILS
GVP	GRATED VEE PIT - AS SCHEDULED / REFER SECTIONS AND DETAILS
HBC	HOSE BIB COCK
IL	INVERT LEVEL
IO	INSPECTION OPENING - FINISHED TO SURFACE LEVEL
M	METER
MH	MANHOLE - AS SCHEDULED / REFER SECTIONS AND DETAILS
ORG	OVERFLOW RELIEF GULLY
OSDW	ONSITE STORMWATER UNDERGROUND DETENTION SYSTEM
RL	REDUCED LEVEL
S	SEWER
SEP	SIDE ENTRY PIT - AS SCHEDULED / REFER SECTIONS AND DETAILS
SM	SUB-METER
SV	STOP / SWITCH VALVE
SW	STORMWATER
VD	VEE DRAIN - AS SCHEDULED / REFER SECTIONS AND DETAILS
W	WATER

STORMWATER PIPE SCHEDULE

MARK	PIPE SIZE	TYPE	CLASS	GRADE
SW-100	DN100	PVC	SN8	MIN 1%
SW-150	DN150	PVC	SN8	MIN 1%
SW-225	DN225	BLACKMAX	SN8	MIN 1%
SW-225 RCP	DN225	REINFORCED CONC	CLASS 4	MIN 1%
SW-300	DN300	BLACKMAX	SN8	MIN 1%

STORMWATER DRAIN / PIT / MANHOLE SCHEDULE

MARK	SIZE	TYPE	ACCESSORIES
GP1-SW	600 x 300	PRECAST CONC.	GULLY PIT INTEGRAL WITH KERB
GP2-SW	300 x 300	BLACK PLASTIC	S/S GRATED LID
GP3-SW	600 x 600	PRECAST CONC.	CLASS 'B' GALV. GRATE
MH1-SW	Ø1050	PRECAST CONC.	CLASS 'D' GATIC TRAFFICABLE LID
IO1-SW	DN100	PVC	PLASTIC NON-TRAFFICABLE LID
HW1-SW	MATCH PIPE	PRECAST CONC.	-

EXISTING SEWER PIPE SCHEDULE

MARK	EXISTING PIPE SIZE	EXISTING PIPE TYPE
eS-150	DN150	PVC

SEWER PIPE SCHEDULE

MARK	PIPE SIZE	TYPE	CLASS	GRADE
S-63	DN63	RISING MAIN	PEX 100	N/A
S-100	DN100	PVC	SN8	MIN 1.67%

SEWER PIT / MANHOLE SCHEDULE

MARK	SIZE	TYPE	ACCESSORIES
MH1-S	Ø1050	PRECAST CONC.	CLASS 'D' GATIC TRAFFICABLE LID
PUMP-S	Ø1500	PRECAST CONC	SEWER PUMP
IO1-S	DN100	PVC	PLASTIC NON-TRAFFICABLE LID

EXISTING WATER MAIN SCHEDULE

MARK	EXISTING PIPE SIZE	EXISTING PIPE TYPE
eW-100	DN100	DUCTILE IRON CEMENT LINED
eW-150	DN150	CAST IRON

WATER MAIN SCHEDULE

MARK	PIPE SIZE	TYPE
W-20	ID 20 (OD 32)	PEX 100
W-32	ID 32 (OD 40)	HDPE SDR11 PN16
W-40	ID 40 (OD 50)	HDPE SDR11 PN16

REV:	ISSUED FOR / DESCRIPTION:	BY:	DATE:
F	REVISED DEVELOPMENT APPLICATION	SCP	29-05-25
E	REVISED DEVELOPMENT APPLICATION	OWM	18-02-25
D	REVIEW / INFORMATION	OWM	07-02-25
C	REVISED DEVELOPMENT APPLICATION	OWM	07-11-24
B	DEVELOPMENT APPLICATION	OWM	27-09-24
A	REVIEW / INFORMATION	OWM	26-09-24

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DRAWING TITLE:
INFRASTRUCTURE PLAN

DESIGN BY:	DESIGN CHECK:	DRAWN BY:	DRAFT CHECK:	CERTIFIER:
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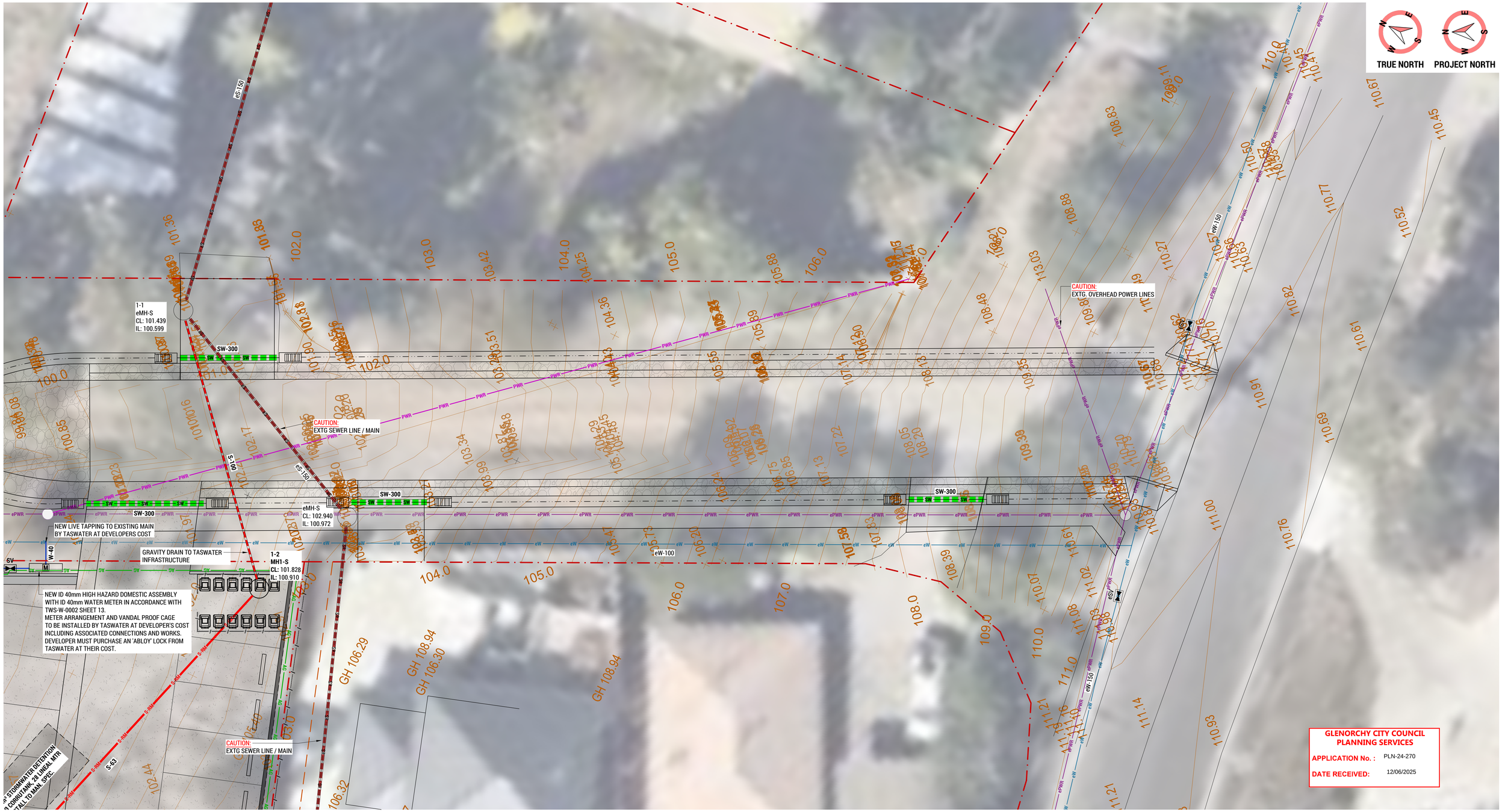
SCALE:	PROJECT No:	DRAWING No:	REVISION:
1:100 @ A1 (1:200 @ A3)	241043	C401	F



GLENORCHY CITY COUNCIL
PLANNING SERVICES
APPLICATION No. : PLN-24-270
DATE RECEIVED: 12/06/2025

INFRASTRUCTURE NOTES:

1. THE FOLLOWING IS TO BE READ IN CONJUNCTION WITH NOTES ON DRAWING C001.
2. STORMWATER PIPES SHALL BE INSTALLED WITH MIN. 0.5% GRADE FOR SIZES Ø225 AND ABOVE UNLESS NOTED / SCHEDULED OTHERWISE.
3. STORMWATER PIPES SHALL BE INSTALLED WITH MIN. 1.0% GRADE FOR PIPE SIZES Ø150 AND BELOW UNLESS NOTED / SCHEDULED OTHERWISE.
4. SEWER PIPES SHALL BE INSTALLED WITH MIN. 1.0% GRADE FOR PIPE SIZES Ø150 AND AND ABOVE UNLESS NOTED / SCHEDULED OTHERWISE.
5. SEWER PIPES SHALL BE INSTALLED WITH MIN. 1.65% GRADE FOR PIPE SIZES Ø100 AND BELOW UNLESS NOTED / SCHEDULED OTHERWISE.
6. ALL 'DN' SIZES SCHEDULED OR NOTED INDICATE INTERNAL DIAMETER.
7. REFER SECTIONS AND DETAILS FOR PIPE TRENCHING SPEC'S.
8. WATER LINES SHALL GENERALLY BE LAID ABOVE SEWER PIPES WHEREVER POSSIBLE.
9. ALL PIPES SHALL BE INSTALLED WITH MIN. 750mm COVER (U.N.O.)

INFRASTRUCTURE LEGEND:

CL	COVER LEVEL
DN	NOMINAL PIPE DIAMETER - INTERNAL DIAMETER (U.N.O.)
DP	DOWNPIPE - AS SCHEDULED
e / EXTG	EXISTING ITEM / ELEMENT
FH	FIRE HYDRANT - REFER SECTIONS AND DETAILS
FM	FIRE WATER SERVICE LINE / MAIN
FP	FIRE PLUG
GD	GRATED DRAIN - AS SCHEDULED / REFER SECTIONS AND DETAILS
GP	GRATED / GULLY PIT - AS SCHEDULED / REFER SECTIONS AND DETAILS
GVP	GRATED VEE PIT - AS SCHEDULED / REFER SECTIONS AND DETAILS
HBC	HOSE BIB COCK
IL	INVERT LEVEL
IO	INSPECTION OPENING - FINISHED TO SURFACE LEVEL
M	METER
MH	MANHOLE - AS SCHEDULED / REFER SECTIONS AND DETAILS
ORG	OVERFLOW RELIEF GULLY
OSDW	ONSITE STORMWATER UNDERGROUND DETENTION SYSTEM
RL	REDUCED LEVEL
S	SEWER
SEP	SIDE ENTRY PIT - AS SCHEDULED / REFER SECTIONS AND DETAILS
SM	SUB-METER
SV	STOP / SWITCH VALVE
SW	STORMWATER
VD	VEE DRAIN - AS SCHEDULED / REFER SECTIONS AND DETAILS
W	WATER

STORMWATER PIPE SCHEDULE

MARK	PIPE SIZE	TYPE	CLASS	GRADE
SW-100	DN100	PVC	SN8	MIN 1%
SW-150	DN150	PVC	SN8	MIN 1%
SW-225	DN225	BLACKMAX	SN8	MIN 1%
SW-225 RCP	DN225	REINFORCED CONC	CLASS 4	MIN 1%
SW-300	DN300	BLACKMAX	SN8	MIN 1%

STORMWATER DRAIN / PIT / MANHOLE SCHEDULE

MARK	SIZE	TYPE	ACCESSORIES
GP1-SW	600 x 300	PRECAST CONC.	GULLY PIT INTEGRAL WITH KERB
GP2-SW	300 x 300	BLACK PLASTIC	S/S GRATED LID
GP3-SW	600 x 600	PRECAST CONC.	CLASS 'B' GALV. GRATE
MH1-SW	Ø1050	PRECAST CONC.	CLASS 'D' GATIC TRAFFICABLE LID
IO1-SW	DN100	PVC	PLASTIC NON-TRAFFICABLE LID
HW1-SW	MATCH PIPE	PRECAST CONC.	-

EXISTING SEWER PIPE SCHEDULE

MARK	EXISTING PIPE SIZE	EXISTING PIPE TYPE
eS-150	DN150	PVC

SEWER PIPE SCHEDULE

MARK	PIPE SIZE	TYPE	CLASS	GRADE
S-63	DN63	RISING MAIN	PEX 100	N/A
S-100	DN100	PVC	SN8	MIN 1.67%

SEWER PIT / MANHOLE SCHEDULE

MARK	SIZE	TYPE	ACCESSORIES
MH1-S	Ø1050	PRECAST CONC.	CLASS 'D' GATIC TRAFFICABLE LID
PUMP-S	Ø1500	PRECAST CONC	SEWER PUMP
IO1-S	DN100	PVC	PLASTIC NON-TRAFFICABLE LID

EXISTING WATER MAIN SCHEDULE

MARK	EXISTING PIPE SIZE	EXISTING PIPE TYPE
eW-100	DN100	DUCTILE IRON CEMENT LINED
eW-150	DN150	CAST IRON

WATER MAIN SCHEDULE

MARK	PIPE SIZE	TYPE
W-20	ID 20 (OD 32)	PEX 100
W-32	ID 32 (OD 40)	HDPE SDR11 PN16
W-40	ID 40 (OD 50)	HDPE SDR11 PN16

REV:	ISSUED FOR / DESCRIPTION:	BY:	DATE:
E	REVISED DEVELOPMENT APPLICATION	OWM	18-02-25
D	REVIEW / INFORMATION	OWM	07-02-25
C	REVISED DEVELOPMENT APPLICATION	OWM	07-11-24
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A	REVIEW / INFORMATION	OWM	26-09-24

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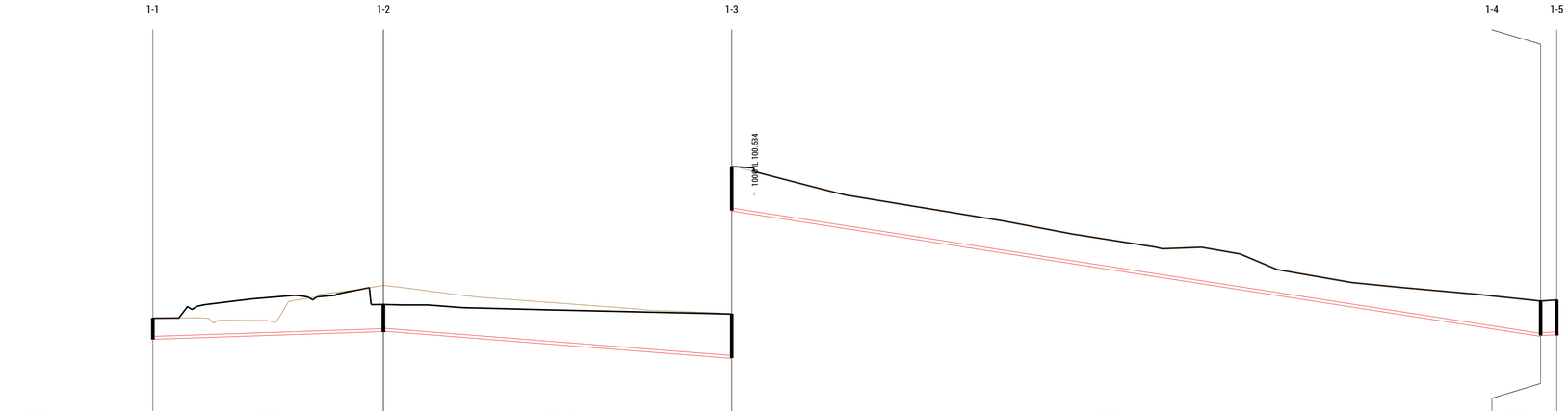
CLIENT / ARCHITECT:
CUNIC HOMES
CUNIC homes Built for you

PROJECT DETAILS:
168a ABBOTSFIELD ROAD, CLAREMONT
UNITS DEVELOPMENT

DRAWING TITLE:
INFRASTRUCTURE PLAN

DESIGN BY:	DESIGN CHECK:	DRAWN BY:	DRAFT CHECK:	CERTIFIER:
AJL	JTA	OWM	JTA	

SCALE:	PROJECT No:	DRAWING No:	REVISION:
1:100 @ A1 (1:200 @ A3)	241043	C402	E



PIPE DETAILS		100Ø PVC S		100Ø PVC S		100Ø PVC S		100Ø PVC S
SLOPE/GRADE		1.66%		-3.92%		-7.75%		2.29%
DATUM RL 96.7					RL 91.7			
DEPTH TO INVERT	0.713		0.918 0.903		1.508		1.194 1.169		1.180
INVERT LEVEL	100.650		100.910 100.925		100.000		95.750 95.775		95.800
FINISHED SURFACE	101.363		101.828		101.508		96.944		96.980
EXISTING SURFACE	101.363		102.476		101.523		96.944		96.980
CHAINAGE	0.000	15.631	15.631	23.617	39.248	54.838	94.087	1.094	95.180

LONGITUNDINAL SECTION FOR LINE 1
SCALES: HORIZONTAL 1:200 VERTICAL 1:100

PIPE DETAILS		100Ø PVC S	
SLOPE/GRADE		7.42%	
DATUM RL 91.8			
DEPTH TO INVERT	1.180		1.506
INVERT LEVEL	95.800		99.500
FINISHED SURFACE	96.980		101.006
EXISTING SURFACE	96.980		101.006
CHAINAGE	95.180	49.845	145.025

LONGITUNDINAL SECTION FOR LINE 1
SCALES: HORIZONTAL 1:200 VERTICAL 1:100

GLENORCHY CITY COUNCIL
PLANNING SERVICES
APPLICATION No. : PLN-24-270
DATE RECEIVED: 12/06/2025

REV:	ISSUED FOR / DESCRIPTION:	BY:	DATE:
B	REVISED DEVELOPMENT APPLICATION	OWM	18-02-25
A	REVIEW / INFORMATION	OWM	07-02-25

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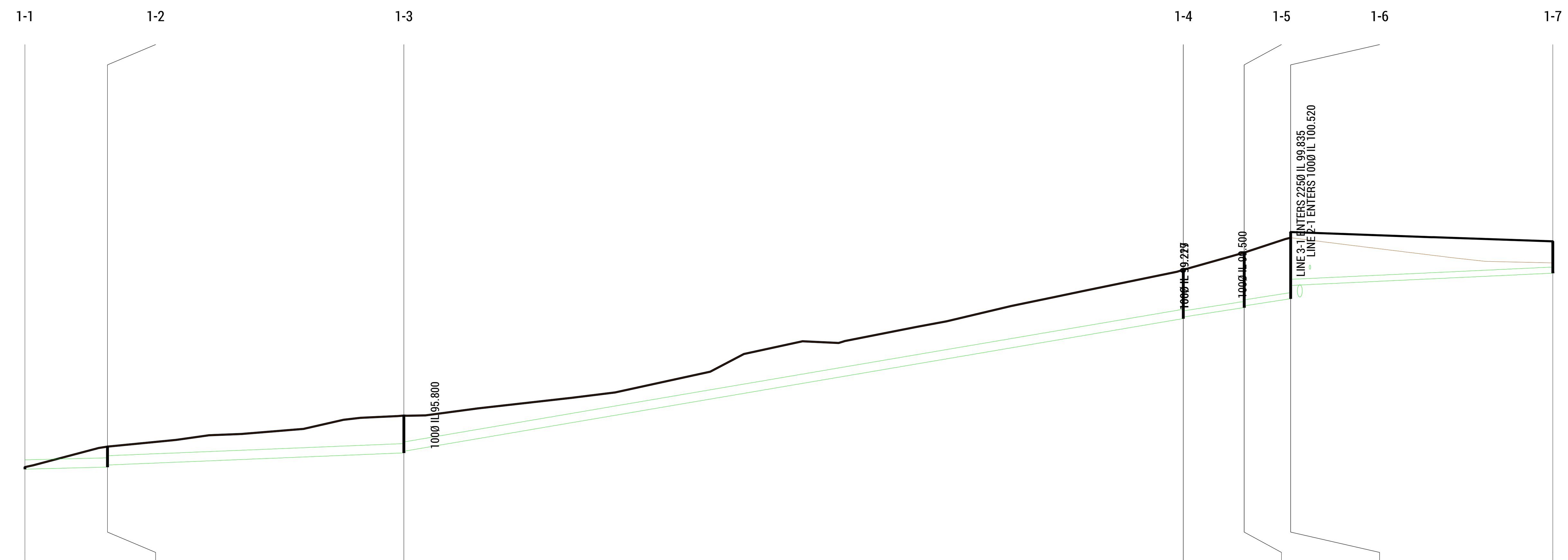
CLIENT / ARCHITECT:
CUNIC HOMES
CUNIC homes Built for you

PROJECT DETAILS:
168a ABBOTSFIELD ROAD, CLAREMONT
UNITS DEVELOPMENT

DESIGN BY:	DESIGN CHECK:	DRAWN BY:	DRAFT CHECK:	CERTIFIER:
AJL	JTA	OWM	JTA	

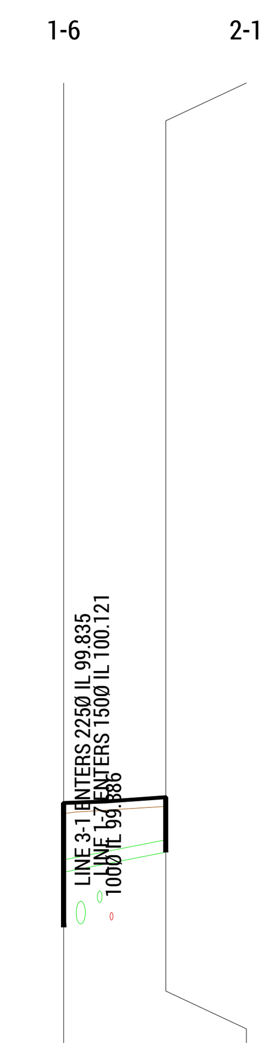
DRAWING TITLE:
SEWER LONGITUDINAL SECTIONS

SCALE:	PROJECT No:	DRAWING No:	REVISION:
VARIES	241043	C411	B



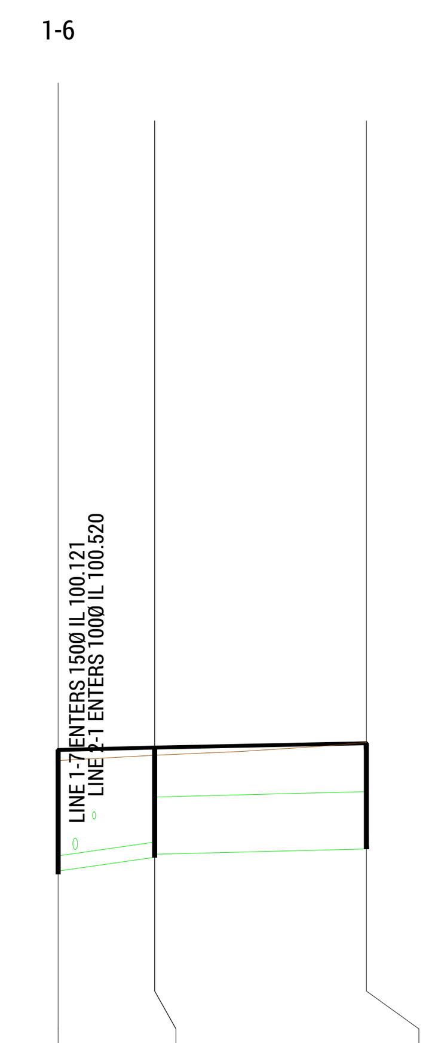
	1-1		1-2		1-3		1-4		1-5		1-6		1-7
PIPE DETAILS		225Ø RCP SW		225Ø BLACKMAX SW		225Ø BLACKMAX SW		150Ø PVC SW		150Ø PVC SW		150Ø PVC SW	
SLOPE/GRADE		0.99%		1.65%		6.82%		6.20%		6.27%		1.88%	
DATUM RL 91.6													
DEPTH TO INVERT	0.050		0.505 / 0.455		0.918 / 0.879		1.200 / 1.162		1.354 / 1.314		1.640 / 1.314		0.782
INVERT LEVEL	95.600		95.650 / 95.700		96.000 / 96.039		99.306 / 99.344		99.576 / 99.616		99.795 / 100.121		100.424
FINISHED SURFACE	95.650		96.155		96.918		100.506		100.930		101.435		101.206
EXISTING SURFACE	95.650		96.155		96.918		100.506		100.930		101.296		100.674
CHAINAGE	0.000	5.075	5.075	18.217	23.292	47.912	71.204	3.743	74.946	2.855	77.802	16.116	93.918

LONGITUNDINAL SECTION FOR LINE 1
SCALES: HORIZONTAL 1:250 VERTICAL 1:100



	1-6		2-1
PIPE DETAILS		150Ø PVC SW	
SLOPE/GRADE		7.86%	
RL 96.5			
DEPTH TO INVERT	0.915		0.726
INVERT LEVEL	100.520		100.785
FINISHED SURFACE	101.435		101.510
EXISTING SURFACE	101.296		101.391
CHAINAGE	77.802	3.372	81.174

LONGITUDINAL SECTION FOR LINE 2
SCALES: HORIZONTAL 1:250 VERTICAL 1:100



	1-6				
PIPE DETAILS		150Ø		750Ø CORRUTANK	
SLOPE/GRADE		5.70%		1.00%	
RL 95.8					
DEPTH TO INVERT	1.600		1.450 / 1.410		1.398
INVERT LEVEL	99.835		100.017 / 100.057		100.127
FINISHED SURFACE	101.435		101.467		101.525
EXISTING SURFACE	101.296		101.367		101.535
CHAINAGE	77.802	3.193	80.995	7.000	87.995

LONGITUNDINAL SECTION FOR LINE 3
SCALES: HORIZONTAL 1:250 VERTICAL 1:100

REV:	ISSUED FOR / DESCRIPTION:	BY:	DATE:
C	REVISED DEVELOPMENT APPLICATION	SCP	29-05-25
B	REVISED DEVELOPMENT APPLICATION	OWM	18-02-25
A	REVIEW / INFORMATION	OWM	07-02-25

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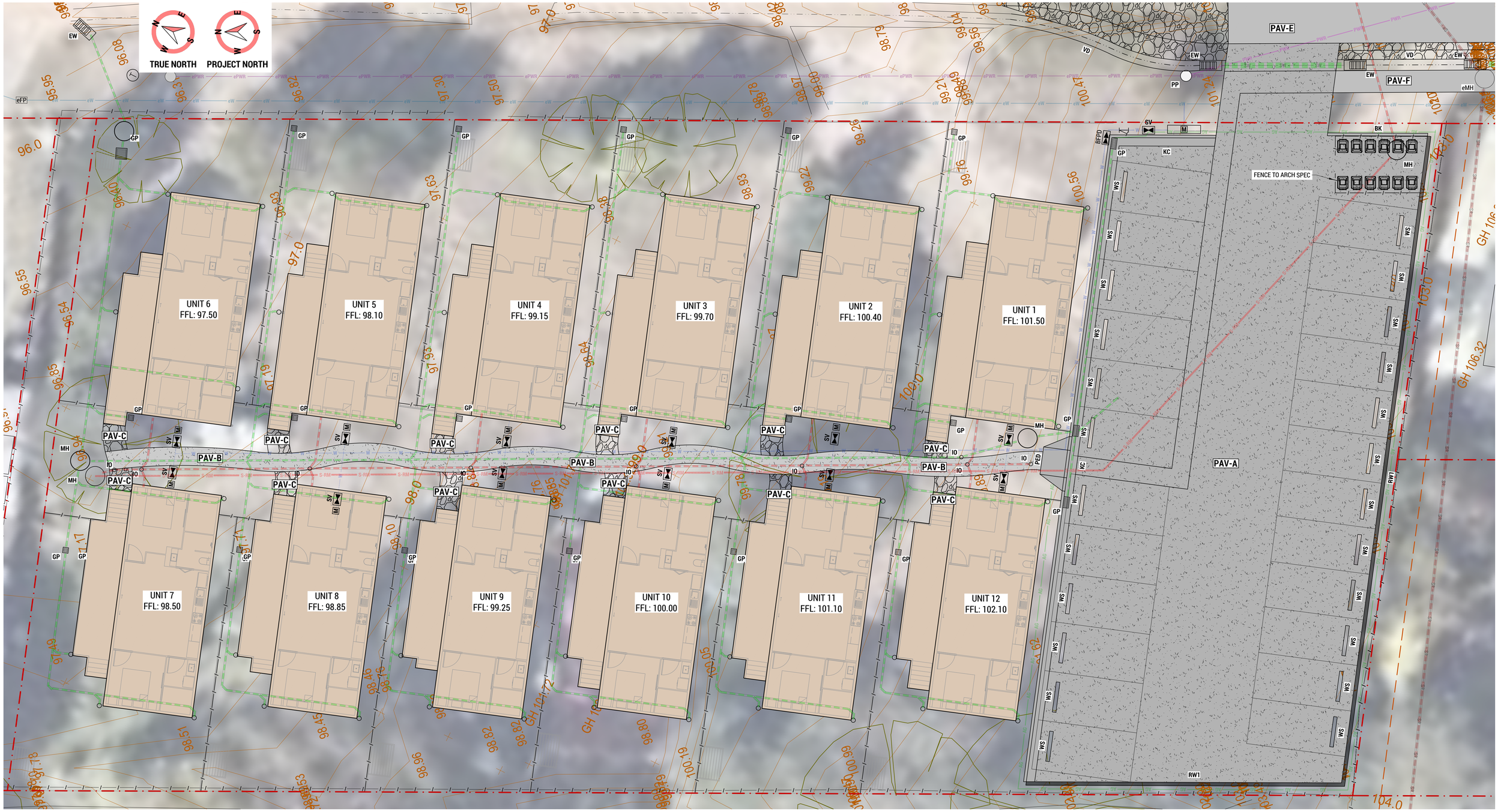
CLIENT / ARCHITECT:
CUNIC HOMES

PROJECT DETAILS:
168a ABBOTSFIELD ROAD, CLAREMONT
UNITS DEVELOPMENT

DRAWING TITLE:
STORMWATER LONGITUDINAL SECTIONS

DESIGN BY:	DESIGN CHECK:	DRAWN BY:	DRAFT CHECK:	CERTIFIER:	SCALE:	PROJECT No:	DRAWING No:	REVISION:
AJL	JTA	OWM	JTA		VARIES	241043	C421	C

Document Set ID: 3503388
Version: 2, Version Date: 26/06/2025



CIVIL WORKS NOTES:

1. THE FOLLOWING IS TO BE READ IN CONJUNCTION WITH NOTES ON DRAWING C001.

PAV-A — **PAV-A - CONCRETE HARDSTAND - TRAFFICABLE**
REFER SECTIONS AND DETAILS

PAV-B — **PAV-B - CONCRETE HARDSTAND - NON-TRAFFICABLE**
REFER SECTIONS AND DETAILS

PAV-C — **PAV-C - GRAVEL HARDSTAND - NON-TRAFFICABLE**
REFER SECTIONS AND DETAILS

PAV-D — **PAV-D - GRAVEL HARDSTAND - TRAFFICABLE**
REFER SECTIONS AND DETAILS

PAV-E — **PAV-E - TWO COAT SEAL - TRAFFICABLE**
REFER SECTIONS AND DETAILS

PAV-F — **PAV-F - TWO COAT SEAL - NON-TRAFFICABLE**
REFER SECTIONS AND DETAILS

SURF-A — **SURF-A - LANDSCAPING / SOFT AREAS**
200mm MINIMUM GOOD QUALITY TOPSOIL
(UNLESS SPECIFIED OTHERWISE BY ARCHITECT / PRINCIPAL)

BK	BARRIER KERB - REFER SECTIONS AND DETAILS	FSL	FINISHED SURFACE LEVEL
Bol	BOLLARD - REFER SECTIONS AND DETAILS	GP	GRATED / GULLY PIT - AS SCHEDULED / REFER SECTIONS AND DETAILS
CL	COVER LEVEL	HBC	HOSE BIB COCK
DN	NOMINAL PIPE DIAMETER - INTERNAL DIAMETER (U.N.O.)	HW	HEADWALL - AS SCHEDULED / REFER SECTIONS AND DETAILS
DP	DOWNPIPE - AS SCHEDULED	IL	INVERT LEVEL
e / EXTG	EXISTING ITEM / ELEMENT	IO	INSPECTION OPENING
EW	DRIVEABLE CULVERT ENDWALL	KC	KERB AND CHANNEL - REFER SECTIONS AND DETAILS
FFL	FINISHED FLOOR LEVEL	KCV	KERB AND CHANNEL - VEHICULAR - REFER SECTIONS AND DETAILS
FH	FIRE HYDRANT - AS SCHEDULED / REFER SECTIONS AND DETAILS	M	WATER METER - AS SCHEDULED / REFER SECTIONS AND DETAILS
FK	FLUSH KERB - REFER SECTIONS AND DETAILS	ME	MATCH EXISTING / MAKE GOOD TO PRINCIPAL SATISFACTION
FM	FIRE MAIN SERVICE LINE	MH	MANHOLE - AS SCHEDULED / REFER SECTIONS AND DETAILS
FP	FIRE PLUG	NSL	NATURAL SURFACE LEVEL
		PED	PEDESTRIAN ACCESS RAMP - REFER SECTIONS AND DETAILS
		RW	RETAINING WALL - AS SCHEDULED / REFER SECTIONS AND DETAILS
		S	SEWER
		SC	WORKS TO A SAWCUT EDGE - MAKE GOOD TO PRINCIPAL SATISFACTION
		SCJ	SLAB SAWCUT JOINT - AS SCHEDULED / REFER SECTIONS AND DETAILS
		SL	SURFACE LEVEL
		SV	STOP / SWITCH VALVE
		SW	STORMWATER
		VD	VEE DRAIN - AS SCHEDULED / REFER SECTIONS AND DETAILS
		W	WATER
		WS	WHEEL STOP - IN ACCORDANCE WITH AS2890.1 - REFER SECTIONS AND DETAILS

GLENORCHY CITY COUNCIL
PLANNING SERVICES
APPLICATION No. : PLN-24-270
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F	REVISED DEVELOPMENT APPLICATION	SCP	29-05-25
E	REVISED DEVELOPMENT APPLICATION	OWM	18-02-25
D	REVIEW / INFORMATION	OWM	07-02-25
C	REVISED DEVELOPMENT APPLICATION	OWM	07-11-24
B	DEVELOPMENT APPLICATION	OWM	27-09-24
A	REVIEW / INFORMATION	OWM	26-09-24

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CLIENT / ARCHITECT:
CUNIC HOMES
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PROJECT DETAILS:
168a ABBOTSFIELD ROAD, CLAREMONT
UNITS DEVELOPMENT

DRAWING TITLE:
CIVIL WORKS PLAN

DESIGN BY:	DESIGN CHECK:	DRAWN BY:	DRAFT CHECK:	CERTIFIER:
AJL	JTA	OWM	JTA	

SCALE:	PROJECT No:	DRAWING No:	REVISION:
1:100 @ A1 (1:200 @ A3)	241043	C501	F

TRUE NORTH PROJECT NORTH

FORM NEW ROAD AND VEE DRAIN INTO EXISTING

UNIT 1
FFL: 101.50

FENCE TO ARCH SPEC

CIVIL WORKS NOTES:

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REFER SECTIONS AND DETAILS

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REFER SECTIONS AND DETAILS

PAV-C — **PAV-C - GRAVEL HARDSTAND - NON-TRAFFICABLE**
REFER SECTIONS AND DETAILS

PAV-D — **PAV-D - GRAVEL HARDSTAND - TRAFFICABLE**
REFER SECTIONS AND DETAILS

PAV-E — **PAV-E - TWO COAT SEAL - TRAFFICABLE**
REFER SECTIONS AND DETAILS

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REFER SECTIONS AND DETAILS

SURF-A — **SURF-A - LANDSCAPING / SOFT AREAS**
200mm MINIMUM GOOD QUALITY TOPSOIL
(UNLESS SPECIFIED OTHERWISE BY ARCHITECT / PRINCIPAL)

BK	BARRIER KERB - REFER SECTIONS AND DETAILS	FSL	FINISHED SURFACE LEVEL	NSL	NATURAL SURFACE LEVEL		
Bol	BOLLARD - REFER SECTIONS AND DETAILS	GP	GRATED / GULLY PIT - AS SCHEDULED / REFER SECTIONS AND DETAILS	**PED**	PEDESTRIAN ACCESS RAMP - REFER SECTIONS AND DETAILS		
CL	COVER LEVEL	**HBC**	HOSE BIB COCK	**RW**	RETAINING WALL - AS SCHEDULED / REFER SECTIONS AND DETAILS		
DN	NOMINAL PIPE DIAMETER - INTERNAL DIAMETER (U.N.O.)	**HW**	HEADWALL - AS SCHEDULED / REFER SECTIONS AND DETAILS	**S**	SEWER		
DP	DOWNPIPE - AS SCHEDULED	IL	INVERT LEVEL	**SC**	WORKS TO A SAWCUT EDGE - MAKE GOOD TO PRINCIPAL SATISFACTION		
e / EXTG	EXISTING ITEM / ELEMENT	**IO**	INSPECTION OPENING	**SCJ**	SLAB SAWCUT JOINT - AS SCHEDULED / REFER SECTIONS AND DETAILS		
EW	DRIVEABLE CULVERT ENDWALL	**KC**	KERB AND CHANNEL - REFER SECTIONS AND DETAILS	SL	SURFACE LEVEL		
FFL	FINISHED FLOOR LEVEL	**KCV**	KERB AND CHANNEL - VEHICULAR - REFER SECTIONS AND DETAILS	**SV**	STOP / SWITCH VALVE		
FH	FIRE HYDRANT - AS SCHEDULED / REFER SECTIONS AND DETAILS	**M**	WATER METER - AS SCHEDULED / REFER SECTIONS AND DETAILS	**SW**	STORMWATER		
FK	FLUSH KERB - REFER SECTIONS AND DETAILS	**ME**	MATCH EXISTING / MAKE GOOD TO PRINCIPAL SATISFACTION	**VD**	VEE DRAIN - AS SCHEDULED / REFER SECTIONS AND DETAILS		
FM	FIRE MAIN SERVICE LINE	**MH**	MANHOLE - AS SCHEDULED / REFER SECTIONS AND DETAILS	**W**	WATER		
FP	FIRE PLUG			**WS**	WHEEL STOP - IN ACCORDANCE WITH AS2890.1 - REFER SECTIONS AND DETAILS		

GLENORCHY CITY COUNCIL PLANNING SERVICES
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CLIENT / ARCHITECT:
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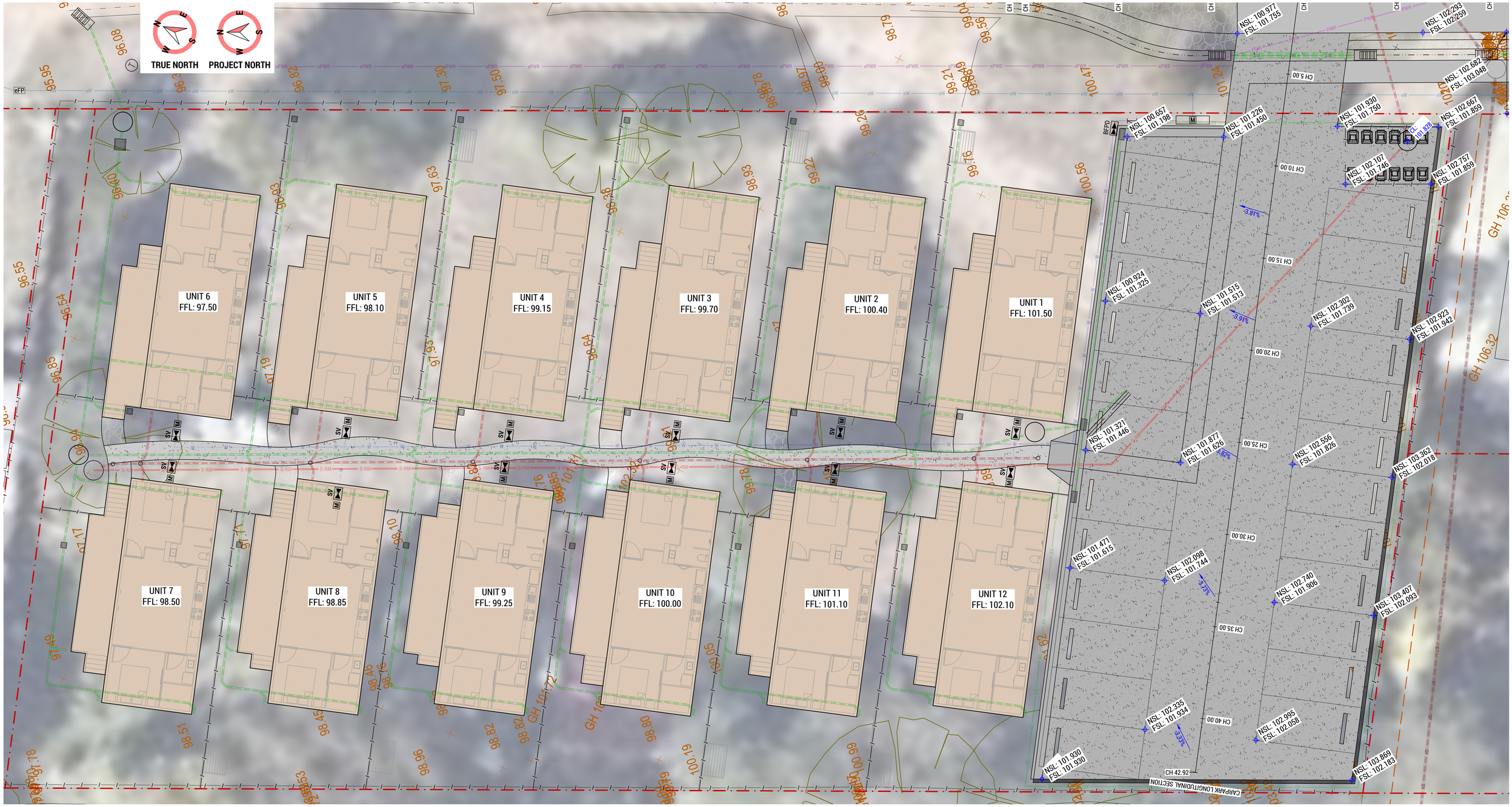
PROJECT DETAILS:
168a ABBOTSFIELD ROAD, CLAREMONT
UNITS DEVELOPMENT

DESIGN BY:	DESIGN CHECK:	DRAWN BY:	DRAFT CHECK:	CERTIFIER:
AJL	JTA	OWM	JTA	

DRAWING TITLE:
CIVIL WORKS PLAN

SCALE:	PROJECT No:	DRAWING No:	REVISION:
1:100 @ A1 (1:200 @ A3)	241043	C502	E



CIVIL LEVELS NOTES:

1. THE FOLLOWING IS TO BE READ IN CONJUNCTION WITH NOTES ON DRAWING C001.

CIVIL LEVELS LEGEND:

CL	COVER LEVEL
e / EXTG	EXISTING ITEM / ELEMENT
FFL	FINISHED FLOOR LEVEL
FSL	FINISHED SURFACE LEVEL
IL	INVERT LEVEL
NSL	NATURAL SURFACE LEVEL
SL	SURFACE LEVEL
TOK	TOP OF KERB
TOW	TOP OF WALL
eSL 100.0	EXISTING SURFACE LEVEL MARKER AND HEIGHT / RL
SL 100.0	PROPOSED SURFACE LEVEL MARKER AND HEIGHT / RL

GLENORCHY CITY COUNCIL PLANNING SERVICES

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CLIENT / ARCHITECT:
CUNIC HOMES

PROJECT DETAILS:
168a ABBOTSFIELD ROAD, CLAREMONT
UNITS DEVELOPMENT

DRAWING TITLE:
CIVIL LEVELS PLAN

DESIGN BY:	DESIGN CHECK:	DRAWN BY:	DRAFT CHECK:	CERTIFIER:
AJL	JTA	OWM	JTA	

SCALE:	PROJECT No:	DRAWING No:	REVISION:
1:100 @ A1 (1:200 @ A3)	241043	C511	C

CIVIL LEVELS NOTES:
1. THE FOLLOWING IS TO BE READ IN CONJUNCTION WITH NOTES ON DRAWING C001.

CIVIL LEVELS LEGEND:

CL	COVER LEVEL
e / EXTG	EXISTING ITEM / ELEMENT
FFL	FINISHED FLOOR LEVEL
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eSL 100.0	EXISTING SURFACE LEVEL MARKER AND HEIGHT / RL
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GLENORCHY CITY COUNCIL
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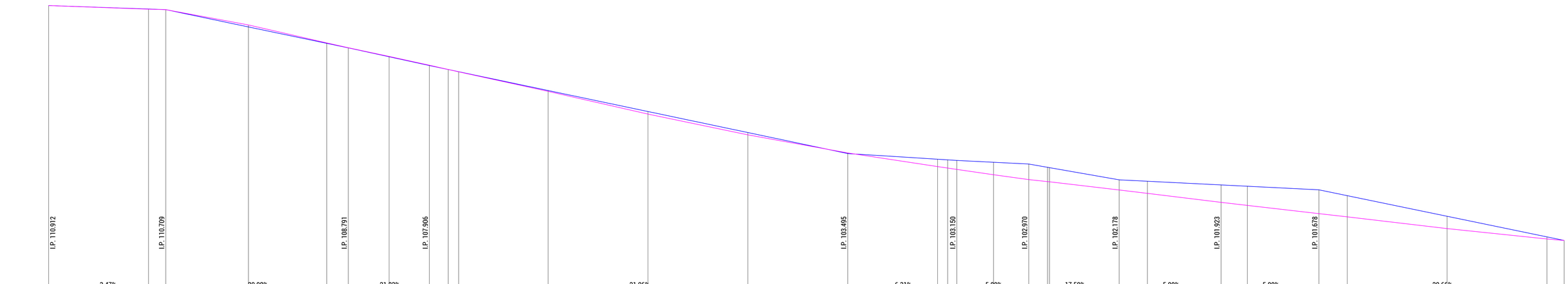
UNITS DEVELOPMENT

DESIGN BY:	DESIGN CHECK:	DRAWN BY:	DRAFT CHECK:	CERTIFIER:
AJL	JTA	OWM	JTA	

DRAWING TITLE:
CIVIL LEVELS PLAN

SCALE:	PROJECT No:	DRAWING No:	REVISION:
1:100 @ A1 (1:200 @ A3)	241043	C512	C

Document Set ID: 3503383
Version: 2, Version Date: 26/06/2025



R.L. 95.700

Cut Fill	+0.000	+0.008	+0.000	-0.098	-0.021	-0.008	-0.017	-0.015	-0.001	+0.007
Design Levels	110.912	110.738	110.709	109.840	109.018	108.791	108.346	107.906	107.707	107.597
Existing Levels	110.912	110.730	110.709	109.938	109.039	108.799	108.363	107.921	107.708	107.590
Chainage	0.000	5.000	5.860	10.000	13.920	15.000	17.042	19.059	20.000	20.523

Cut Fill	+0.043	+0.127	+0.115	-0.032	+0.368	+0.411	+0.449	+0.625	+0.780	+0.716
Design Levels	106.654	105.601	104.548	103.495	103.211	103.179	103.150	103.058	102.970	102.806
Existing Levels	106.611	105.474	104.433	103.527	102.843	102.768	102.701	102.433	102.190	102.090
Chainage	25.000	30.000	35.000	40.000	44.496	45.000	45.458	47.296	49.059	50.000

Cut Fill	+0.710	+0.508	+0.607	+0.873	+0.959	+1.194	+1.062	+0.618	+0.104	+0.000
Design Levels	102.789	102.178	102.107	101.923	101.857	101.678	101.385	100.352	99.319	99.141
Existing Levels	102.079	101.670	101.500	101.050	100.898	100.484	100.323	99.734	99.215	99.141
Chainage	50.096	53.588	55.000	58.686	60.000	63.582	65.000	70.000	75.000	75.860

LONGITUDINAL SECTION
Scales: H 1:100 V 1:100

ROAD 1

R.L. 96.800

Cut Fill	+0.972	+0.541	+0.336	+0.018	-0.006	-0.083	-0.265	-0.373	-0.492	-0.551	-0.637	-0.723
Design Levels	101.848	101.911	101.782	101.600	101.619	101.669	101.720	101.770	101.847	101.923	101.999	102.044
Existing Levels	100.876	101.370	101.446	101.582	101.625	101.752	101.985	102.143	102.339	102.474	102.636	102.767
Chainage	0.000	2.776	5.000	8.128	10.000	15.000	20.000	25.000	30.000	35.000	40.000	42.921

LONGITUDINAL SECTION
Scales: H 1:100 V 1:100

CARPARK LONGITUDINAL SECTION

R.L. 104.000

Cut Fill	+0.000	+0.274	+0.226	+0.008	-0.017
Design Levels	108.080	108.629	108.593	108.428	108.346
Existing Levels	108.080	108.355	108.367	108.420	108.363
Chainage	0.000	2.931	4.151	5.680	8.430

LONGITUDINAL SECTION
Scales: H 1:100 V 1:100

DRIVE 1

R.L. 98.400

Cut Fill	+0.000	-0.079	+0.042	+0.146	+0.338	+0.625
Design Levels	103.443	103.107	103.071	103.094	103.141	103.058
Existing Levels	103.443	103.186	103.029	102.948	102.803	102.433
Chainage	0.000	1.676	2.875	3.377	4.376	7.126

LONGITUDINAL SECTION
Scales: H 1:100 V 1:100

DRIVE 2

GLENORCHY CITY COUNCIL
PLANNING SERVICES
APPLICATION No. : PLN-24-270
DATE RECEIVED: 12/06/2025

REV:	ISSUED FOR / DESCRIPTION:	BY:	DATE:
C	REVISED DEVELOPMENT APPLICATION	SCP	29-05-25
B	REVISED DEVELOPMENT APPLICATION	OWM	18-02-25
A	REVIEW / INFORMATION	OWM	07-02-25

COLLECTIVE CONSULTING DISCLAIMER:

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collectiveconsulting.com.au

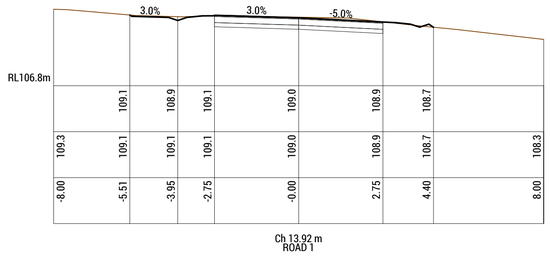
CLIENT / ARCHITECT:
CUNIC HOMES
CUNIC homes Built for you

PROJECT DETAILS:
168a ABBOTSFIELD ROAD, CLAREMONT
UNITS DEVELOPMENT

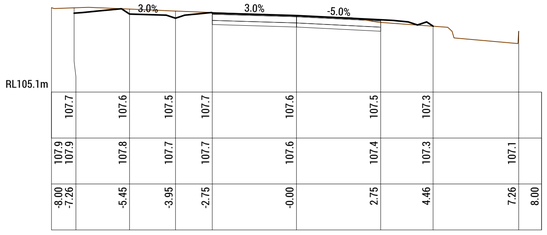
DRAWING TITLE:
LONGITUDINAL & CROSS SECTIONS - SHEET 1

DESIGN BY:	DESIGN CHECK:	DRAWN BY:	DRAFT CHECK:	CERTIFIER:
AJL	JTA	OWM	JTA	

SCALE:	PROJECT No:	DRAWING No:	REVISION:
VARIES	241043	C521	C



Ch 13.92 m
ROAD 1



Ch 20.52 m
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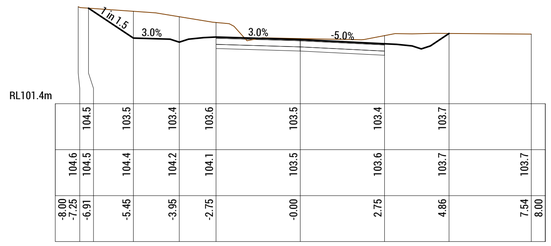
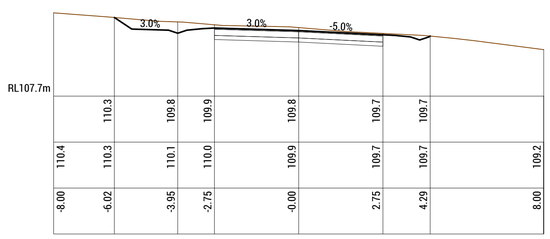


Ch 40.00 m
ROAD 1



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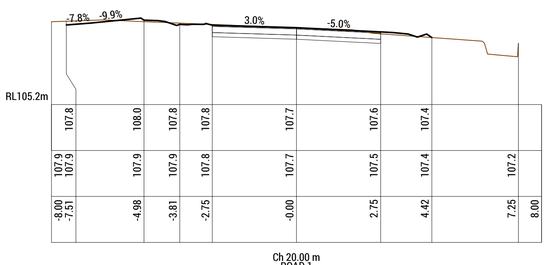

Ch 20.00 m
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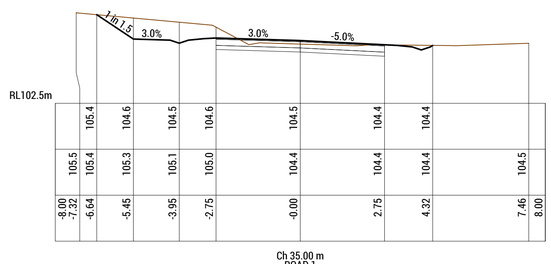
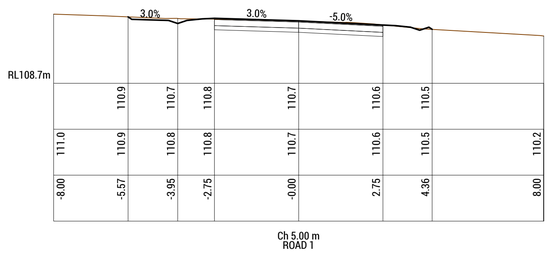


Ch 35.00 m
ROAD 1



Ch 5.00 m
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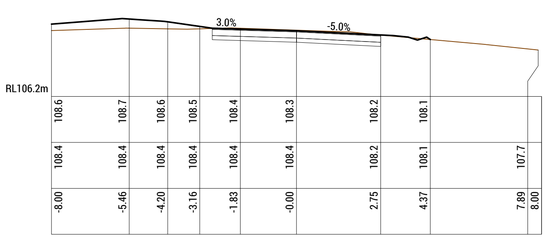

Ch 17.04 m
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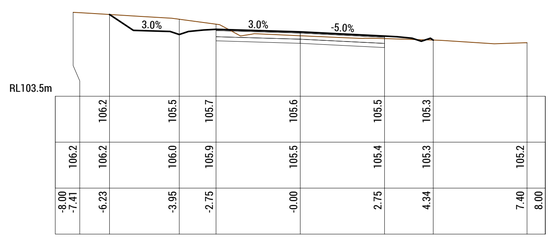
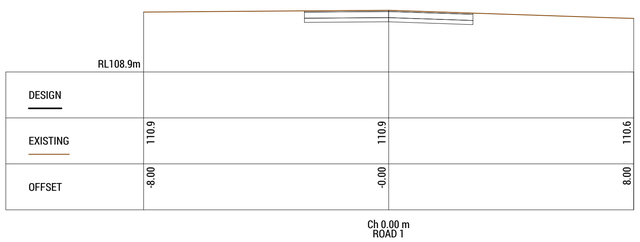
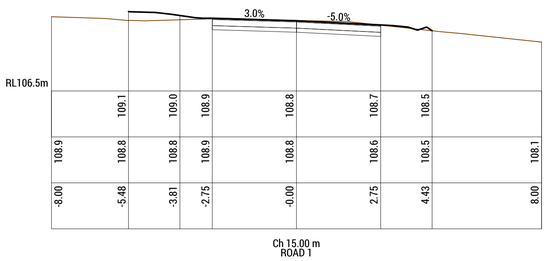


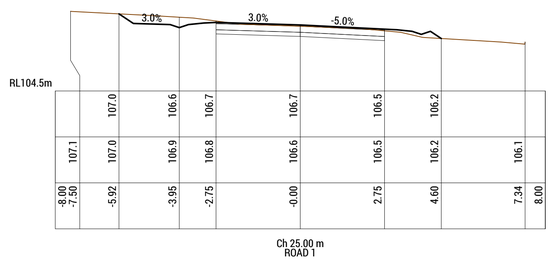
Ch 30.00 m
ROAD 1



Ch 0.00 m
ROAD 1



Ch 15.00 m
ROAD 1



Ch 25.00 m
ROAD 1

GLENORCHY CITY COUNCIL PLANNING SERVICES

APPLICATION No. : PLN-24-270

DATE RECEIVED: 12/06/2025

REV:	ISSUED FOR / DESCRIPTION:	BY:	DATE:
C	**REVISED DEVELOPMENT APPLICATION**	**SCP**	**29-05-25**
B	**REVISED DEVELOPMENT APPLICATION**	**OWM**	**18-02-25**
A	**REVIEW / INFORMATION**	**OWM**	**07-02-25**

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CLIENT / ARCHITECT:
CUNIC HOMES

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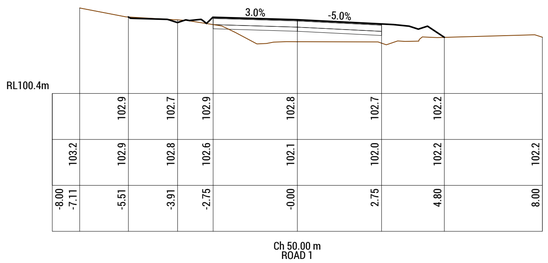
PROJECT DETAILS:
168a ABBOTSFIELD ROAD, CLAREMONT
UNITS DEVELOPMENT

DESIGN BY:	DESIGN CHECK:	DRAWN BY:	DRAFT CHECK:	CERTIFIER:
AJL	JTA	OWM	JTA	

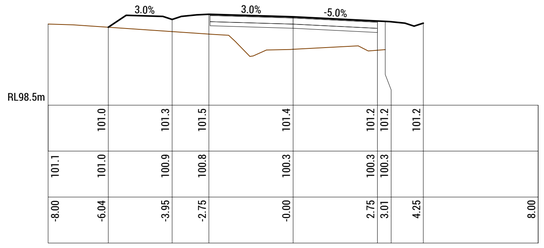
DRAWING TITLE:
LONGITUDINAL & CROSS SECTIONS - SHEET 2

SCALE:	PROJECT No:	DRAWING No:	REVISION:
VARIES	241043	C522	C

Document Set ID: 3503383
Version: 2, Version Date: 26/06/2025



Ch 50.00 m
ROAD 1



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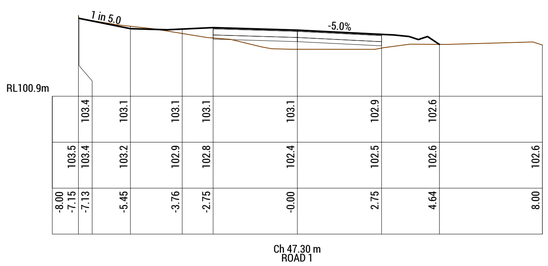

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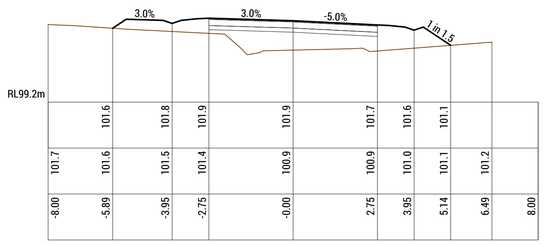

Ch 60.00 m
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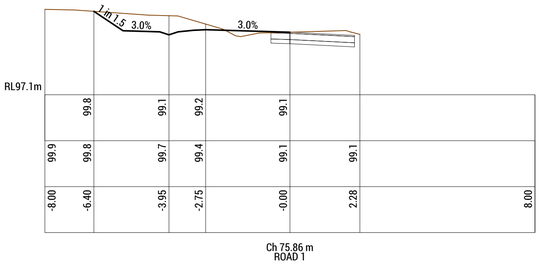
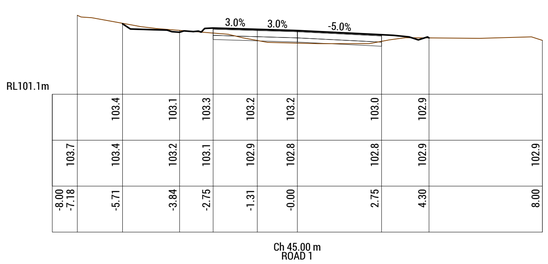


Ch 75.86 m
ROAD 1



Ch 45.00 m
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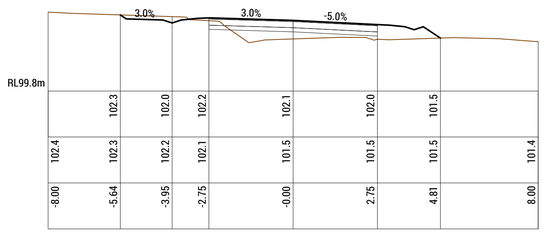

Ch 55.00 m
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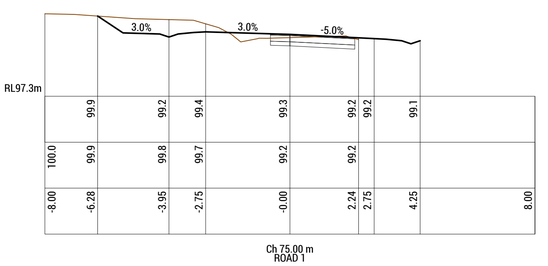

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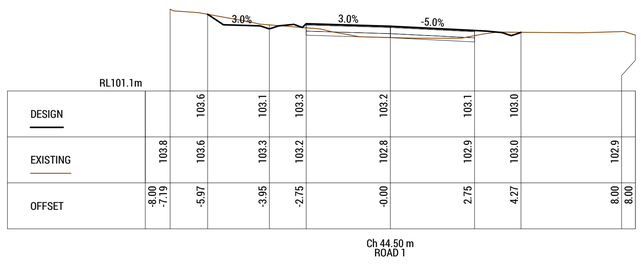
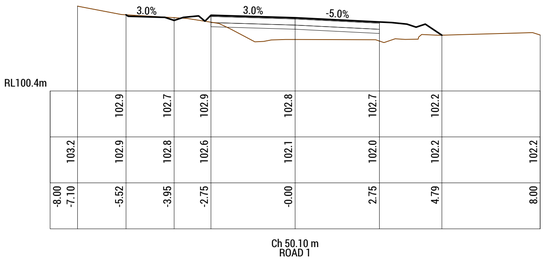
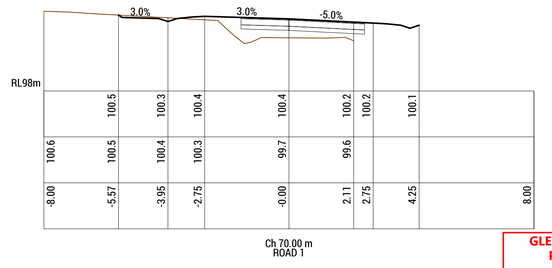


Ch 44.50 m
ROAD 1



Ch 50.10 m
ROAD 1



Ch 70.00 m
ROAD 1

GLENORCHY CITY COUNCIL
PLANNING SERVICES

APPLICATION No. : PLN-24-270

DATE RECEIVED: 12/06/2025

REV.	ISSUED FOR / DESCRIPTION:	BY:	DATE:
C	REVISED DEVELOPMENT APPLICATION	SCP	29-05-25
B	REVISED DEVELOPMENT APPLICATION	OWM	18-02-25
A	REVIEW / INFORMATION	OWM	07-02-25

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Launceston TAS 7250
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collectiveconsulting.com.au

CLIENT / ARCHITECT:
CUNIC HOMES

CUNIC homes Built for you

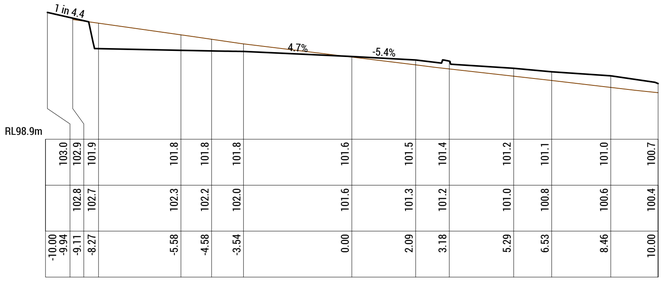
PROJECT DETAILS:
168a ABBOTSFIELD ROAD, CLAREMONT
UNITS DEVELOPMENT

DESIGN BY:	DESIGN CHECK:	DRAWN BY:	DRAFT CHECK:	CERTIFIER:
AJL	JTA	OWM	JTA	

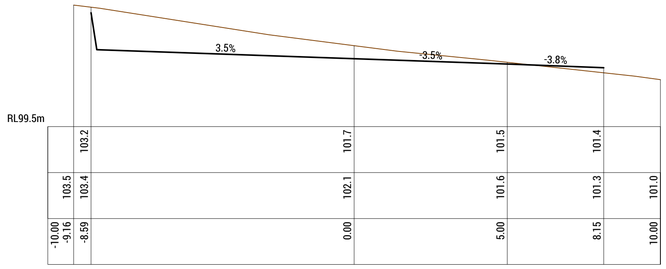
DRAWING TITLE:
LONGITUDINAL & CROSS SECTIONS - SHEET 3

SCALE:	PROJECT No:	DRAWING No:	REVISION:
VARIES	241043	C523	C

Document Set ID: 3503383
Version: 2, Version Date: 26/06/2025



Ch 8.13 m
CARPARK LONGITUDINAL SECTION



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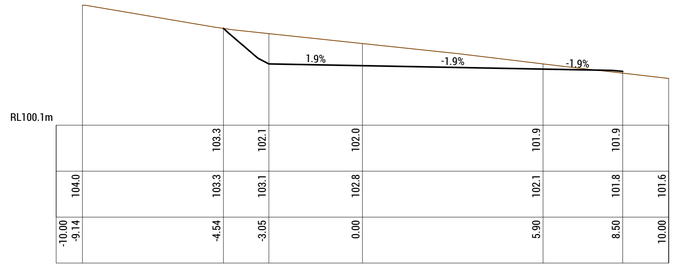
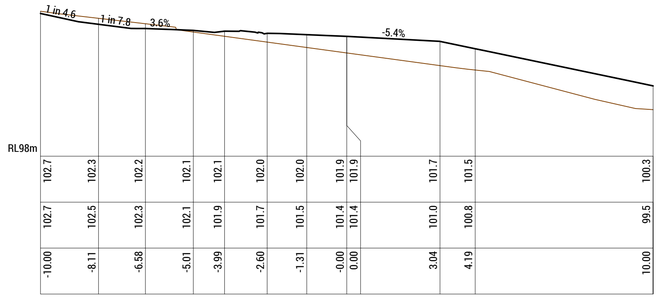
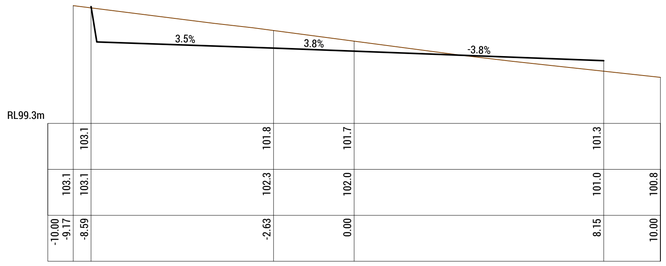


Ch 42.92 m
CARPARK LONGITUDINAL SECTION



Ch 2.78 m
CARPARK LONGITUDINAL SECTION



Ch 20.00 m
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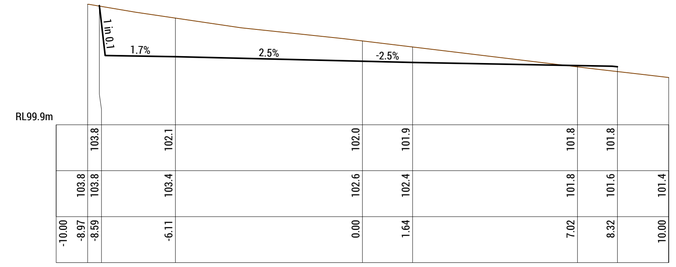

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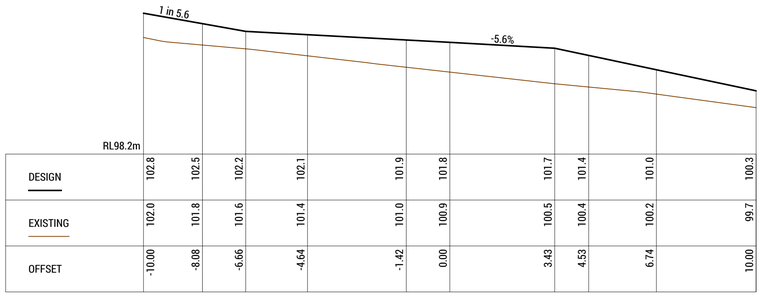
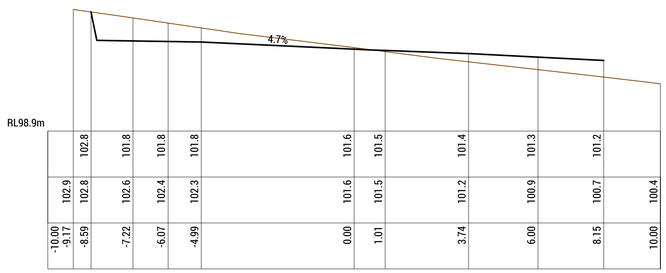
CARPARK LONGITUDINAL SECTION



Ch 0.00 m
CARPARK LONGITUDINAL SECTION



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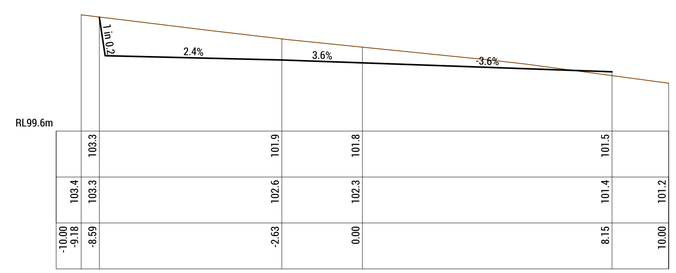

Ch 30.00 m
CARPARK LONGITUDINAL SECTION

GLENORCHY CITY COUNCIL PLANNING SERVICES

APPLICATION No. : PLN-24-270

DATE RECEIVED: 12/06/2025

REV:	ISSUED FOR / DESCRIPTION:	BY:	DATE:
C	**REVISED DEVELOPMENT APPLICATION**	SCP	29-05-25
B	**REVISED DEVELOPMENT APPLICATION**	OWM	18-02-25
A	**REVIEW / INFORMATION**	OWM	07-02-25

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Launceston TAS 7250
P (03) 6334 0834
collectiveconsulting.com.au

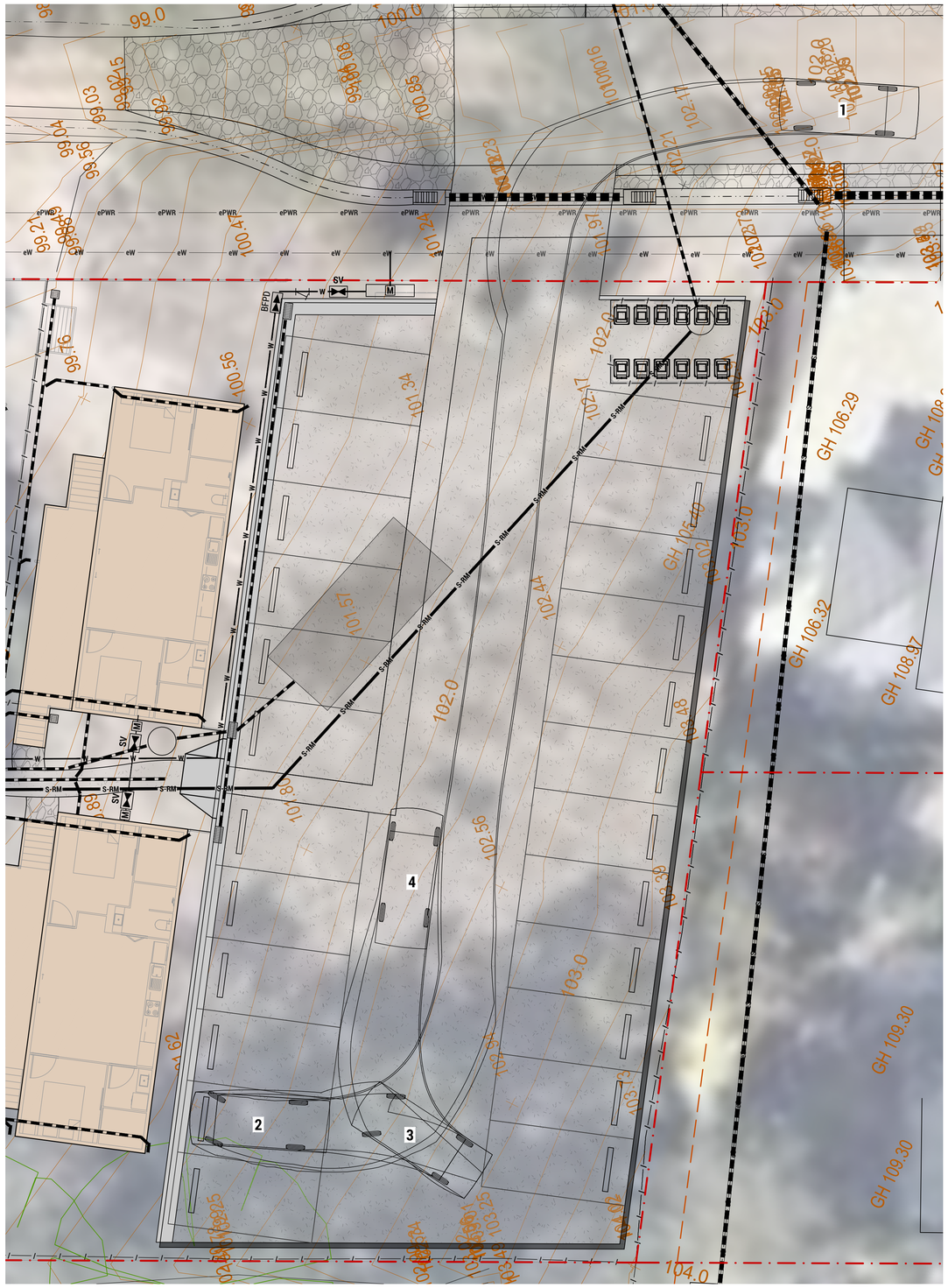
CLIENT / ARCHITECT:
CUNIC HOMES
CUNIC homes Built for you

PROJECT DETAILS:
168a ABBOTSFIELD ROAD, CLAREMONT
UNITS DEVELOPMENT

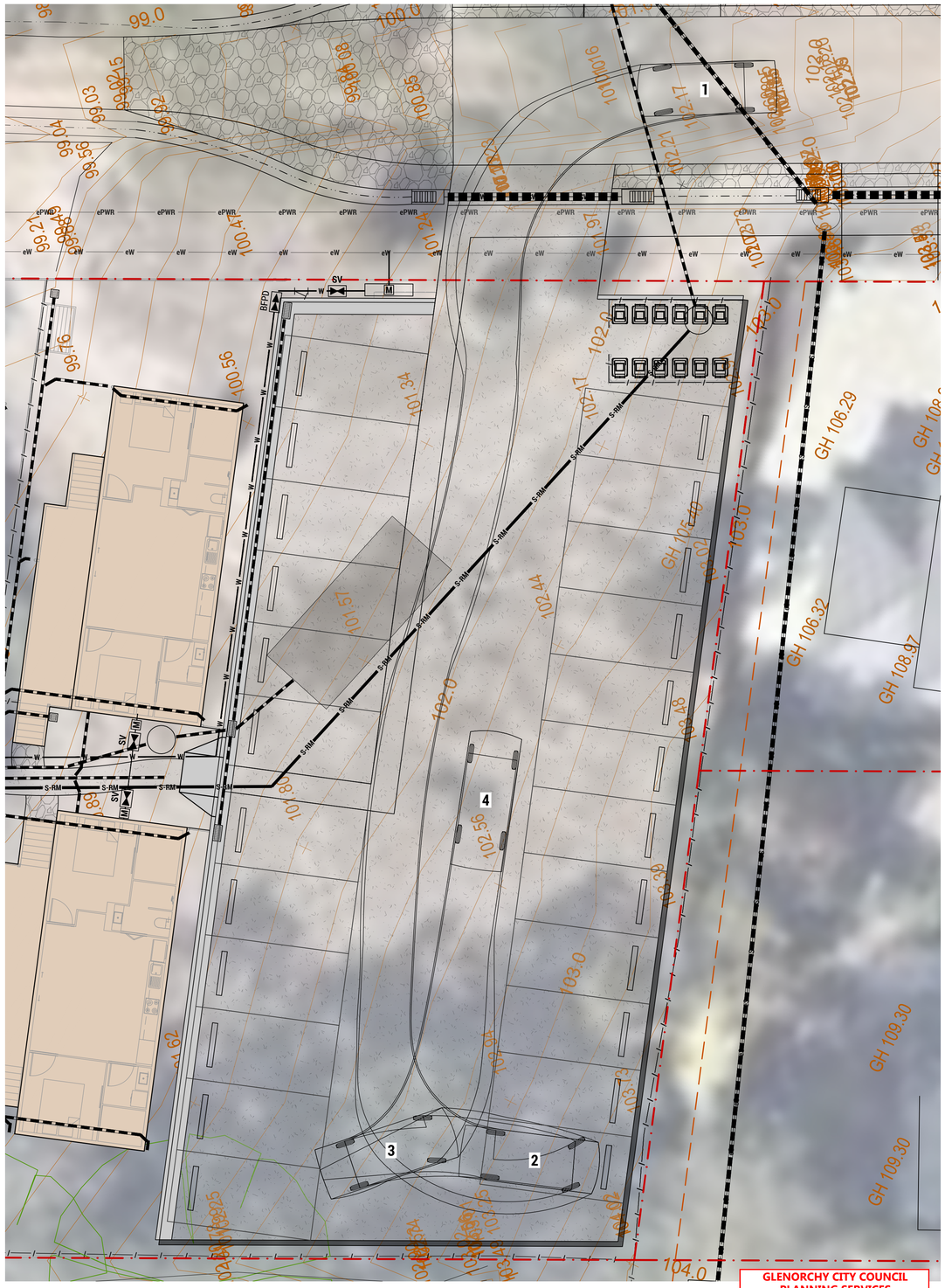
DRAWING TITLE:
LONGITUDINAL & CROSS SECTIONS - SHEET 4

DESIGN BY:	DESIGN CHECK:	DRAWN BY:	DRAFT CHECK:	CERTIFIER:
AJL	JTA	OWM	JTA	

SCALE:	PROJECT No:	DRAWING No:	REVISION:
VARIES	241043	C524	C



TURNING PATHS - B99 VEHICLE
SCALE 1:100



TURNING PATHS - B99 VEHICLE
SCALE 1:100

GLENORCHY CITY COUNCIL
PLANNING SERVICES
APPLICATION No. : PLN-24-270
DATE RECEIVED: 12/06/2025

REV:	ISSUED FOR / DESCRIPTION:	BY:	DATE:
E	REVISED DEVELOPMENT APPLICATION	OWM	18-02-25
D	REVIEW / INFORMATION	OWM	07-02-25
C	REVISED DEVELOPMENT APPLICATION	OWM	07-11-24
B	DEVELOPMENT APPLICATION	OWM	27-09-24
A	REVIEW / INFORMATION	OWM	26-09-24

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Launceston TAS 7250
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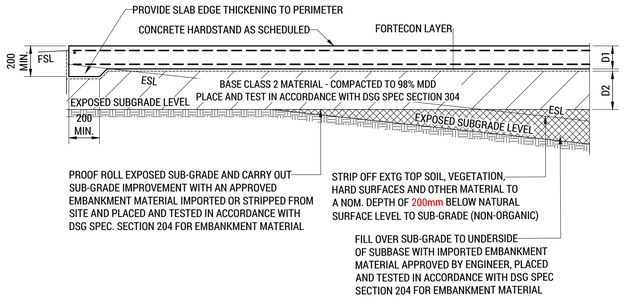
CLIENT / ARCHITECT:
CUNIC HOMES
CUNIC homes Built for you

PROJECT DETAILS:
168a ABBOTSFIELD ROAD, CLAREMONT
UNITS DEVELOPMENT

DESIGN BY:	DESIGN CHECK:	DRAWN BY:	DRAFT CHECK:	CERTIFIER:
AJL	JTA	OWM	JTA	

DRAWING TITLE:
VEHICLE TURNING MOVEMENTS PLAN - SHEET 1

SCALE:	PROJECT No:	DRAWING No:	REVISION:
1:100 @ A1 (1:200 @ A3)	241043	C701	E

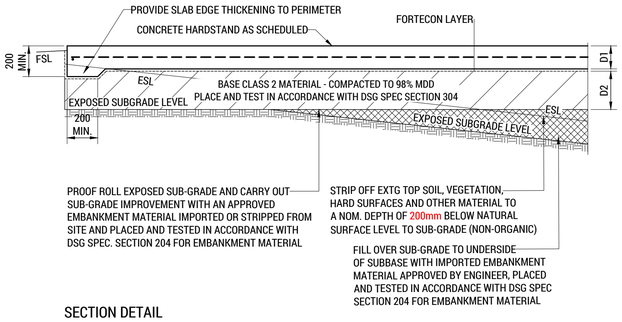


SECTION DETAIL

PAVEMENT - 'PAV-A' - CONCRETE HARDSTAND - TRAFFICABLE (TYP.)

SCALE 1:20

NOTE: REFER CIVIL WORKS PAVEMENT / SURFACE SCHEDULE FOR DEPTHS
REFER LGAT STANDARD DRAWING TSD-R09-v3 FOR ADDITIONAL DRIVEWAY DETAILS

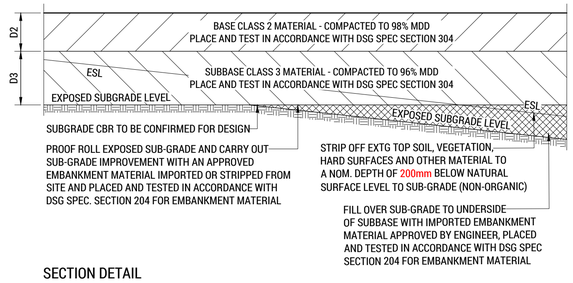


SECTION DETAIL

PAVEMENT - 'PAV-B' - CONCRETE HARDSTAND - NON-TRAFFICABLE (TYP.)

SCALE 1:20

NOTE: REFER CIVIL WORKS PAVEMENT / SURFACE SCHEDULE FOR DEPTHS
REFER LGAT STANDARD DRAWING TSD-R09-v3 FOR ADDITIONAL DRIVEWAY DETAILS

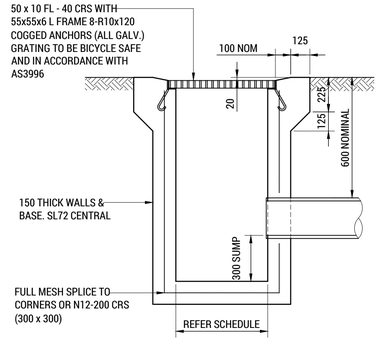


SECTION DETAIL

PAVEMENT - 'PAV-C' - COMPACTED GRAVEL - NON-TRAFFICABLE (TYP.)

SCALE 1:20

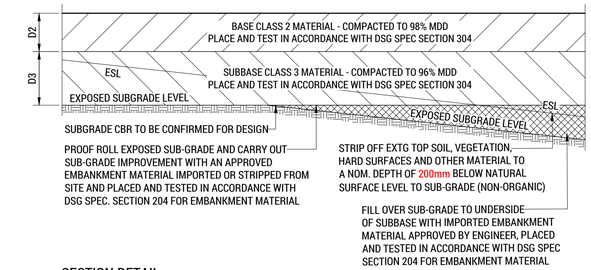
NOTE: REFER CIVIL WORKS PAVEMENT / SURFACE SCHEDULE FOR DEPTHS



GRATED PIT - TRAFFICABLE

SCALE 1:20

REFER IPWEA STANDARD DRAWINGS FOR ALTERNATE PIT CONSTRUCTION DETAILS.
APPROVED PRECAST UNIT MAYBE SUBSTITUTED.

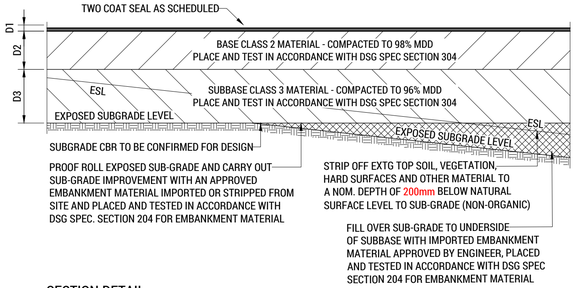


SECTION DETAIL

PAVEMENT - 'PAV-D' - COMPACTED GRAVEL - TRAFFICABLE (TYP.)

SCALE 1:20

NOTE: REFER CIVIL WORKS PAVEMENT / SURFACE SCHEDULE FOR DEPTHS

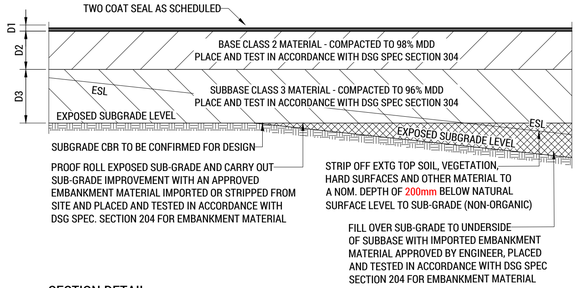


SECTION DETAIL

PAVEMENT - 'PAV-E' - TWO COAT SEAL - TRAFFICABLE (TYP.)

SCALE 1:20

NOTE: REFER CIVIL WORKS PAVEMENT / SURFACE SCHEDULE FOR DEPTHS

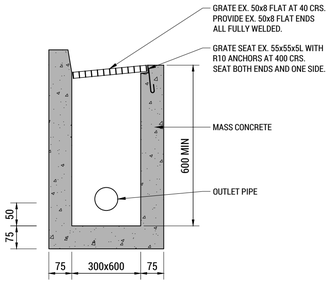


SECTION DETAIL

PAVEMENT - 'PAV-F' - TWO COAT SEAL - TRAFFICABLE (TYP.)

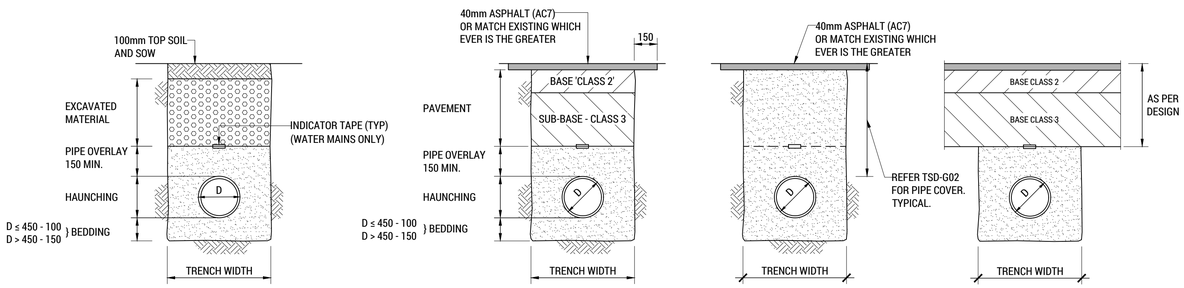
SCALE 1:20

NOTE: REFER CIVIL WORKS PAVEMENT / SURFACE SCHEDULE FOR DEPTHS



GRATED GULLY PIT - TRAFFICABLE - GP1

SCALE 1:10



TRENCHES - NON-TRAFFICABLE

SCALE 1:20

TRENCHES - EXISTING ROADS

SCALE 1:20

TRENCHES - NEW ROADS

SCALE 1:20

TRENCH WIDTH

PIPE TYPE	NOM. DIA (D)	TRENCH WIDTH
CONCRETE	≤ 1500	D + 300
	> 1500	DESIGN REQ.
OTHER PIPES	100	300
	150	450
	225-300	600
	450	750
	450-1500	D + 600
	> 1500	DESIGN REQ.

MINIMUM TRENCH WIDTHS MAY BE VARIED ABOVE THE PIPE OVERLAY ZONE TO MEET 'WORKPLACE STANDARDS' REQUIREMENTS.
ie EXCAVATIONS OVER 1.5m MAY REQUIRE RISK ASSESSMENT.

COMPACTION OF BEDDING, HAUNCHING & OVERLAY
REFER TO AS 1289-5.5
CONCRETE PIPES = MIN. DENSITY INDEX = 60% (85% STD. COMPACTION)
uPVC PIPES = DENSITY INDEX = 65% (90% STD. COMPACTION)
DICL PIPES = DENSITY INDEX = 65% (90% STD. COMPACTION)

BEDDING, HAUNCHING AND OVERLAY MATERIAL
BEDDING , HAUNCHING AND PIPE OVERLAY MATERIAL SHALL CONTAIN NO DELETERIOUS MATERIAL OR CLAY LUMPS AND SHALL COMPLY WITH THE FOLLOWING GRADINGS:

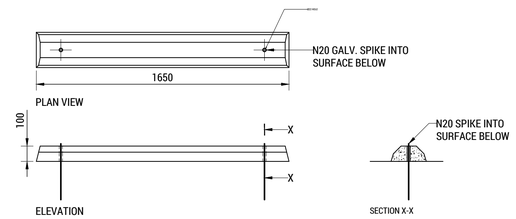
FOR uPVC AND DUCTILE IRON PIPES
SAND OR CRUSHED ROCK (STONE DUST)

SIEVE APERTURE (mm) TO AS1152	% PASSING (BY MASS)
6.7	100
2.36	70-100
0.6	20-90
0.3	8-50
0.15	0-20
0.075	0-10

FOR CONCRETE PIPES
CRUSHED ROCK

SIEVE APERTER (mm) TO AS1152	% PASSING (BY MASS)
19	100
2.36	50-100
0.6	20-90
0.3	10-60
0.15	0-25
0.075	0-10

ALL MATERIAL SHALL BE PLACED AND COMPACTED IN ACCORDANCE WITH AS3725 AND TO THE SATISFACTION OF THE SUPERINTENDENT.



WHEEL STOP DETAIL (TYP.)

SCALE 1:20

NOTE:
HUDSON CIVIL PRODUCT WHEEL STOP (1650 LONG x 100 HIGH)
INSTALLED TO MANUFACTURERS SPECIFICATION

GLENORCHY CITY COUNCIL
PLANNING SERVICES
APPLICATION No. : PLN-24-270
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E	REVISED DEVELOPMENT APPLICATION	OWM	18-02-25
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COLLECTIVE CONSULTING
E admin@collectiveconsulting.com.au
Level 1, 10-14 Paterson Street
Launceston TAS 7250
P (03) 6334 0834
collectiveconsulting.com.au

CLIENT / ARCHITECT:
CUNIC HOMES

PROJECT DETAILS:
168a ABBOTSFIELD ROAD, CLAREMONT
UNITS DEVELOPMENT

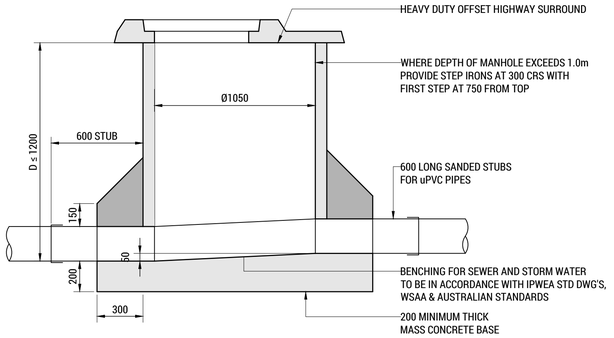
DRAWING TITLE:
SECTIONS & DETAILS - SHEET 1

DESIGN BY:	DESIGN CHECK:	DRAWN BY:	DRAFT CHECK:	CERTIFIER:
AJL	JTA	OWM	JTA	

SCALE:	PROJECT No:	DRAWING No:	REVISION:
1:20, 1:10 @ A1 1:40, 1:20 @ A3	241043	C801	E

MANHOLE LIDS TO BE 'GATIC' TYPE OR SIMILAR
- HEAVY DUTY 'CLASS D' FOR TRAFFIC AREAS
- MEDIUM DUTY 'CLASS B' FOR ALL OTHER AREAS

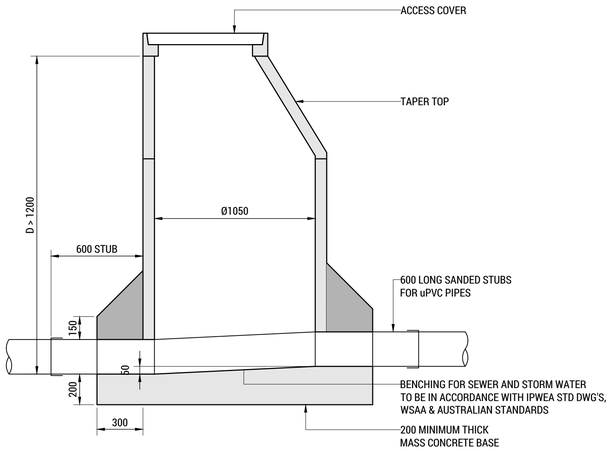
ALL STORM WATER MANHOLES TO HAVE 'SW' CAST INTO LIDS. SEWER MANHOLES TO HAVE 'S' CAST IN.



MANHOLE DETAIL - D < 1200

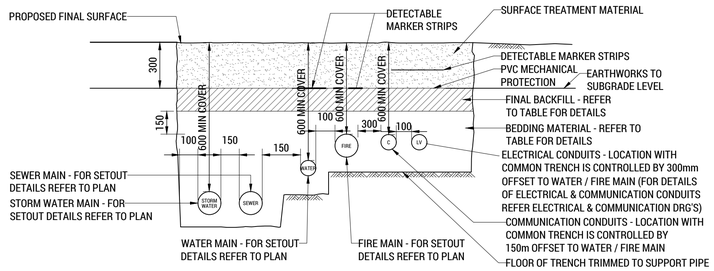
SCALE 1:20

REFER IPWEA STD DWG TSD-SW02-v3
FOR STORMWATER MANHOLE DETAILS
REFER WSAA STD DWG'S FOR SEWER MANHOLE DETAILS



MANHOLE DETAIL - D > 1200

SCALE 1:20

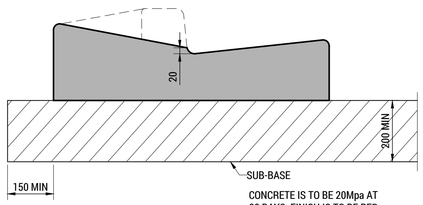


COMPACTION DETAILS		
EXPRESSED AS MMDD		
MATERIAL	**GENERAL**	**UNDER ROADS***
BEDDING MATERIAL	90%	90%
INITIAL BACKFILL	90%	95%
FINAL BACKFILL	SAME AS SURROUNDING SOIL	95%

* OR AS DIRECTED BY SUPERINTENDENT

TYPICAL COMBINED TRENCH DETAIL

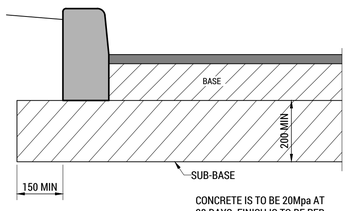
SCALE 1:10



KERB AND CHANNEL VEHICULAR CROSSING 'KCV' (TYP.)

SCALE 1:10

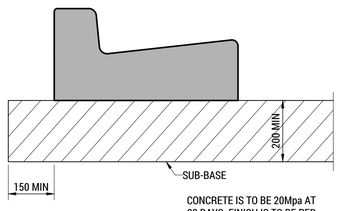
REFER IPWEA STD DWG TSD-R14-v3
FOR APPROVED KERB & CHANNEL
PROFILES & DIMENSIONS



BARRIER KERB 'BK' (TYP.)

SCALE 1:10

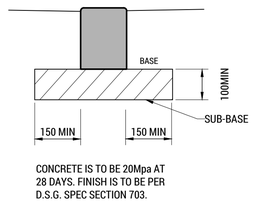
REFER IPWEA STD DWG TSD-R14-v3
FOR APPROVED KERB & CHANNEL
PROFILES & DIMENSIONS



KERB AND CHANNEL 'KC' (TYP.)

SCALE 1:10

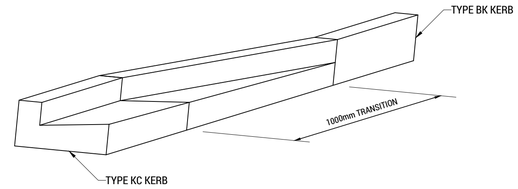
REFER IPWEA STD DWG TSD-R14-v3
FOR APPROVED KERB & CHANNEL
PROFILES & DIMENSIONS



FLUSH KERB 'FK' (TYP.)

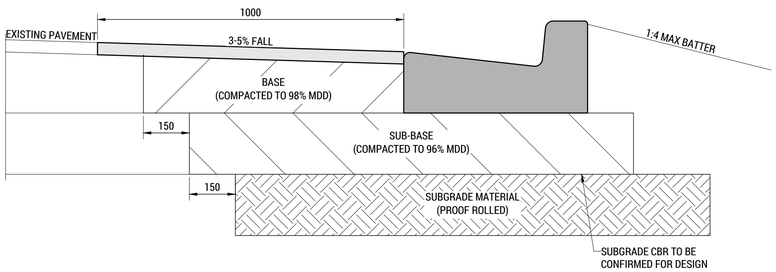
SCALE 1:10

REFER IPWEA STD DWG TSD-R14-v3
FOR APPROVED KERB & CHANNEL
PROFILES & DIMENSIONS



KC TO BK KERB TRANSITION (TYP.)

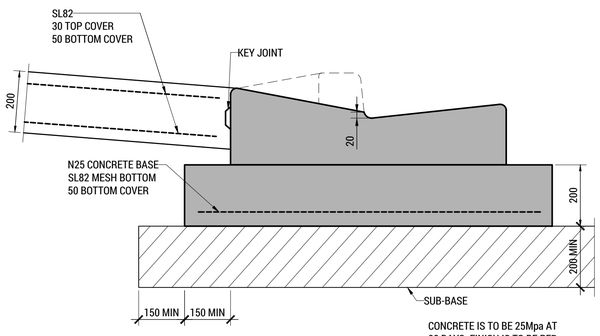
SCALE 1:20



KERB CUT IN DETAIL (TYP.)

SCALE 1:10

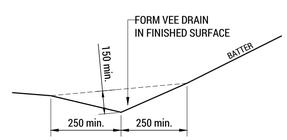
NOTE - HOTMIX PAVEMENT - ROADWAYS FOR BASE MATERIAL TYPES & DEPTHS



VEHICULAR CROSSING HEAVY DUTY BASE 'KCRB' (TYP.)

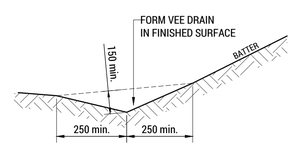
SCALE 1:10

REFER IPWEA STD DWG TSD-R16-v3
FOR APPROVED KERB & CHANNEL
PROFILES & DIMENSIONS



VEE DRAIN DETAIL

SCALE 1:20



VEE DRAIN DETAIL

SCALE 1:20

GLENORCHY CITY COUNCIL
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CLIENT / ARCHITECT:
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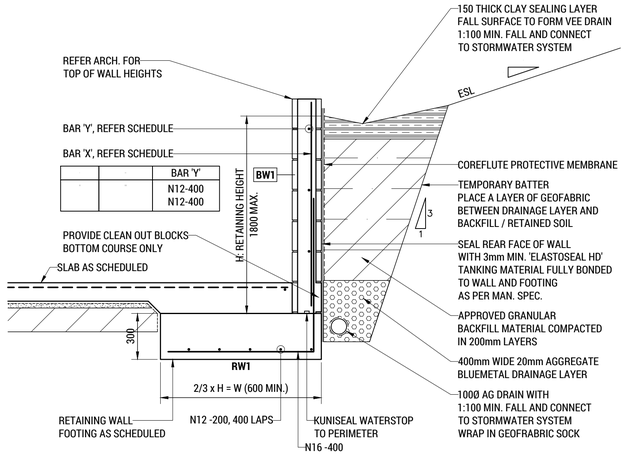
PROJECT DETAILS:
168a ABBOTSFIELD ROAD, CLAREMONT
UNITS DEVELOPMENT

DESIGN BY:	DESIGN CHECK:	DRAWN BY:	DRAFT CHECK:	CERTIFIER:
AJL	JTA	OWM	JTA	

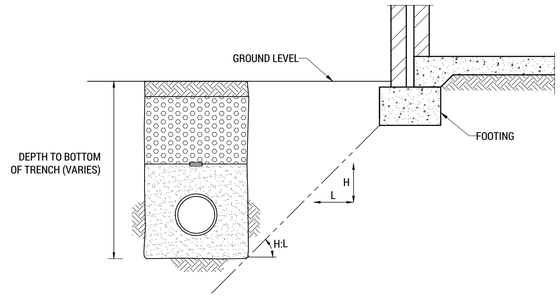
DRAWING TITLE:
SECTIONS & DETAILS - SHEET 2

SCALE:	PROJECT No:	DRAWING No:	REVISION:
1:20, 1:10 @ A1 1:40, 1:20 @ A3	241043	C802	E



SECTION DETAIL - RETAINING WALL 'RW1' (TYP.)

SCALE 1:20

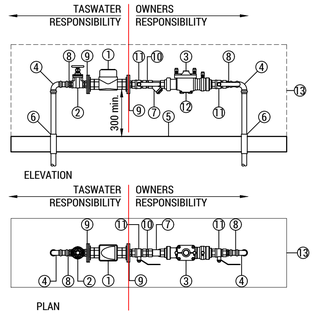


SOIL TYPE	SLOPE H:L	
	COMPACTED FILL	UNDISTURBED GROUND
STABLE ROCK	2:3	8:1
SAND	1:2	1:2
SILT	1:4	1:4
FIRM CLAY	1:2	1:1
SOFT CLAY	NOT SUITABLE	2:3
SOFT SOILS	NOT SUITABLE	NOT SUITABLE

(TABLE ADAPTED FROM NCC 2016 BCA - VOLUME TWO)

TYPICAL 'ZONE OF INFLUENCE' DETAIL

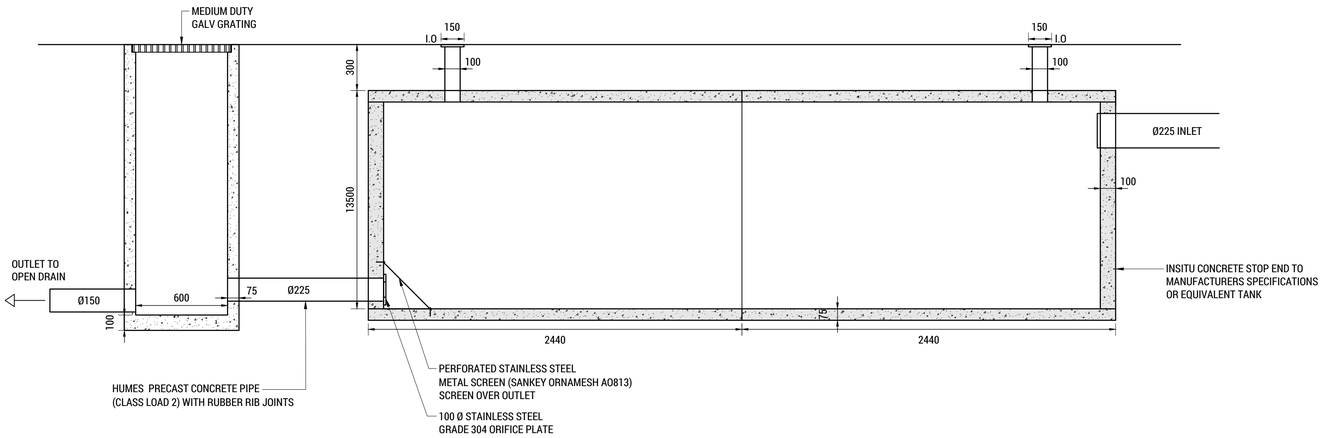
SCALE 1:20



EQUIPMENT SCHEDULE (PER UNIT)	
ITEM	DESCRIPTION
1	40mm 'SENSUS' IPERL WATER METER WITH DUAL CHECK VALVE - SUPPLIED BY TASWATER
2	40mm GATE VALVE - AVK OR SIMILAR - SUPPLIED BY TASWATER
3	40mm HIGH HAZARD 'VALVCHEQ' RPZD RP03 VALVE ONLY
4	40mm TYPE 'A' COPPER PIPE WORK
5	100 THICK N24 CONCRETE SLAB, SL72 CENTRAL
6	DENSO WRAP PIPE THROUGH SLAB PENETRATION
7	40mm STRAINER
8	B PRESS FITTINGS OR SIMILAR
9	40mm BSP TO TABLE E FLANGE ADAPTOR
10	40mm NIPPLE
11	40mm BALL VALVE - LOCKABLE QUARTER TURN BRASS DZR WITH BRASS HANDLE, RESILIENT SEATED
12	VENT
13	GALV. METAL CAGE - REFER TWS-W-0003 SERIES DRAWINGS FOR DETAILS

D01 - 40mm HIGH HAZARD METER DETAIL

SCALE 1:20



ALTERNATE ONSITE STORMWATER UNDERGROUND DETENTION

SCALE 1:20

GLENORCHY CITY COUNCIL
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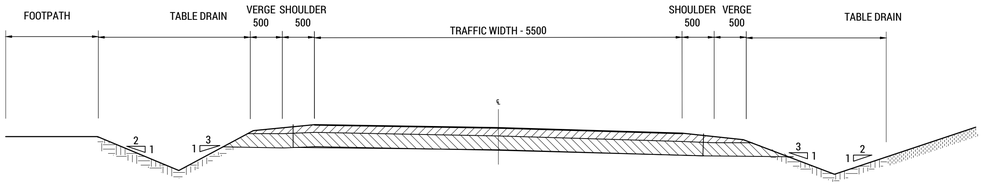
PROJECT DETAILS:
168a ABBOTSFIELD ROAD, CLAREMONT
UNITS DEVELOPMENT

DESIGN BY:	DESIGN CHECK:	DRAWN BY:	DRAFT CHECK:	CERTIFIER:
AJL	JTA	OWM	JTA	

DRAWING TITLE:
SECTIONS & DETAILS - SHEET 3

SCALE:	PROJECT No:	DRAWING No:	REVISION:
1:20, 1:10 @ A1 1:40, 1:20 @ A3	241043	C803	E



TYPICAL ROAD CROSS SECTION - ROAD 1

SCALE 1:50

GLENORCHY CITY COUNCIL
PLANNING SERVICES

APPLICATION No. : PLN-24-270

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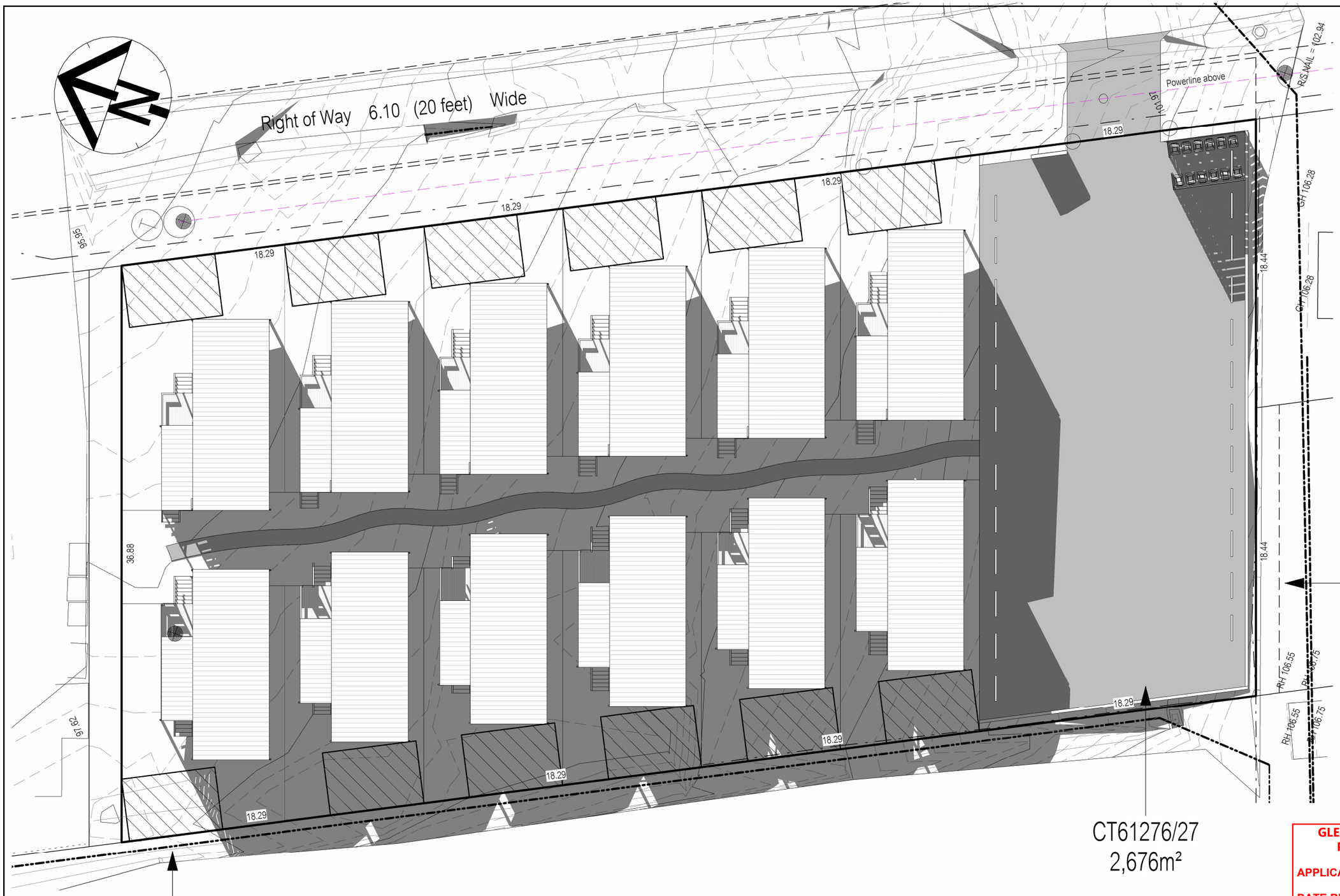
UNITS DEVELOPMENT

DESIGN BY:	DESIGN CHECK:	DRAWN BY:	DRAFT CHECK:	CERTIFIER:
AJL	JTA	OWM	JTA	

DRAWING TITLE:
SECTIONS & DETAILS - SHEET 4

SCALE:	PROJECT No:	DRAWING No:	REVISION:
1:50 @ A1 1:100 @ A3	241043	C804	B



"THIS PLAN AND ASSOCIATED DIGITAL MODEL IS PREPARED FOR CUNIC HOMES PTY LTD FROM A COMBINATION OF FIELD SURVEY AND EXISTING RECORDS FOR THE PURPOSE OF DESIGNING NEW CONSTRUCTIONS ON THE LAND AND SHOULD NOT BE USED FOR ANY OTHER PURPOSE.

THE TITLE BOUNDARIES AS SHOWN ON THIS PLAN WERE NOT MARKED AT THE TIME OF THE SURVEY AND HAVE BEEN DETERMINED BY PLAN DIMENSIONS ONLY AND NOT BY FIELD SURVEY. NO MEASUREMENTS OR OFFSETS ARE TO BE DERIVED BETWEEN THE FEATURES ON THIS PLAN AND THE BOUNDARY LAYER. THE RELATIONSHIP BETWEEN THE FEATURES IN THIS MODEL AND THE BOUNDARY LAYERS CANNOT BE USED FOR ANY SET OUT PURPOSES OR TO CONFIRM THE POSITION OF THE TITLE BOUNDARIES ON SITE. DUE TO THE NATURE OF THE TITLE BOUNDARY INFORMATION, IF ANY STRUCTURES ARE DESIGNED ON OR NEAR A BOUNDARY WE WOULD RECOMMEND A RE-MARK SURVEY BE COMPLETED AND LODGED WITH THE LAND TITLES OFFICE TO SUPPORT THE BOUNDARY DEFINITION.

SERVICES SHOWN HAVE BEEN LOCATED WHERE VISIBLE BY FIELD SURVEY. SERVICES DENOTED AS BEING "PER DBYD ONLY" ARE APPROXIMATE AND FOR ILLUSTRATIVE PURPOSES ONLY. PRIOR TO ANY DEMOLITION, EXCAVATION OR CONSTRUCTION ON THE SITE, THE RELEVANT AUTHORITY SHOULD BE CONTACTED FOR POSSIBLE LOCATION OF FURTHER UNDERGROUND SERVICES AND DETAILED LOCATIONS OF ALL SERVICES.

IF SUBSEQUENT DESIGN IS INTENDED FOR CONSTRUCTION SETOUT, FUTURE SURVEYING SETOUT COSTS ARE INCREASED IF THE DIGITAL DATA PROVIDED IS ROTATED, SCALED OR MOVED.

THIS NOTE FORMS AN INTEGRAL PART OF THE PLAN/DATA. ANY REPRODUCTION OF THIS PLAN/MODEL WITHOUT THIS NOTE ATTACHED WILL RENDER THE INFORMATION SHOWN INVALID.

GLENORCHY CITY COUNCIL PLANNING SERVICES

APPLICATION No. : PLN-23-270

DATE RECEIVED: 25/02/2025

0 2 4 6 8m

1:250

NOTES
- LATITUDE: -42°47' LONGITUDE: 147°14'
- No allowance for surrounding topography.
- Ground terrain for development site derived from detail survey.

No.	Date	Int.	Amendment changes as per cover sheet
B	2 August 2024	ST	

Notes
- Builder to verify all dimensions and levels on site prior to commencement of work
- All work to be carried out in accordance with the current National Construction Code.
- All materials to be installed according to manufacturers specifications.
- Do not scale from these drawings.
- No changes permitted without consultation with designer.

Designer:

ANOTHER PERSPECTIVE PTY LTD
PO BOX 21
NEW TOWN
LIC. NO. 685230609 (S. Turvey)
Ph: (03) 6231 4122
Fx: (03) 6231 4166
Email:
info@anotherperspective.com.au

Client / Project info

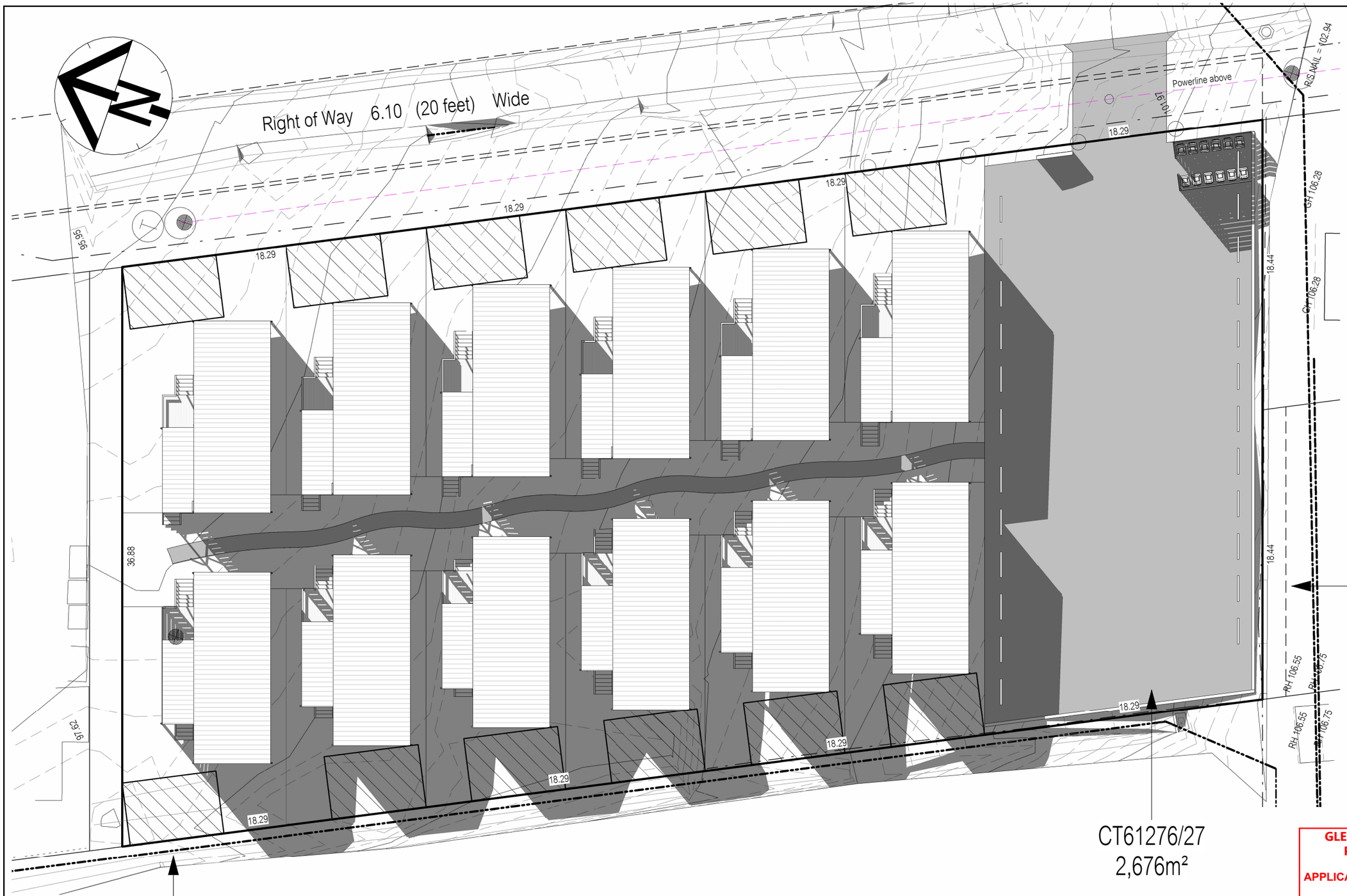
PROPOSED CUNIC UNIT DEVELOPMENT

168a Abbotsfield Road
CLAREMONT

CUNIC homes
Built for you.

SHADOW DIAGRAM - 21/06/25 9am

Drawn	ST	U249	
Date	11 July 2024	Sheet	N/A
Scale	1 : 250		



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GLENORCHY CITY COUNCIL PLANNING SERVICES

APPLICATION No. : PLN-23-270

DATE RECEIVED: 25/02/2025

0 2 4 6 8m

1:250

NOTES
- LATITUDE: -42°47'
  LONGITUDE: 147°14'
- No allowance for surrounding topography.
- Ground terrain for development site derived from detail survey.

No.	Date	Int.
B	2 August 2024	ST

Amendment changes as per cover sheet

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Client / Project info

PROPOSED CUNIC UNIT DEVELOPMENT

168a Abbotsfield Road
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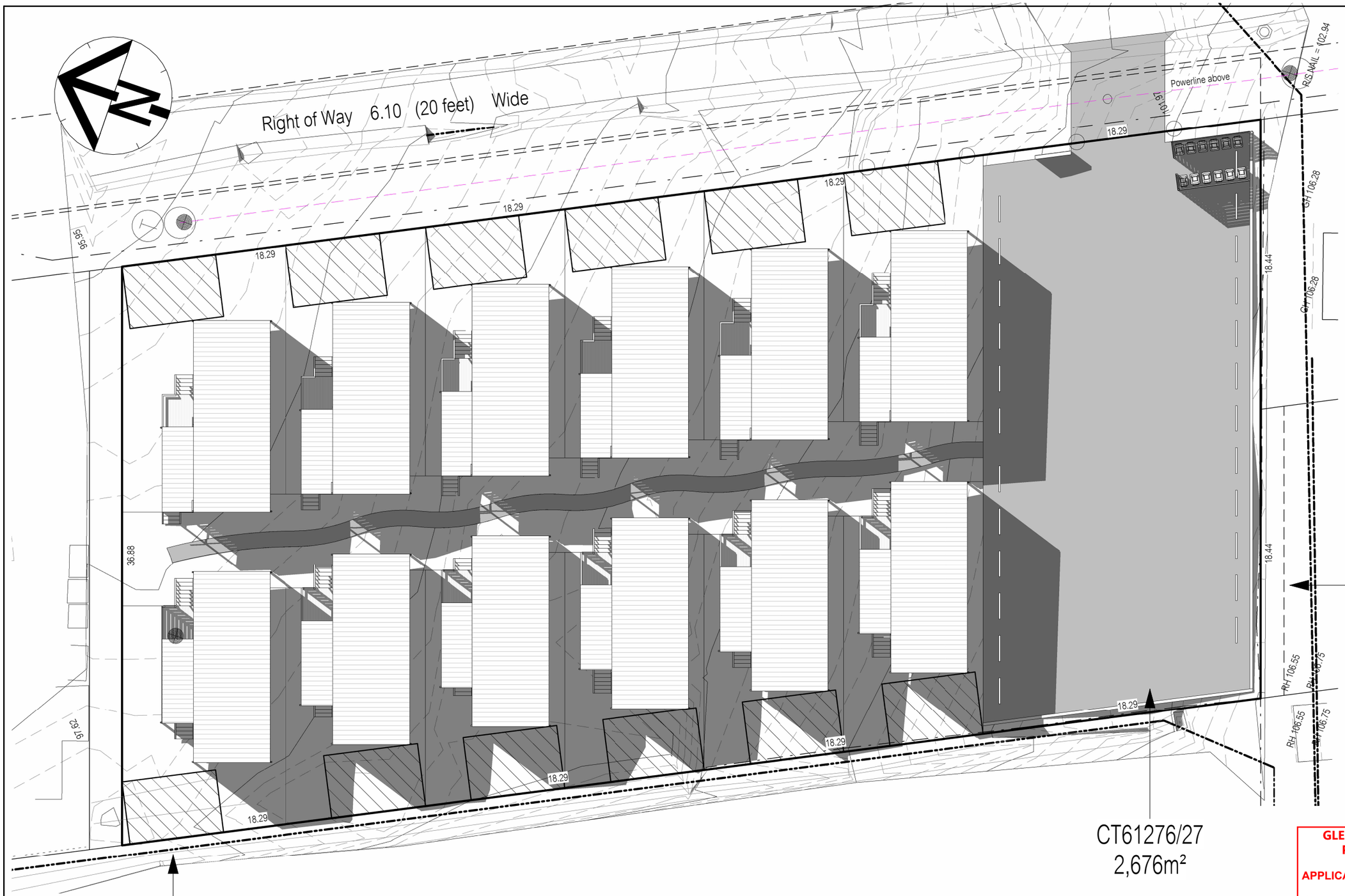
SHADOW DIAGRAM - 21/06/25 10am

Drawn	ST	U249	
Date	11 July 2024	Sheet	N/A
Scale	1 : 250		



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APPLICATION No. : PLN-23-270

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PROPOSED CUNIC UNIT DEVELOPMENT

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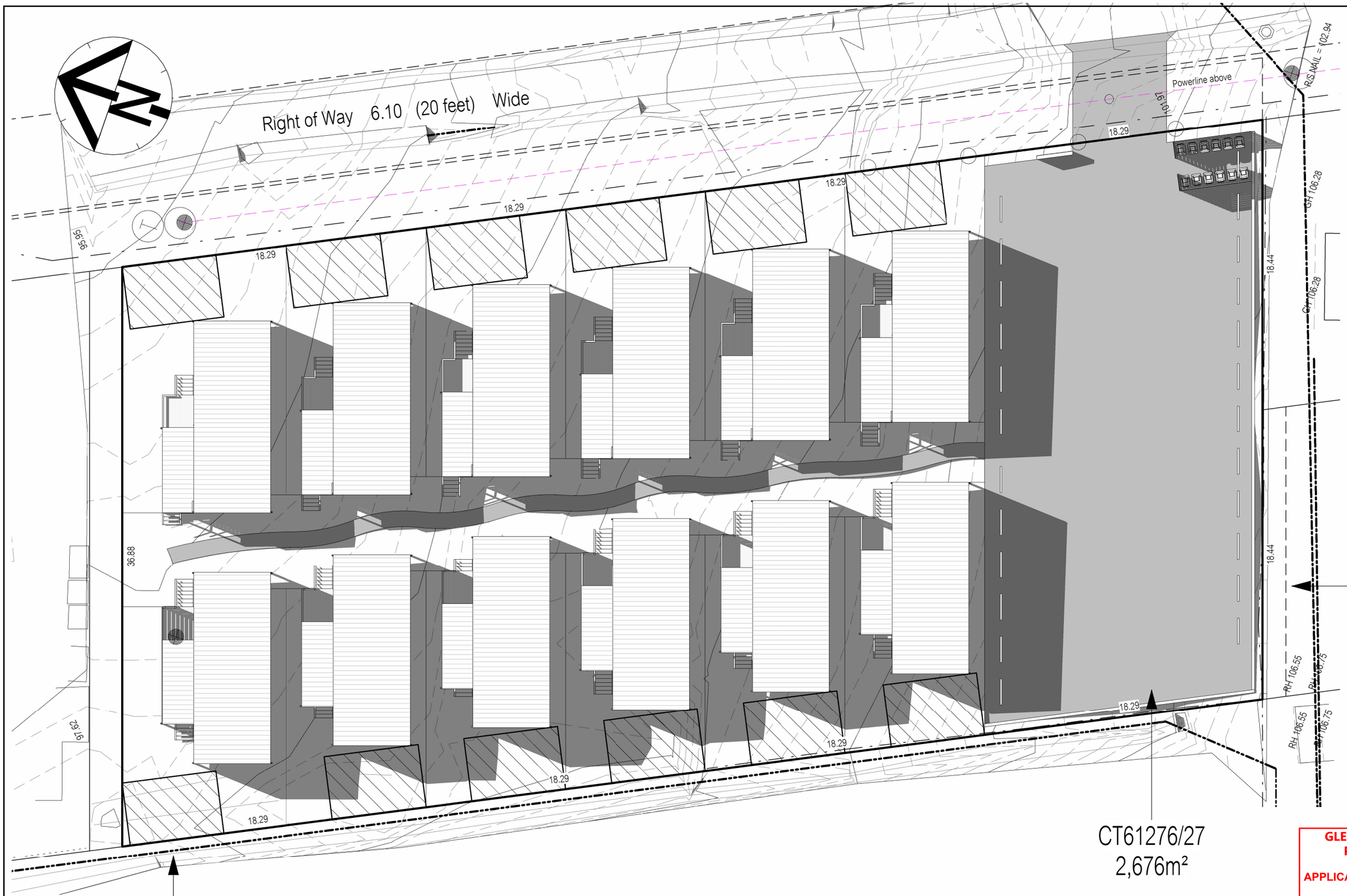
SHADOW DIAGRAM - 21/06/25 11am

Drawn	ST	U249	
Date	11 July 2024	Sheet	N/A
Scale	1 : 250		



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GLENORCHY CITY COUNCIL PLANNING SERVICES

APPLICATION No. : PLN-23-270

DATE RECEIVED: 25/02/2025

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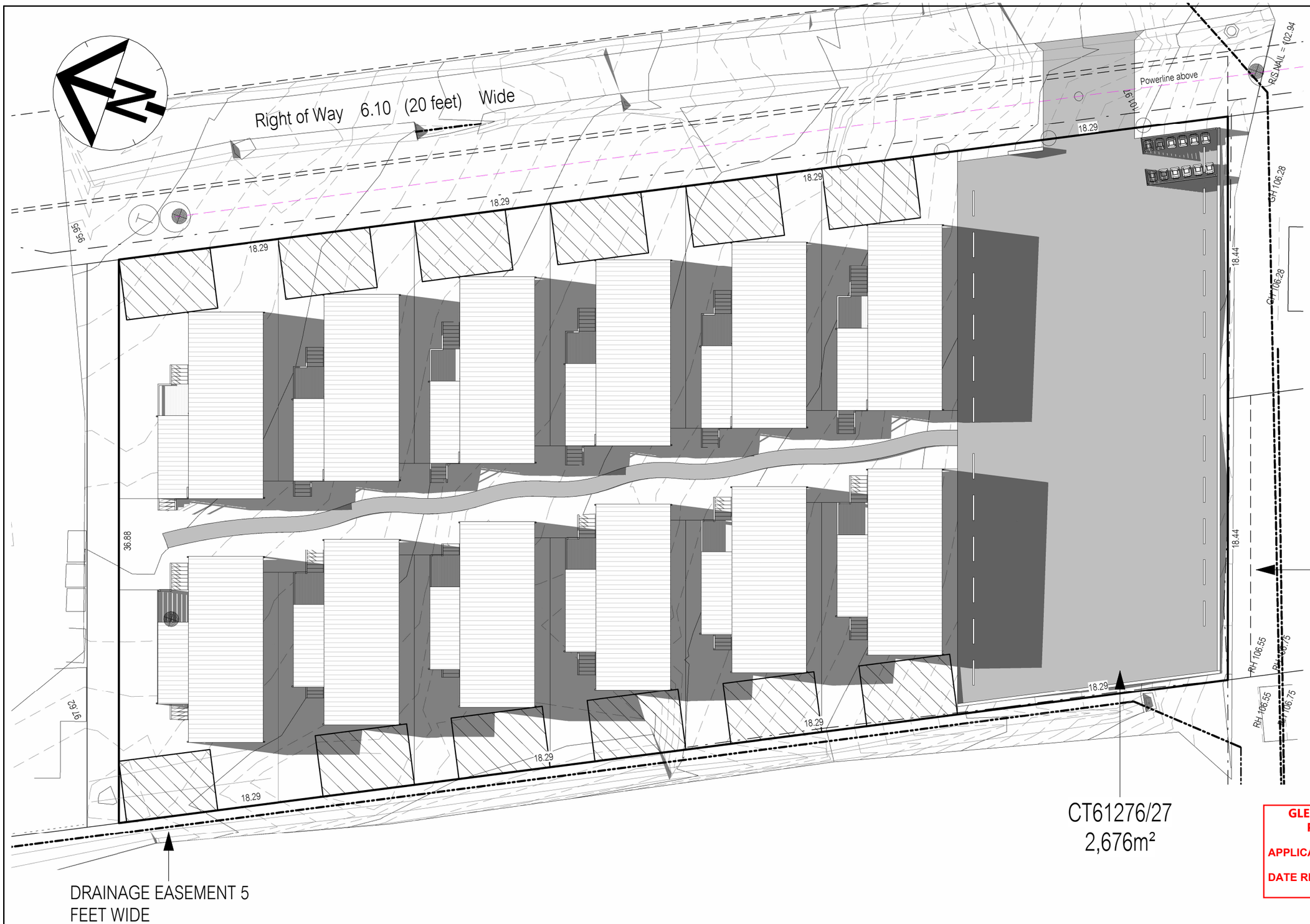
SHADOW DIAGRAM - 21/06/25 12pm

Drawn	ST	U249	
Date	11 July 2024	Sheet	N/A
Scale	1 : 250		



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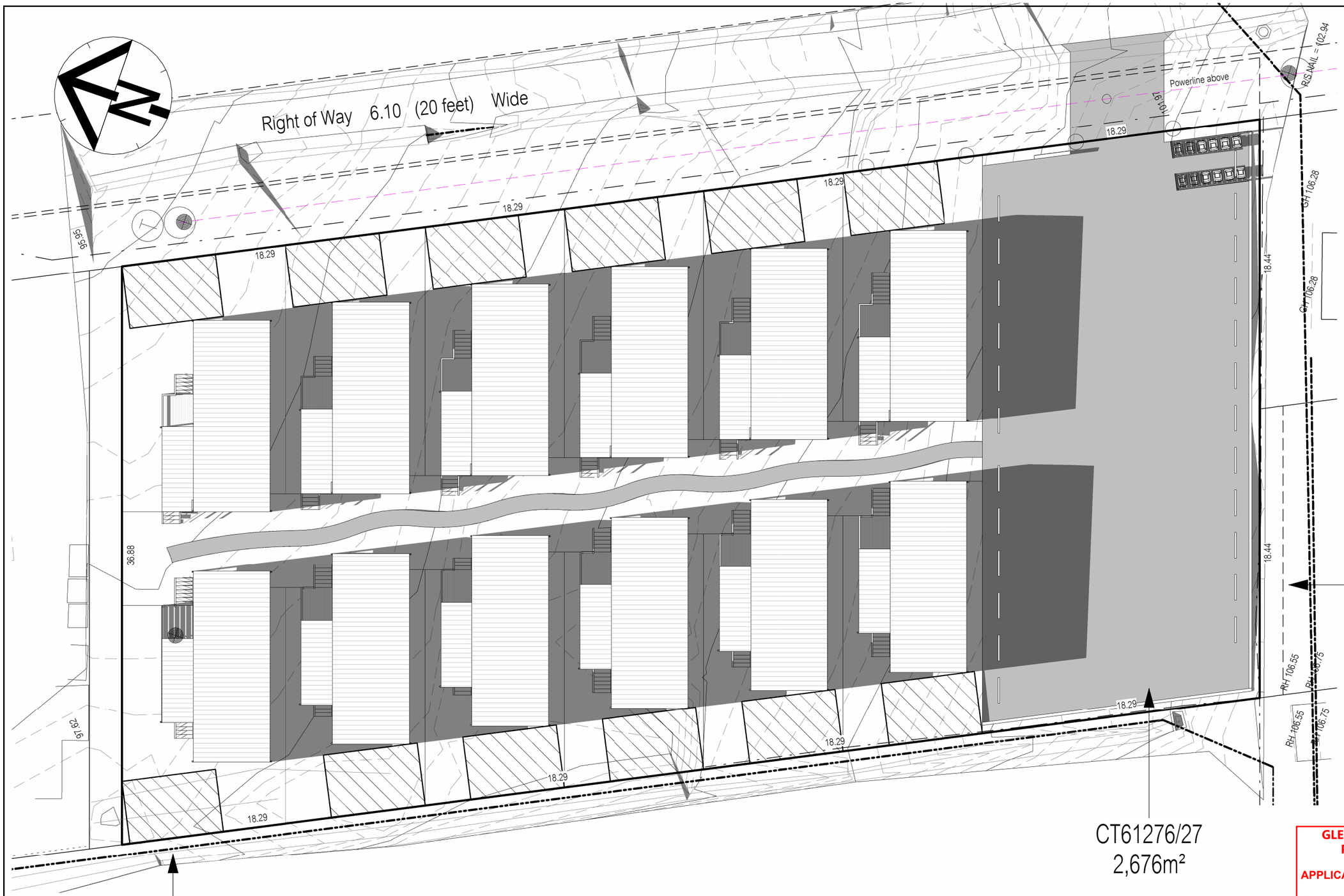
SHADOW DIAGRAM - 21/06/25 1pm

Drawn	ST	U249	
Date	11 July 2024	Sheet	N/A
Scale	1 : 250		



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SHADOW DIAGRAM - 21/06/25 2pm

Drawn	ST	U249	
Date	11 July 2024	Sheet	N/A
Scale	1 : 250		

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Drawn	ST	U249	
Date	11 July 2024	Sheet	N/A
Scale	1 : 250		